



## HARDIN COUNTY ROAD DEPARTMENT

501 Bacon Creek Road  
Elizabethtown, Kentucky 42701  
Phone: 270-737-6046  
Email: dmorgan@hcky.org

### UTILITY ENCROACHMENT PERMIT

Name of Applicant Kentucky Utilities Company

Type of Underground Utility Overhead Electric Transmission

Plan Sheets or Project Profile Yes  No

Name of Location of Proposed Project Overhead Electric Transmission

Near Glendale, KY (See attached plan sheets for exact location of crossings)

Length of Project 8.4 miles

Name of Contractor To be determined

Address To be determined

Name of Insurance Company Marsh USA Inc

Address 1717 Arch Street, Philadelphia, PA 19103-2797

Phone Number 215-246-1000

Begin Date August 2022 End Date August 2023

Hardin County Road Department Inspection Date \_\_\_\_\_

Issued By [Signature] Date 5/20/22



Andy Beshear  
Governor

COMMONWEALTH OF KENTUCKY  
TRANSPORTATION CABINET  
Department of Highways, District 4 Office  
634 East Dixie Highway  
Elizabethtown, Kentucky 42701  
(270) 766-5066  
[www.transportation.ky.gov/](http://www.transportation.ky.gov/)

Jim Gray  
Secretary

February 28, 2022

Kentucky Utilities  
Mike Kern  
1 Quality St.  
Lexington, Kentucky 40507

Subject: Permit #: 04-2022-00059  
Permit Type: Utilities - Electric  
Approval

Dear Applicant:

Attached is your permit approval and documentation for the subject permit.

Be advised that all work must be done in conformity with permit and application conditions. If you have any questions, please contact the Permits Section at this office.

Sincerely,

A handwritten signature in cursive script that reads "Jacob Riggs".

Jacob Riggs  
D4 Permits - Supervisor

Attachments



An Equal Opportunity Employer M/F/D



Kentucky Transportation Cabinet  
 Department of Highways  
 Division of Maintenance  
 Permits Branch

TC 99-1 (B)  
 07/2018  
 Page 1 of 1

**ENCROACHMENT PERMIT**

**KYTC KEPT #:** 04-2022-00059

**Permittee:** Kentucky Utilities

**Permit Type / Subtype:** Utilities / Electric

**Work Completion Date:** 8/28/2022

INDEMNITIES		
Type	Amount Required	Tracking Number
Performance Bond	\$0.00	
Cash / Check	\$0.00	
Self-Insured	\$0.00	
Payment Bond	\$0.00	
Liability Insurance	\$0.00	

This permit has been:      **APPROVED**       **DENIED**

Jacob Riggs	D4 Permits - Supervisor	2/28/2022
<b>SIGNATURE</b>	<b>TITLE</b>	<b>DATE</b>

The TC 99-1(B), including the application TC-99 1(A) and all related and accompanying documents and drawings make up the permit. It is not a permit unless both the TC 99-1(A) and TC 99-1(B) are both present.

LOCATION(S)			
Description	County - Route	Latitude	Longitude
	Hardin - WK 9001	37.647991	-85.909687
	Hardin - KY 1136	37.607721	-85.903754
	Hardin - KY 222	37.599950	-85.879895
	Hardin - KY 222	37.601593	-85.900439



To Submit a Locate Request  
 24 Hours a Day, Seven Days a Week:  
 Call 811 or 800-752-6007



KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 PERMITS BRANCH

TC 99-1A  
 Rev. 10/2020  
 Page 1 of 4

**APPLICATION FOR ENCROACHMENT PERMIT**

KYTC KEPT #: \_\_\_\_\_

**SECTION 1: APPLICANT CONTACT INFORMATION**

<b>APPLICANT</b> Kentucky Utilities	<b>ADDRESS</b> 1 Quality Street		
<b>EMAIL</b> Michael.kern@lge-ku.com	<b>CITY</b> Lexington	<b>STATE</b> KY	<b>ZIP</b> 40507
<b>CONTACT NAME 1</b> Mike Kern	<b>EMAIL</b> Michael.kern@lge-ku.com	<b>PHONE #</b> 859-367-5674	
		<b>CELL #</b> 859-310-3230	
<b>CONTACT NAME 2 (if applicable)</b> Addam Gooch	<b>EMAIL</b> addam.gooch@lge-ku.com	<b>PHONE #</b> 859-367-5416	
		<b>CELL #</b> 859-613-4682	

**SECTION 2: PROPOSED WORK LOCATION**

<b>ADDRESS</b> KY-222	<b>CITY</b> Glendale	<b>STATE</b> Kentucky	<b>ZIP</b> 42740
<b>COUNTY</b> Hardin	<b>ROUTE #</b> KY-222	<b>MILE POINT</b> 5.8	<b>LONGITUDE (X)</b> -85.879895°
			<b>LATITUDE (Y)</b> 37.599950°

**ADDITIONAL LOCATION INFORMATION:**

**FOR KYTC USE ONLY**

**PERMIT TYPE:**  Air Right  Entrance  Utilities  Vegetation Removal  Other: \_\_\_\_\_

**ACCESS:**  Full  Partial  by Permit **LOCATION:**  Left  Right  Crossing

**SECTION 3: GENERAL DESCRIPTION OF WORK**

Installing a new 345kV transmission line. The wire heights above the road shall be at or above KYTC minimum clearance and the structures will be off of KYTC R/W. The project will require a temporary closure of KY - 222. See "Exhibit A" for a plan view drawing of the encroachment location. Also see the traffic control plan and TC 99-212 for the project attached.

THE UNDERSIGNED APPLICANT(s), being duly authorized representative(s) or owner(s), DO AGREE TO ALL ORIGINAL UNEDITED TERMS AND CONDITIONS ON THE TC 99-1A, pages 1-4.

24 February 2022

\_\_\_\_\_  
**SIGNATURE**

\_\_\_\_\_  
**DATE**

This is not a permit unless and until the applicant(s) receives an approved TC 99-1B from KYTC. This application shall become void if not approved by the cancellation date. The cancellation date shall be a minimum of one year from the date the applicant submits their application.



KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 PERMITS BRANCH

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**APPLICATION FOR ENCROACHMENT PERMIT**

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<b>COUNTY</b> Hardin	<b>ROUTE #</b> KY-222	<b>MILE POINT</b> 4.7	<b>LONGITUDE (X)</b> -85.900439°
<b>LATITUDE (Y)</b> 37.601593°			

**ADDITIONAL LOCATION INFORMATION:**

**FOR KYTC USE ONLY**

**PERMIT TYPE:**  Air Right  Entrance  Utilities  Vegetation Removal  Other: \_\_\_\_\_

**ACCESS:**  Full  Partial  by Permit **LOCATION:**  Left  Right  Crossing

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THE UNDERSIGNED APPLICANT(s), being duly authorized representative(s) or owner(s), DO AGREE TO ALL ORIGINAL UNEDITED TERMS AND CONDITIONS ON THE TC 99-1A, pages 1-4.

17 February 2022

\_\_\_\_\_  
**SIGNATURE**

\_\_\_\_\_  
**DATE**

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KENTUCKY TRANSPORTATION CABINET  
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**APPLICATION FOR ENCROACHMENT PERMIT**

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		<b>CELL #</b> 859-310-3230	
<b>CONTACT NAME 2 (if applicable)</b> Addam Gooch	<b>EMAIL</b> addam.gooch@lge-ku.com	<b>PHONE #</b> 859-367-5416	
		<b>CELL #</b> 859-613-4682	

**SECTION 2: PROPOSED WORK LOCATION**

<b>ADDRESS</b> KY-1136	<b>CITY</b> Glendale	<b>STATE</b> Kentucky	<b>ZIP</b> 42740
<b>COUNTY</b> Hardin	<b>ROUTE #</b> KY-1136	<b>MILE POINT</b> 4.5	<b>LONGITUDE (X)</b> -85.903730°
			<b>LATITUDE (Y)</b> 37.607702°

**ADDITIONAL LOCATION INFORMATION:**

**FOR KYTC USE ONLY**

**PERMIT TYPE:**  Air Right  Entrance  Utilities  Vegetation Removal  Other: \_\_\_\_\_

**ACCESS:**  Full  Partial  by Permit **LOCATION:**  Left  Right  Crossing

**SECTION 3: GENERAL DESCRIPTION OF WORK**

Installing a new 345kV transmission line. The wire heights above the road shall be at or above KYTC minimum clearance and the structures will be off of KYTC R/W. The project will require a temporary closure of KY - 1136. See "Exhibit A" for a plan view drawing of the encroachment location. Also see the traffic control plan and TC 99-212 for the project attached.

THE UNDERSIGNED APPLICANT(s), being duly authorized representative(s) or owner(s), DO AGREE TO ALL ORIGINAL UNEDITED TERMS AND CONDITIONS ON THE TC 99-1A, pages 1-4.

17 February 2022

\_\_\_\_\_  
**SIGNATURE**

\_\_\_\_\_  
**DATE**

This is not a permit unless and until the applicant(s) receives an approved TC 99-1B from KYTC. This application shall become void if not approved by the cancellation date. The cancellation date shall be a minimum of one year from the date the applicant submits their application.



KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
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 Rev. 10/2020  
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**APPLICATION FOR ENCROACHMENT PERMIT**

KYTC KEPT #: \_\_\_\_\_

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<b>APPLICANT</b> Kentucky Utilities	<b>ADDRESS</b> 1 Quality Street		
<b>EMAIL</b> Michael.kern@lge-ku.com	<b>CITY</b> Lexington	<b>STATE</b> KY	<b>ZIP</b> 40507
<b>CONTACT NAME 1</b> Mike Kern	<b>EMAIL</b> Michael.kern@lge-ku.com	<b>PHONE #</b> 859-367-5674	
		<b>CELL #</b> 859-310-3230	
<b>CONTACT NAME 2 (if applicable)</b> Addam Gooch	<b>EMAIL</b> addam.gooch@lge-ku.com	<b>PHONE #</b> 859-367-5416	
		<b>CELL #</b> 859-613-4682	

**SECTION 2: PROPOSED WORK LOCATION**

<b>ADDRESS</b> KY-9001	<b>CITY</b> Glendale	<b>STATE</b> Kentucky	<b>ZIP</b> 42740
<b>COUNTY</b> Hardin	<b>ROUTE #</b> KY-9001	<b>MILE POINT</b> 132.2	<b>LONGITUDE (X)</b> -85.909681°
			<b>LATITUDE (Y)</b> 37.647987°

**ADDITIONAL LOCATION INFORMATION:**

**FOR KYTC USE ONLY**

**PERMIT TYPE:**  Air Right  Entrance  Utilities  Vegetation Removal  Other: \_\_\_\_\_

**ACCESS:**  Full  Partial  by Permit **LOCATION:**  Left  Right  Crossing

**SECTION 3: GENERAL DESCRIPTION OF WORK**

Installing a new 345kV transmission line. The wire heights above the road shall be at or above KYTC minimum clearance and the structures will be off of KYTC R/W. The project will require a temporary closure of KY - 9001. See "Exhibit A" and "Exhibit B" for plan and profile view drawings of the encroachment location. Also see the traffic control plan and TC 99-212 for the project attached.

THE UNDERSIGNED APPLICANT(s), being duly authorized representative(s) or owner(s), DO AGREE TO ALL ORIGINAL UNEDITED TERMS AND CONDITIONS ON THE TC 99-1A, pages 1-4.

21 February 2022

\_\_\_\_\_  
**SIGNATURE**

\_\_\_\_\_  
**DATE**

This is not a permit unless and until the applicant(s) receives an approved TC 99-1B from KYTC. This application shall become void if not approved by the cancellation date. The cancellation date shall be a minimum of one year from the date the applicant submits their application.



KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
PERMITS BRANCH

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## APPLICATION FOR ENCROACHMENT PERMIT

### TERMS AND CONDITIONS

1. The permit, including this application and all related and accompanying documents and drawings making up the permit, remains in effect and is binding upon the Applicant/Permittee, its successors and assigns, as long as the encroachment(s) exists and also until the permittee is finally relieved by the Department of Highways from all its obligations.
2. Applicant shall meet all requirements of the Clean Water Act if the project will disturb one acre or more, the applicant shall obtain a KPDES KYR10 Permit from the Kentucky Division of Water. All disturbed areas shall meet the requirements of the Department of Highway's Standard Specifications, Sections 212 and 213, as amended.
3. **INDEMNITY:**
  - A. **PERFORMANCE BOND:** The permittee shall provide to the Department a performance bond according to the Permits Manual, Section PE-203 as a guarantee of conformance with the Department's Encroachment Permit requirements.
  - B. **PAYMENT BOND:** At the discretion of the department, a payment bond shall be required of the permittee to ensure payment of liquidated damages assessed to the permittee.
  - C. **LIABILITY INSURANCE:** Liability insurance shall be required of the permittee (in an amount approved by the department) to cover all liabilities associated with the encroachment.
  - D. It shall be the responsibility of the permittee, its successors and assigns, to maintain all indemnities in full force and effect until the permittee is authorized to release the indemnity by the Department.
4. A copy of this application and all related documents making up the approved permit shall be given to the applicant and shall be made readily available for review at the work site at all times.
5. Perpetual maintenance of the encroachment is the responsibility of the permittee, its successors and assigns, with the approval of the Department as required, unless otherwise stated.
6. Permittee, its successors and assigns, shall comply with and agree to be bound by the requirements and terms of (a) this application and all related documents making up the approved permit, (b) by the Department's Permits Manual, and (c) by the Manual on Uniform Traffic Control Devices, both manuals as revised to and in effect on the date of issuance of the permit, all of which documents are made a part thereof by this reference. Compliance by the permittee, its successors and assigns, with subsequent revisions to applicable provisions of either manual or other policy of the Department may be made a condition of allowing the encroachment to persist under the permit.
7. Permittee agrees that this and any encroachment may be ordered removed by the Department at any time, and for any reason, upon thirty days written notice to the last known address of the applicant or to the address at the location of the encroachment. The permittee agrees that the cost of removing and of restoring the associated right-of-way is the responsibility of the permittee, its successors and assigns.
8. Permittee, its successors and assigns, agree that if the Department determines that motor vehicular safety deficiencies develop as a result of the installation or use of the encroachment, the permittee, its successors and assigns, shall provide and bear the expenses to adjust, relocate, or reconstruct the facilities, add signs, auxiliary lanes, or other corrective measures reasonably deemed necessary by the Department within a reasonable time after receipt of a written notice of such deficiency. The period within which such adjustments, relocations, additions, modifications, or other corrective measures must be completed will be specified in the notice.
9. Where traffic signals are required as a condition of granting the requested permit or are thereafter required to correct motor vehicular safety deficiencies, as determined by the Department, the costs for signal equipment and installation(s) shall be borne by the permittee, its successors and assigns and the Department in its reasonable discretion and only in accordance with the Department's current policy set forth in the Traffic Operations Manual and Permits Manual. Any modifications to the permittee's entrance necessary to accommodate signalization (including necessary easement(s) on private property) shall be the responsibility of the permittee, its successors and assigns, at no expense to the Department.





KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
PERMITS BRANCH

TC 99-1A  
Rev. 10/2020  
Page 3 of 4

### APPLICATION FOR ENCROACHMENT PERMIT

10. The requested encroachment shall not infringe on the frontage rights of an abutting owner without their written consent as hereinafter described. Each abutting owner shall express their consent, which shall be binding on their successors and assigns, by the submission of a notarized statement as follows, "I (we), \_\_\_\_\_, hereby consent to the granting of the permit requested by the applicant along Route \_\_\_\_\_, which permit does affect frontage rights along my (our) adjacent real property." By signature(s) \_\_\_\_\_, subscribed and sworn by \_\_\_\_\_, on this date \_\_\_\_\_.
11. The permit, if approved, is subject to the agreement that it shall not interfere with any similar rights or permit(s) previously granted to any other party, except as otherwise provided by law.
12. Permittee shall include documentation which describes the facilities to be constructed. Permittee, its successors and assigns, agree as a condition of the granting of the permit to construct and maintain any and all permitted facilities or other encroachments in strict accordance with the submitted and approved permit documentation and the policies and procedures of the Department. Permittee, its successors and assigns, shall not use facilities authorized herein in any manner contrary to that prescribed by the approved permit. Only normal usage as contemplated by the parties and by this application and routine maintenance are authorized by the permit.
13. Permittee, its successors and assigns, at all times from the date permitted work is commenced until such time as all permitted facilities or other encroachments are removed from the right-of-way and the right-of-way restored, **shall defend, protect, indemnify and save harmless** the Department from any and all liability claims and demands arising out of the work, encroachment, maintenance, or other undertaking by the permittee, its successors and assigns, related or undertaken pursuant to the granted permit, due to any claimed act or omission by the permittee, its servants, agents, employees, or contractors. This provision shall not inure to the benefit of any third party nor operate to enlarge any liability of the Department beyond that existing at common law or otherwise if this right to indemnity did not exist.
14. Upon a violation of any provision of the permit, or otherwise in its reasonable discretion, the Department may require additional action by the permittee, its successors and assigns, up to and including the removal of the encroachment and restoration of the right-of-way. In the event additional actions required by the Department under the permit are not undertaken as ordered and within a reasonable time, the Department may in its discretion cause those or other additional corrective actions to be undertaken and the Department shall recover the reasonable costs of those corrective actions from the permittee, its successors and assigns.
15. Permittee, its successors and assigns, shall use the encroachment premises in compliance with all requirements of federal law and regulation, including those imposed pursuant to Title VI of the Civil Right Act of 1964 (42 U.S.C. § 2000d et seq.) and the related regulations of the U.S. Department of Transportation in Title 49 C.F.R. Part 21, all as amended.
16. Permittee, its successors and assigns, agree that if the Department determines it is necessary for the facilities or other encroachment authorized by the permit to be removed, relocated or reconstructed in connection with the reconstruction, relocation or improvement of a highway, the Department may revoke permission for the encroachment to remain under the permit and may order its removal, relocation or reconstruction by the permittee, its successors and assigns, at the expense of the permittee, except where the Department is required by law to pay any or all of those costs.



KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
PERMITS BRANCH

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**APPLICATION FOR ENCROACHMENT PERMIT**

17. Permittee agrees that the authorized permit is personal to the permittee and shall remain in effect until such time as (a) the permittee's rights to the adjoining real property to have benefitted from the requested encroachment have been relinquished, (b) until all permit obligations have been assumed by appropriate successors and assigns, and (c) unless and until a written release from permit obligations has been granted by the Department. The permit and its requirements shall also bind the real property to have benefitted from the requested encroachment to the extent permitted by law. The permit and the related encroachment become the responsibility of the successors and assigns of the permittee and the successors and assigns of each property owner benefitting from the encroachment, or the encroachment may not otherwise permissibly continue to be maintained on the right-of-way. (Does not apply to utility encroachments serving the general public.)
18. If work authorized by the permit is within a highway construction project in the construction phase, it shall be the responsibility of the permittee to make personal contact with the Department's Engineer on the project in order to coordinate all permitted work with the Department's prime contractor on the project.
19. This permit is not intended to, nor shall it, affect, alter or alleviate any requirement imposed upon the permittee, its successors and assigns, by any other agency.
20. Permittee, its successors and assigns, agree to contain and maintain all dirt, mud, and other debris emanating from the encroachment away from the surrounding right-of-way and the travel way of the highway hereafter and at all times that its obligations under the permit remain in effect.
21. Before You Dig: The contractor is instructed to call 1-800-752-6007 to reach KY 811, the One-Call system for information on the location of existing underground utilities. The call is to be placed a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor should be aware that the owners of underground facilities are not required to be members of the KY 811 One-Call Before U-Dig (BUD) service. The contractor must coordinate excavation with the utility owners, including those whom do not subscribe to KY 811. It may be necessary for the contractor to contact the County Clerk to determine what utility companies have facilities in the area.
22. The undersigned Utility acknowledges ownership and control of the facilities proposed to be installed, modified, or extended by the Applicant/Permittee and agrees to be bound by the requirements and terms of this application and all related documents making up the approved permit, by the Department's Permits Guidance Manual, and by all applicable regulations and statutes in effect on the date of issuance of the permit. This information and application is certified correct to the best knowledge and belief of the undersigned Utility.

Kentucky Utilities

UTILITY

Michael Kern

NAME (Utility Representative)

SIGNATURE (Utility Representative)

Project Coordinator

TITLE (Utility Representative)

24 February 2022

DATE



To Submit a Locate Request  
24 Hours a Day, Seven Days a Week:  
Call 811 or 800-752-6007

## NOTICE OF COMPLETION OF ENCROACHMENT PERMIT WORK

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### PERMITTEE

Name: Kentucky Utilities  
Contact Person: Mike Kern  
Address: 1 Quality St.  
City: Lexington  
State: Kentucky  
Zip: 40507  
Telephone: (859) 310-3230

### PROJECT IDENTIFICATION

Permit Number: 04-2022-00059

---

I wish to notify the Department of Highways that the above mentioned permit work and any necessary right-of-way restoration have been completed and are ready for final inspection.

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Permittee

---

Please return this form to the address below when work is completed and ready for final inspection.

Please Return to: Permit Engineer  
Department of Highways, District 4 Office  
634 East Dixie Highway  
Elizabethtown, Kentucky 42701  
(270) 766-5066  
[www.transportation.ky.gov/](http://www.transportation.ky.gov/)

LOCATION(S)			
Description	County - Route	Latitude	Longitude
	Hardin - WK 9001	37.647991	-85.909687
	Hardin - KY 1136	37.607721	-85.903754
	Hardin - KY 222	37.599950	-85.879895
	Hardin - KY 222	37.601593	-85.900439

2/28/22, 3:46 PM

Permits

# Kentucky Encroachment Permit Tracking

## Maintenance Division



[Home](#) [Search+](#) [Reports+](#) [Add Permits](#) [Perks Data](#)

Welcome KYTC\steve.peace! [Sign Out](#)

**PERMIT QUICK SEARCH**

Search Criteria:  Search Value:  [Search](#)

**PERMIT GENERAL INFORMATION**

Permit/Tracking #:	T04-2022-00059	<a href="#">Follow</a>	Permit Status:	Active	Status Date:	2/28/2022
District:	04 - Elizabethtown		Created By:	Steve Peace	Created Date:	2/28/2022
Filing Date:	2/28/2022		<b>Kentucky Utilities : Applicant</b> <input type="text"/>			
Cancellation Date:	2/28/2023		Address:	1 Quality St. Lexington, KY 40507	Phone:	(859)310-3230
Completion Date:	8/28/2022				Email:	Michael.kern@lge-ku.com
Permit Type:	Utilities	<input checked="" type="checkbox"/> Company				<a href="#">Maintain Contacts</a>
Permit Subtype:	Electric					

**Application** **Attachments** **Routing** **Comments** **Locations/Inspections** **Indemnity** **Approvals**

**LOCATION DATA**

Description: **MOST RECENT INSPECTION**  
There are no inspections for this location.

County: **Hardin**  
Route: Multiple  
BMP/EMP: Multiple

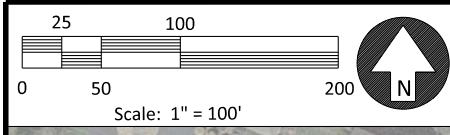
Activity: New Install  
Location: Overhead  
Contract Agr #:   
Bridge #:

Description:  
Applicant to install new 345KV transmission line which will cross KY 9001, 222, and 1136. Overhead wire shall meet KYTC minimum clearance standards and all structures shall be off KYTC ROW.

[Edit](#)

[View on Map](#) [Edit Permit](#)

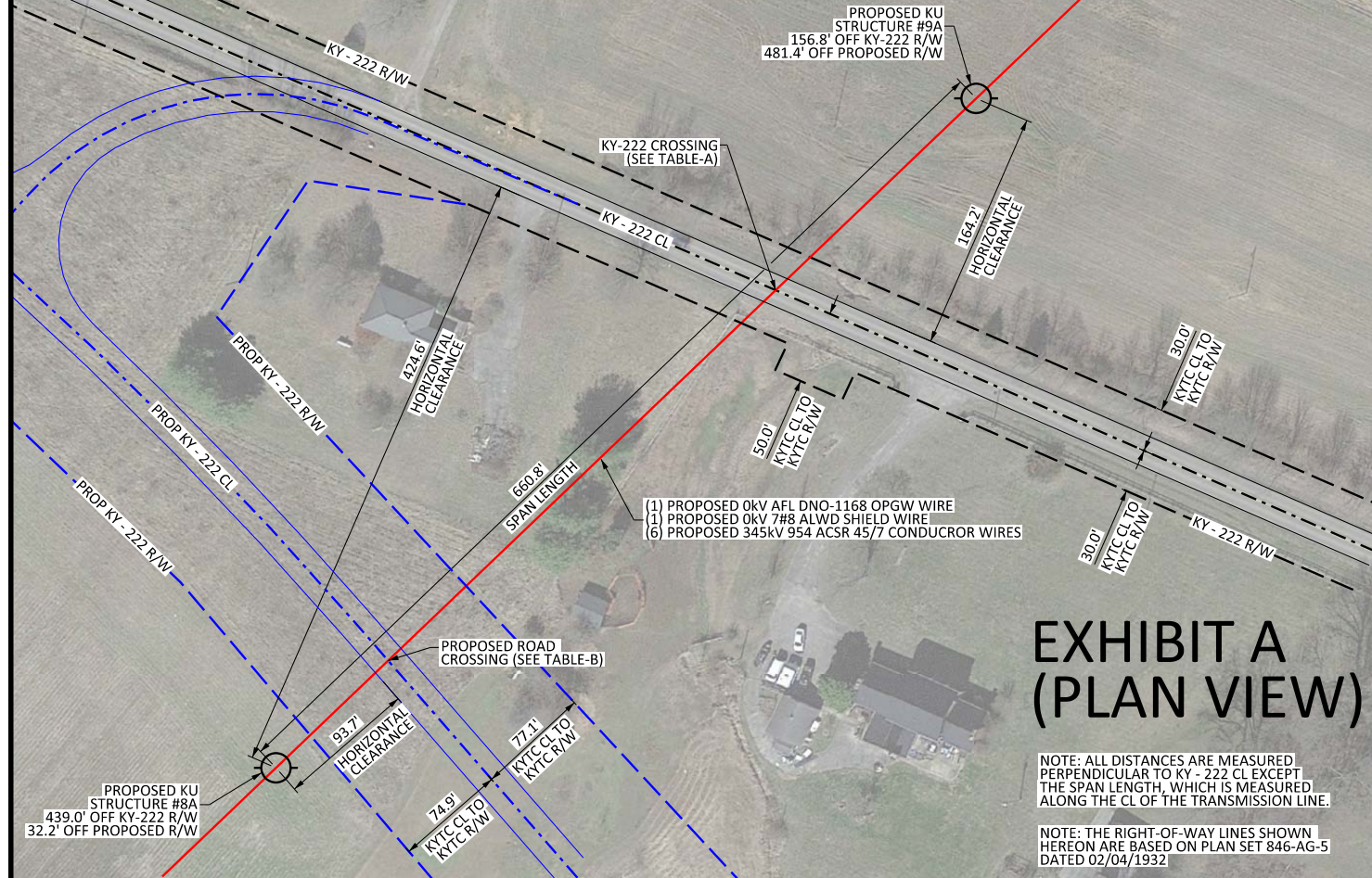
v1.1.7969.26696



NOTE: THE RIGHT-OF-WAY AND CENTERLINE SHOWN IN BLUE IS BASED ON PLANS PROVIDED TO AGE ENGINEERING BY KYTC ON 02/17/2022.

TABLE-A	
LATITUDE:	37.599950°
LONGITUDE:	-85.879895°
MILE MARKER	5.8
ANGLE OF CROSSING:	67.3°
TOTAL LENGTH OF WIRE ON KYTC R/W:	65.0'
LINE VOLTAGE:	345 kV
MINIMUM VERTICAL CLEARANCE:	61.3'

TABLE-B	
LATITUDE:	37.599245°
LONGITUDE:	-85.880805°
MILE MARKER	N/A
ANGLE OF CROSSING:	87.5°
TOTAL LENGTH OF WIRE ON KYTC R/W:	147.2'
LINE VOLTAGE:	345 kV
MINIMUM VERTICAL CLEARANCE:	69.8' (APPX)



- (1) PROPOSED 0kV AFL DNO-1168 OPGW WIRE
- (1) PROPOSED 0kV 7#8 ALWD SHIELD WIRE
- (6) PROPOSED 345kV 954 ACSR 45/7 CONDUCTOR WIRES

# EXHIBIT A (PLAN VIEW)

NOTE: ALL DISTANCES ARE MEASURED PERPENDICULAR TO KY - 222 CL EXCEPT THE SPAN LENGTH, WHICH IS MEASURED ALONG THE CL OF THE TRANSMISSION LINE.

NOTE: THE RIGHT-OF-WAY LINES SHOWN HEREON ARE BASED ON PLAN SET 846-AG-5 DATED 02/04/1932

DATE: 02/22/2022  
 SCALE: 1" = 100'  
 DRAWN BY: HSM  
 APPROVED BY: HSM  
 FILENAME: 21568 HWY

PLAN VIEW  
 KENTUCKY UTILITIES COMPANY  
 KY - 222 (PROPOSED AND EXISTING)  
 HARDIN COUNTY, KENTUCKY

P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097





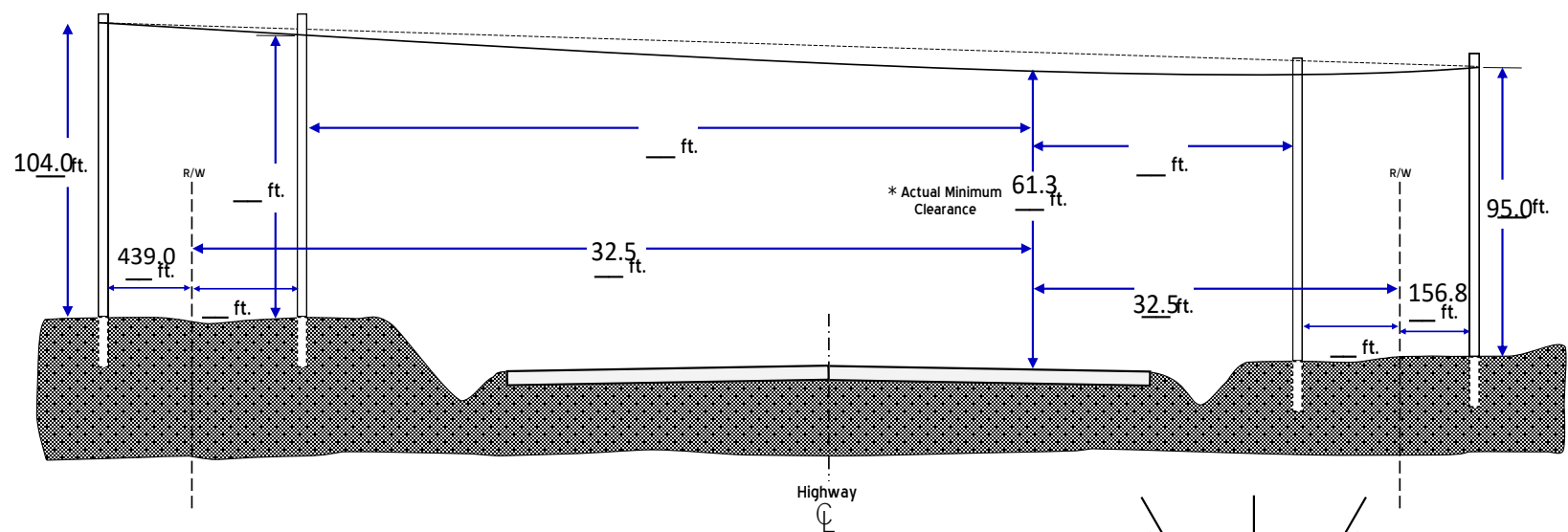
KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 DIVISION OF MAINTENANCE - PERMITS BRANCH

TC 99-212  
 Rev. 10/2018  
 Page 1 of 1

**OVERHEAD UTILITY ENCROACHMENT DIAGRAM FOR NON-FULLY CONTROLLED HIGHWAYS**

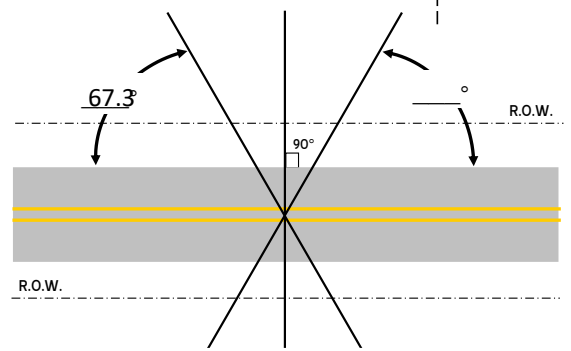
County: HARDIN Minimum Clearance to Roadway: 61.3'  
 Highway Route: KY - 222 Type of Utility Crossing: TRANSMISSION  
 Mile Point: 5.8 Voltage of Line: 345kV

<b>KYTC KEPT #:</b>



**GENERAL NOTES - For Non-Fully Controlled Access Highways:**

- The vertical clearance of overhead utility lines crossing shall be a minimum of 18 feet or greater per NESC guidelines or other applicable codes.





KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 DIVISION OF MAINTENANCE - PERMITS BRANCH

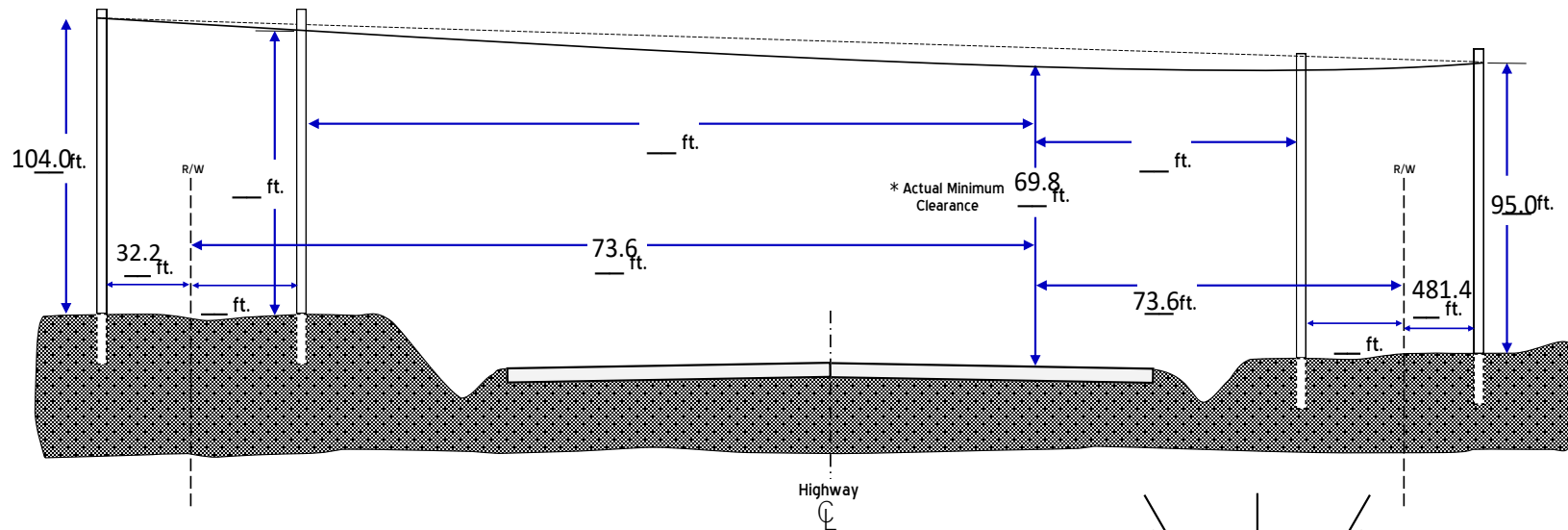
TC 99-212  
 Rev. 10/2018  
 Page 1 of 1

**OVERHEAD UTILITY ENCROACHMENT DIAGRAM FOR NON-FULLY CONTROLLED HIGHWAYS**

County: HARDIN  
 Highway Route: PROPOSED KY - 222  
 Mile Point: N/A

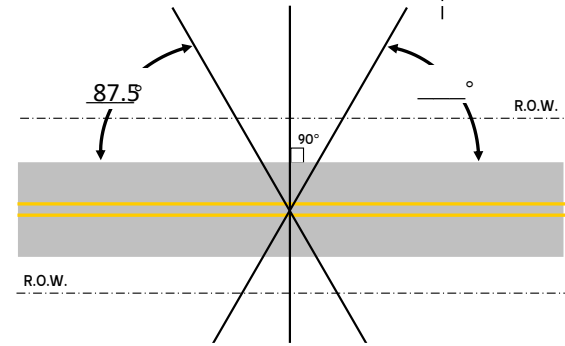
Minimum Clearance to Roadway: 69.8  
 Type of Utility Crossing: TRANSMISSION  
 Voltage of Line: 345kV

<b>KYTC KEPT #:</b>



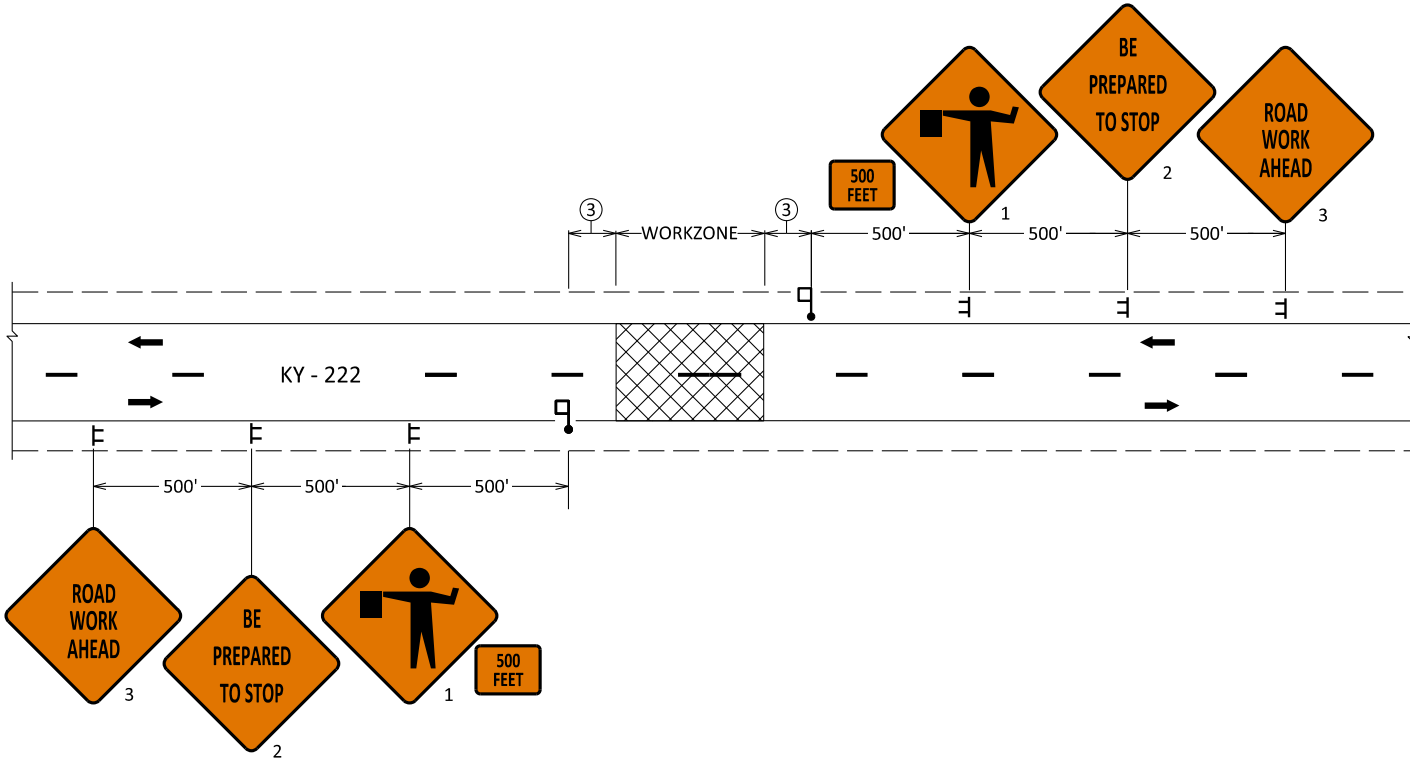
**GENERAL NOTES - For Non-Fully Controlled Access Highways:**

- The vertical clearance of overhead utility lines crossing shall be a minimum of 18 feet or greater per NESG guidelines or other applicable codes.



**KY - 222 TEMPORARY CLOSURE**

DRAWING NOT TO SCALE



~ NOTES ~

- 1.) THE MINIMUM SIZE OF SIGNS 1 THROUGH 3 SHALL BE 48" X 48" WITH 24" X 18' SUPPLEMENTAL PLAQUES.
- 2.) THE FLAGGERS SHALL BE IN SIGHT OF OR IN DIRECT COMMUNICATION WITH EACH OTHER AND THE CONSTRUCTION CREWS AT ALL TIMES.
- 3.) THERE SHALL BE A 50' BUFFER AREA BETWEEN THE FLAGGER STATION AND THE WORK ZONE.

**APPLICATION**

THIS DRAWING APPLIES TO WORK ON KU STRUCTURES WHICH WILL REQUIRE THE TEMPORARY CLOSURE OF KY - 222.

**LEGEND**

- FLAGGER
- WORK ZONE
- SIGN

**TRAFFIC CONTROL SUPERVISOR**

HUNTER MCKINNEY

PRINTED NAME

S18100056

WORK ZONE TRAFFIC CONTROL SUPERVISOR NUMBER

*Hunter McKinney*

APPROVAL SIGNATURE

02/11/2022

DATE

DATE: 02/11/2022

SCALE: NTS

DRAWN BY: HSM

APPROVED BY: HSM

FILENAME: 21568TCP

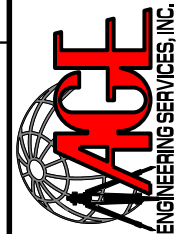
TRAFFIC CONTROL PLAN

KENTUCKY UTILITIES COMPANY

KY - 222

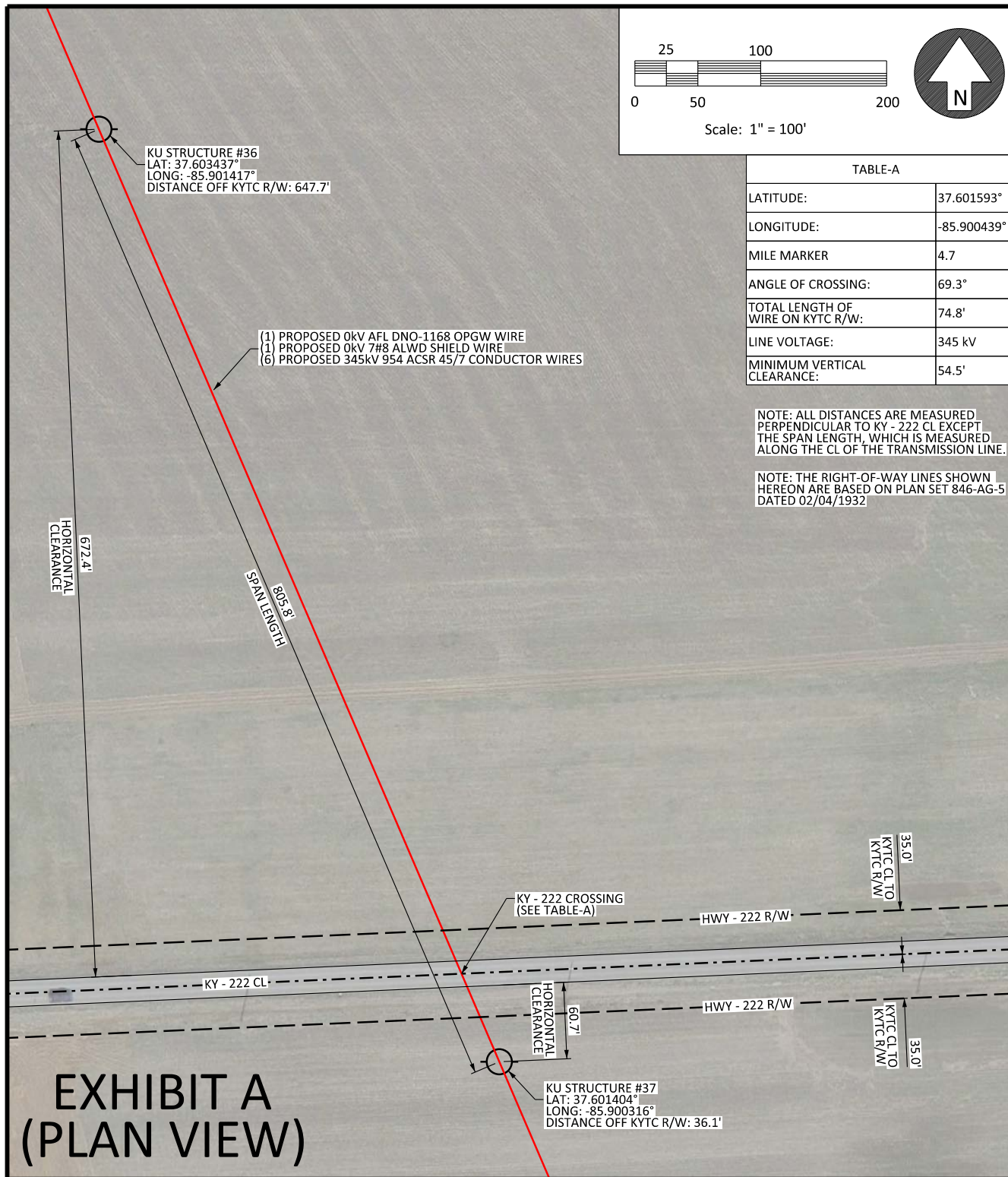
HARDIN COUNTY, KENTUCKY

P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097



ENGINEERING SERVICES, INC.






# EXHIBIT A (PLAN VIEW)



P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097

PLAN VIEW  
 KENTUCKY UTILITIES COMPANY  
 KY - 222  
 HARDIN COUNTY, KENTUCKY

DATE: 02/14/2022  
 SCALE: 1" = 100'  
 DRAWN BY: HSM  
 APPROVED BY: HSM  
 FILENAME: 21568 HWY



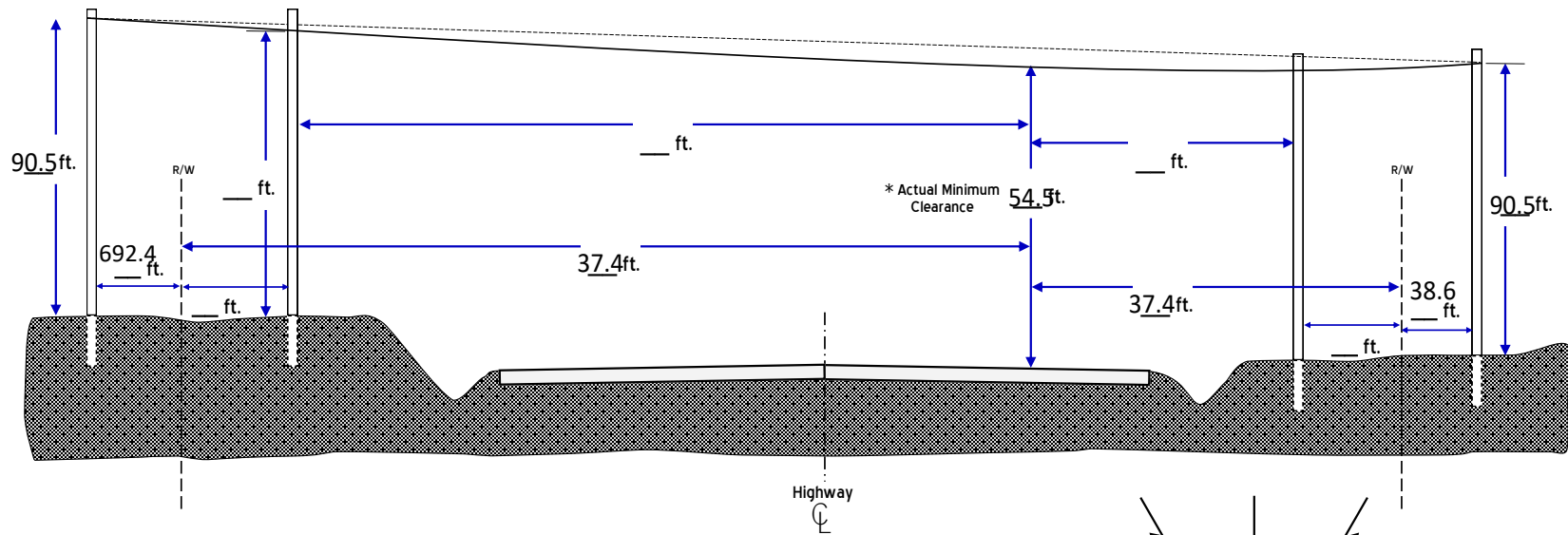
KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 DIVISION OF MAINTENANCE - PERMITS BRANCH

TC 99-212  
 Rev. 10/2018  
 Page 1 of 1

**OVERHEAD UTILITY ENCROACHMENT DIAGRAM FOR NON-FULLY CONTROLLED HIGHWAYS**

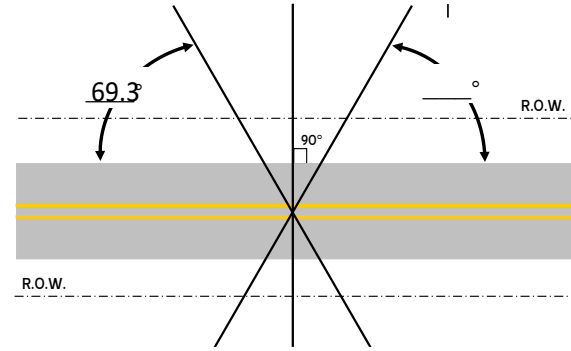
County: HARDIN Minimum Clearance to Roadway: 54.5'  
 Highway Route: KY-222 Type of Utility Crossing: TRANSMISSION  
 Mile Point: 4.7 Voltage of Line: 345kV

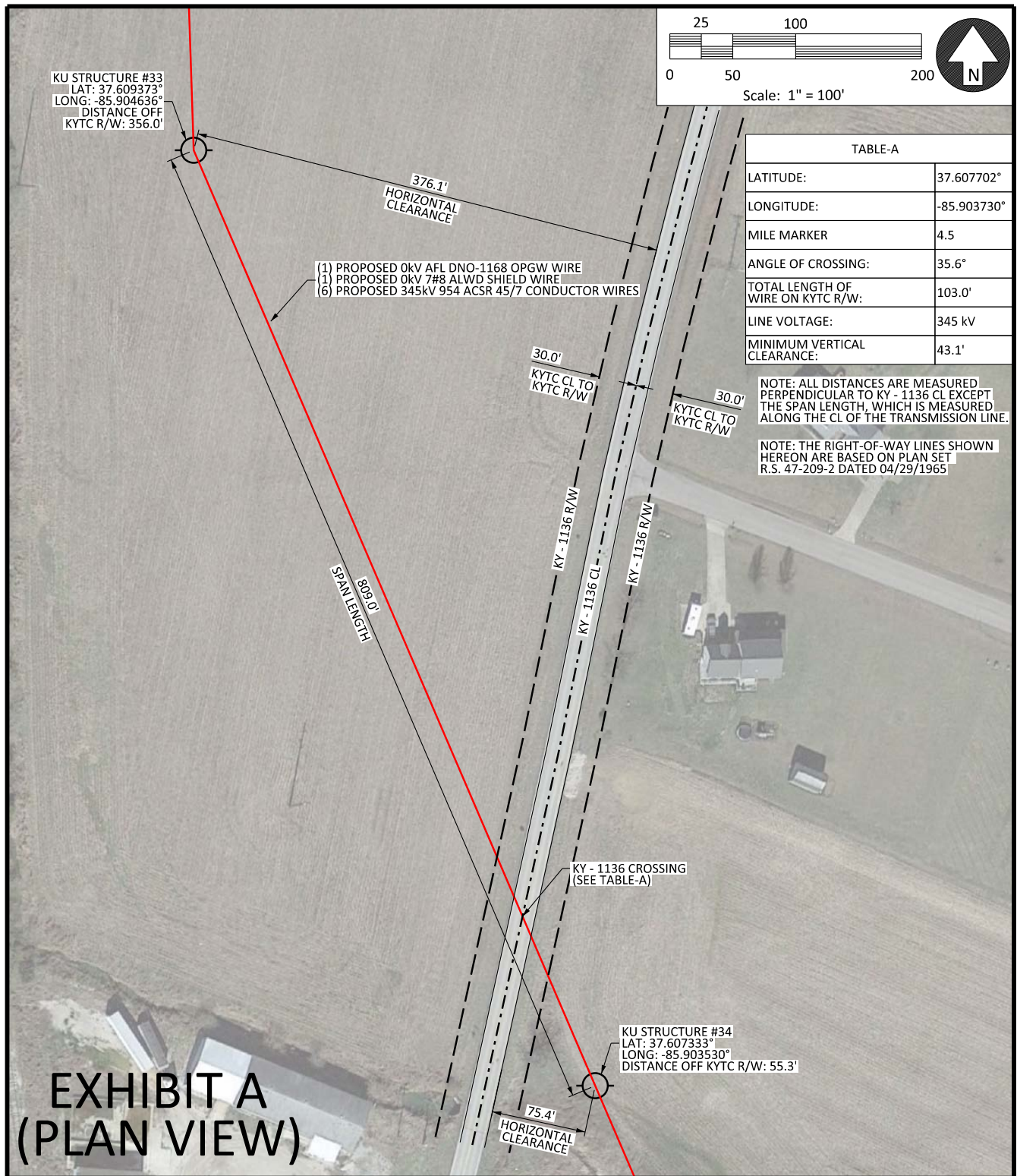
KYTC KEPT #:



**GENERAL NOTES - For Non-Fully Controlled Access Highways:**

- The vertical clearance of overhead utility lines crossing shall be a minimum of 18 feet or greater per NESC guidelines or other applicable codes.





KU STRUCTURE #33  
 LAT: 37.609373°  
 LONG: -85.904636°  
 DISTANCE OFF  
 KYTC R/W: 356.0'

- (1) PROPOSED 0kV AFL DNO-1168 OPGW WIRE
- (1) PROPOSED 0kV 7#8 ALWD SHIELD WIRE
- (6) PROPOSED 345kV 954 ACSR 45/7 CONDUCTOR WIRES

TABLE-A	
LATITUDE:	37.607702°
LONGITUDE:	-85.903730°
MILE MARKER	4.5
ANGLE OF CROSSING:	35.6°
TOTAL LENGTH OF WIRE ON KYTC R/W:	103.0'
LINE VOLTAGE:	345 kV
MINIMUM VERTICAL CLEARANCE:	43.1'

NOTE: ALL DISTANCES ARE MEASURED PERPENDICULAR TO KY - 1136 CL EXCEPT THE SPAN LENGTH, WHICH IS MEASURED ALONG THE CL OF THE TRANSMISSION LINE.

NOTE: THE RIGHT-OF-WAY LINES SHOWN HEREON ARE BASED ON PLAN SET R.S. 47-209-2 DATED 04/29/1965


# EXHIBIT A (PLAN VIEW)



P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097

PLAN VIEW  
 KENTUCKY UTILITIES COMPANY  
 KY - 1136  
 HARDIN COUNTY, KENTUCKY

DATE: 02/14/2022  
 SCALE: 1" = 100'  
 DRAWN BY: HSM  
 APPROVED BY: HSM  
 FILENAME: 21568 HWY



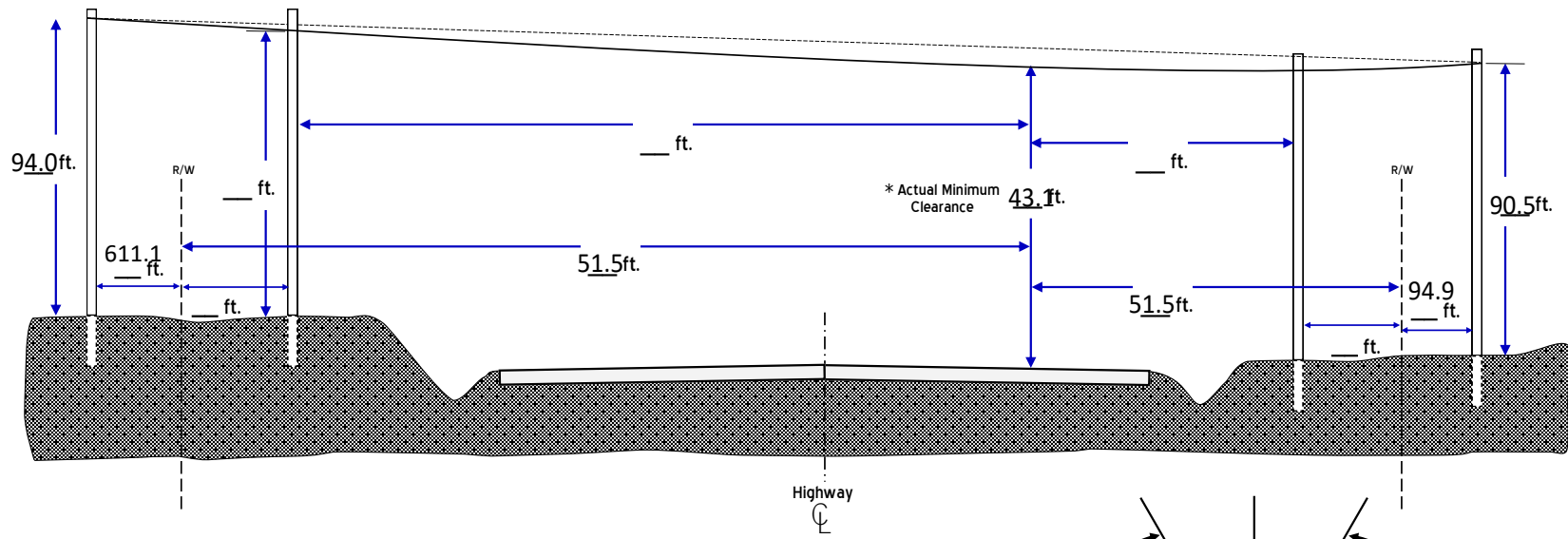
KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 DIVISION OF MAINTENANCE - PERMITS BRANCH

TC 99-212  
 Rev. 10/2018  
 Page 1 of 1

**OVERHEAD UTILITY ENCROACHMENT DIAGRAM FOR NON-FULLY CONTROLLED HIGHWAYS**

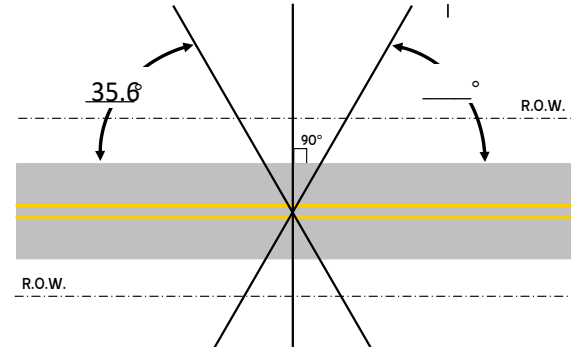
County: HARDIN Minimum Clearance to Roadway: 43.1'  
 Highway Route: KY-1136 Type of Utility Crossing: TRANSMISSION  
 Mile Point: 4.5 Voltage of Line: 345kV

KYTC KEPT #:



**GENERAL NOTES - For Non-Fully Controlled Access Highways:**

- The vertical clearance of overhead utility lines crossing shall be a minimum of 18 feet or greater per NESC guidelines or other applicable codes.



# EXHIBIT A (PLAN VIEW)

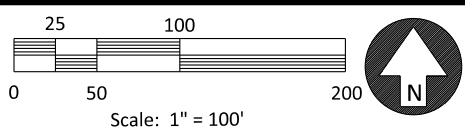
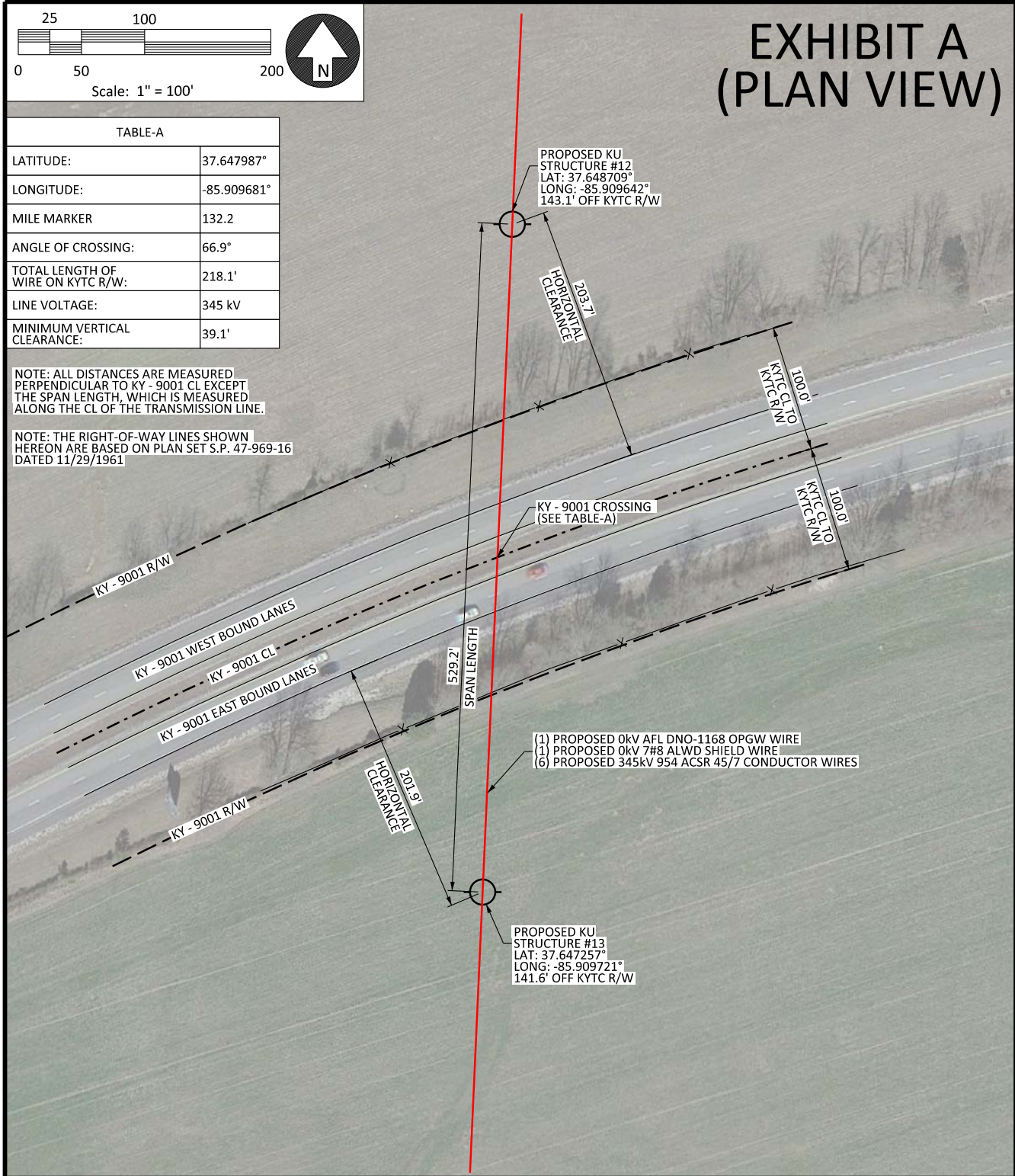


TABLE-A	
LATITUDE:	37.647987°
LONGITUDE:	-85.909681°
MILE MARKER	132.2
ANGLE OF CROSSING:	66.9°
TOTAL LENGTH OF WIRE ON KYTC R/W:	218.1'
LINE VOLTAGE:	345 kV
MINIMUM VERTICAL CLEARANCE:	39.1'

NOTE: ALL DISTANCES ARE MEASURED PERPENDICULAR TO KY - 9001 CL EXCEPT THE SPAN LENGTH, WHICH IS MEASURED ALONG THE CL OF THE TRANSMISSION LINE.

NOTE: THE RIGHT-OF-WAY LINES SHOWN HEREON ARE BASED ON PLAN SET S.P. 47-969-16 DATED 11/29/1961



**MAGE**  
 ENGINEERING SERVICES, INC.

P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097

PLAN VIEW

KENTUCKY UTILITIES COMPANY

KY - 9001 (WESTERN KENTUCKY PARKWAY)

HARDIN COUNTY, KENTUCKY

DATE: 02/14/2022

SCALE: 1" = 100'

DRAWN BY: HSM

APPROVED BY: HSM

FILENAME: 21568 HWY

# EXHIBIT B (PROFILE VIEW)

THE DISTANCES FROM KYTC CL TO KU STRUCTURES ARE MEASURED AT A RIGHT ANGLE TO THE CL AND ARE DENOTED BY A \*;  
 ALL OTHER HORIZONTAL DISTANCES ARE MEASURED ALONG THE CL OF THE PROPOSED TRANSMISSION LINE.

SCALE:  
 HORIZONTAL: 1" = 50'  
 VERTICAL: 1" = 10'

- (6) PROPOSED 345KV 954 ACSR 45/7 CONDUCTOR WIRES
- (1) PROPOSED 0kV AFL DNO-1168 OPGW WIRE
- (1) PROPOSED 0kV 7#8 ALWD SHIELD WIRE

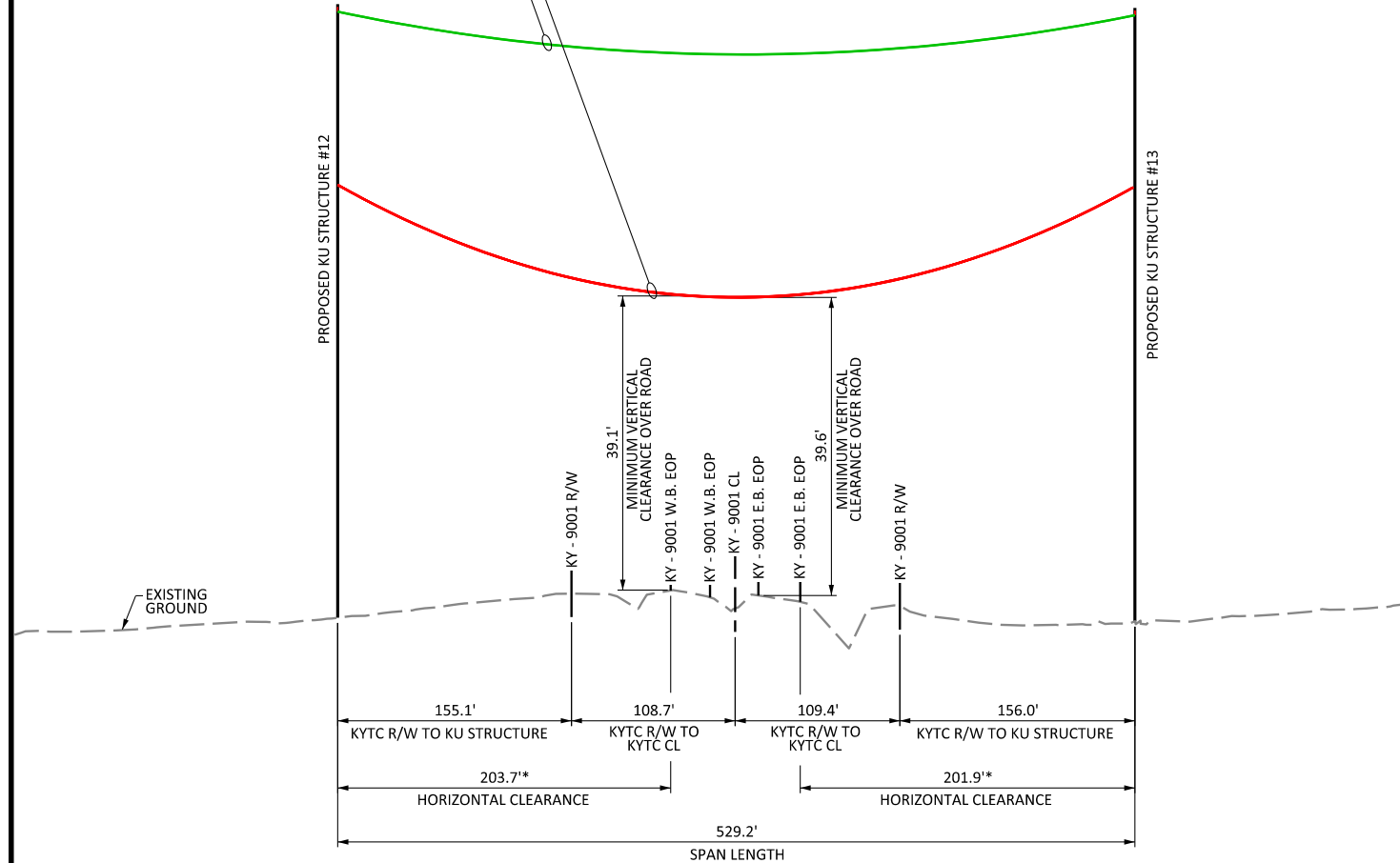


TABLE-A	
LATITUDE:	37.647987°
LONGITUDE:	-85.909681°
MILE MARKER	132.2
ANGLE OF CROSSING:	66.9°
TOTAL LENGTH OF WIRE ON KYTC R/W:	218.1'
LINE VOLTAGE:	345 kV
MINIMUM VERTICAL CLEARANCE:	39.1'

DATE: 02/14/2022  
 SCALE: 1" = AS SHOWN  
 DRAWN BY: HSM  
 APPROVED BY: HSM  
 FILENAME: 21568 HWY

PLAN VIEW  
 KENTUCKY UTILITIES COMPANY  
 KY - 9001 (WESTERN KENTUCKY PARKWAY)  
 HARDIN COUNTY, KENTUCKY

P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097





KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 DIVISION OF MAINTENANCE - PERMITS BRANCH

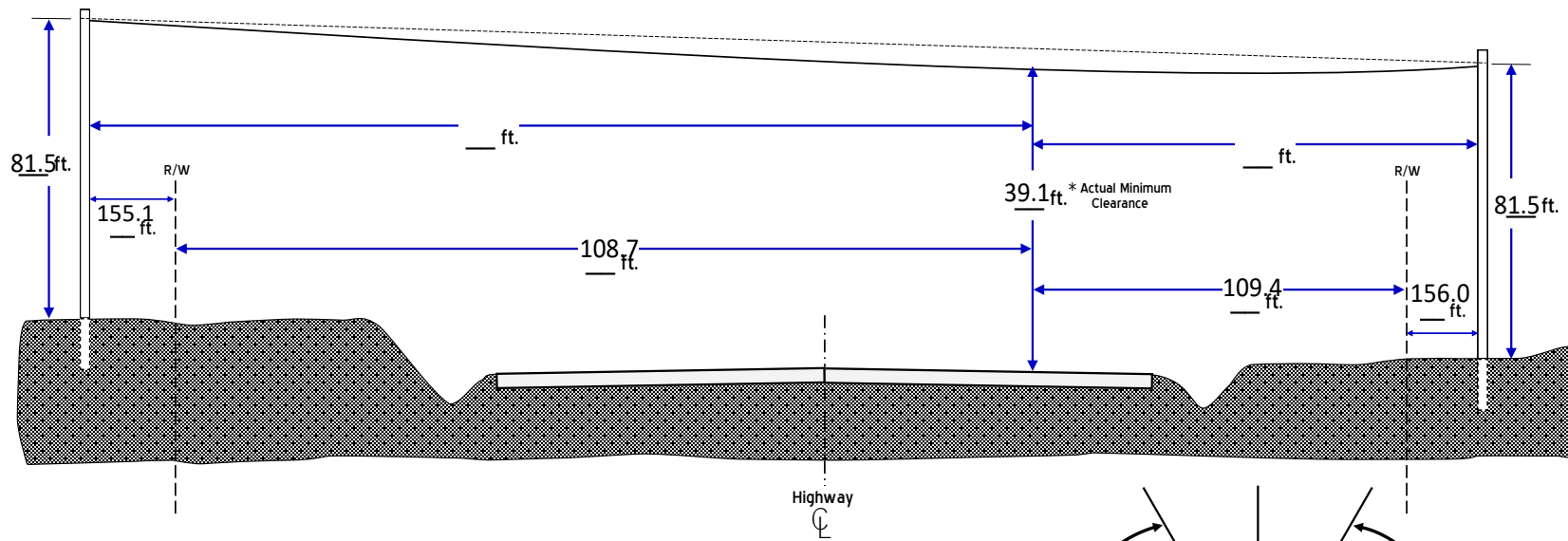
TC 99-211  
 Rev. 10/2018  
 Page 1 of 1

**OVERHEAD UTILITY ENCROACHMENT DIAGRAM FOR FULLY CONTROLLED HIGHWAYS**

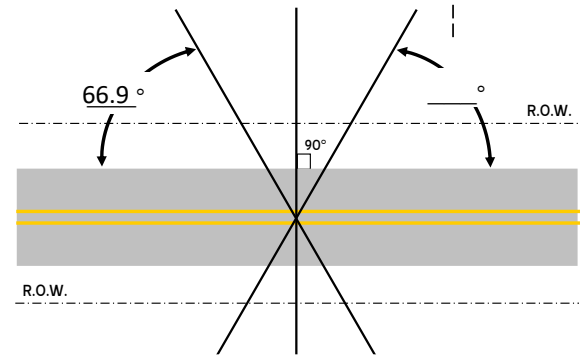
County: HARDIN  
 Highway Route: KY-9001  
 Mile Point: 132.2

Minimum Clearance to Roadway: 39.1'  
 Type of Utility Crossing: TRANSMISSION  
 Voltage of Line: 345 kV

<b>KYTC KEPT #:</b>



- GENERAL NOTES - For Fully Controlled Access Highways:**
- The vertical clearance of overhead utility lines crossing shall be a minimum of **24 feet** or greater per NESC guidelines or other applicable codes.
  - Must include Licensed Professional Engineer certification that crossing meets NESC Grade B Heavy Loading construction standards.



**KY - 9001 WORK ON THE SHOULDER**

DRAWING NOT TO SCALE

TRAFFIC CONTROL SUPERVISOR

HUNTER MCKINNEY

PRINTED NAME

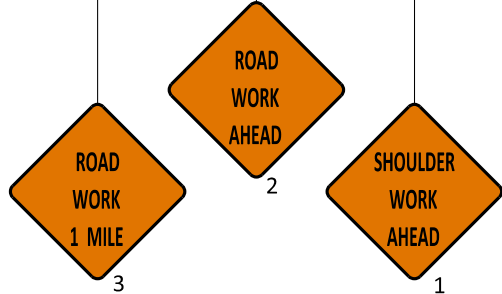
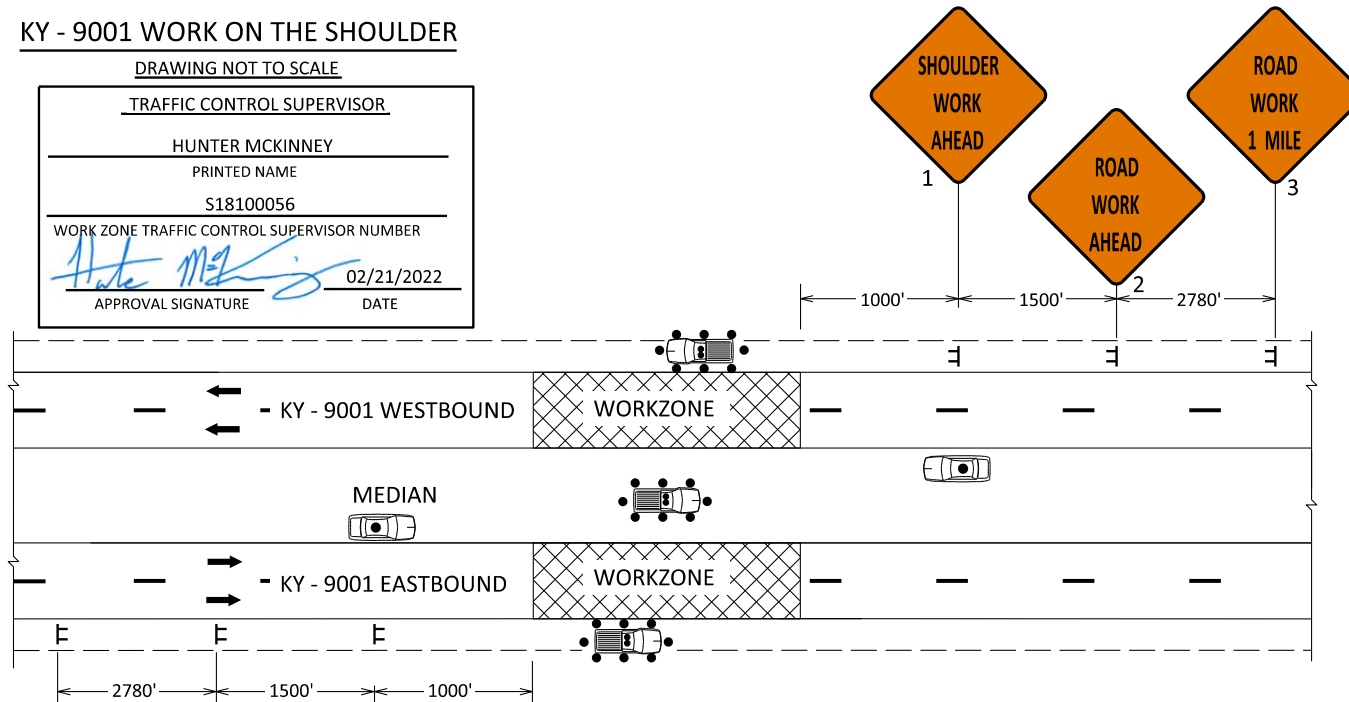
S18100056

WORK ZONE TRAFFIC CONTROL SUPERVISOR NUMBER

*Hunter McKinney* 02/21/2022

APPROVAL SIGNATURE

DATE



~ NOTES ~

- 1.) THE MINIMUM SIZE OF SIGNS 1 THROUGH 3 SHALL BE 48" X 48".
- 2.) ALL TRAFFIC CONTROL SHALL BE REMOVED AT THE END OF EACH WORKING DAY.
- 3.) THERE SHALL BE LAW ENFORCEMENT VEHICLES WITH FLASHING LIGHTS AND BUCKET TRUCKS WITH EXTENDED BOOMS AND FLASHING LIGHTS STATIONED AT THE INDICATED AREAS.
- 4.) SIGNS ALONG KY - 9001 SHALL BE DOUBLE POSTED WHERE THE MEDIAN ALLOWS.
- 5.) THE TRUCKS SHALL BE PLACED AS TO ALLOW A ROW OF BARRELS BETWEEN THE TRUCKS AND THE EDGE OF THE DRIVING LANES. BARRELS SHALL BE PLACED COMPLETELY AROUND THE TRUCKS TO INCREASE VISIBILITY.
- 6.) THIS TEMPORARY TRAFFIC CONTROL PLAN SHALL BE USED IN CONJUNCTION WITH A ROLLING ROAD BLOCK. THE BUCKET TRUCKS SHALL BE PLACED IN THE INDICATED AREAS TO ADD ADDITIONAL SAFETY WHILE PULLING WIRE ACROSS KY - 9001

DATE: 02/21/2022

SCALE: NTS

DRAWN BY: HSM

APPROVED BY: HSM

FILENAME: 21568TCP

TRAFFIC CONTROL PLAN

KENUTCKY UTILITIES COMPANY

KY - 9001 (WESTERN KENTUCKY PARKWAY)

HARDIN COUNTY, KENTUCKY

P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097







KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
DIVISION OF MAINTENANCE – PERMITS BRANCH

TC 99-210  
Rev. 08/2018  
Page 1 of 6

**ROLLING ROADBLOCK  
FOR PERMITTED WORK ON FULLY CONTROLLED HIGHWAYS**

KYTC KEPT #: \_\_\_\_\_

**SECTION 1: DESCRIPTION**

Rolling roadblocks shall be utilized to temporarily slow traffic on fully controlled highways when a completed closure is not warranted or allowed (overhead utilities, signs, etc.). Rolling roadblocks should minimize delays and increase safety of the traveling public.

**SECTION 2: GUIDELINES & REQUIREMENTS**

- Applicable sections of the "*Policy and Procedures for the Safety and Mobility of Traffic through Work Zones*" must be followed.
- Traffic Control Plan (TCP) must be submitted and approved by the District prior to any work on right-of-way.
- A Public Information Plan must be submitted and approved by the District prior to any work on right-of-way.
- Traffic shall be paced at a safe speed to provide a gap in traffic and allow work activities to be performed.
- The pacing of traffic shall be controlled for each lane by pilot vehicles. Law enforcement (fully marked and roof bar blue lights flashing) driven by uniformed law enforcement personnel shall be provided in at least one lane of traffic for each direction. Law enforcement personnel must have completed the FHWA Work Zone for Law Enforcement Training in accordance to 23 CFR Part 630 Subpart K. Additional lanes of traffic can be controlled by contractor equipment with appropriate flashing lights.
- Once notified by the traffic control supervisor to begin the rolling roadblock operation, the safety vehicle at the indicated ramp shall turn their flashing lights on, and position the vehicle across the ramp lane(s) to close ramp access. Once the rolling roadblock operation passes the closed on-ramp, the safety vehicle on the ramp shall turn off their flashing lights, and move from the ramp lane(s) to allow traffic to enter the mainline rolling roadblock operation.
- Communicating devices must be utilized to provide constant communication between the location of work and contractor vehicles.
- Advanced signing warning motorists of the rolling roadblock shall be provided by variable message signs. Variable message signs shall be placed after the last possible entry location prior to the work area.
- Contractor shall provide additional equipment on-site in case of critical equipment malfunctions or failures.
- Truck mounted crash cushions meeting the current MUTCD shall be placed at the work area for any equipment in the driving lanes.
- Equipment located on shoulders must be protected by safety devices meeting the current MUTCD.

**The Permit Traffic Control Plan (TCP) must at a minimum include the following:**

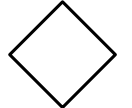

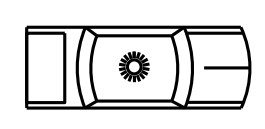
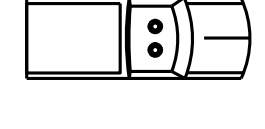


- Identification of the qualified work zone traffic control personnel
- Work schedules
- Rolling roadblock schedule
- Anticipated time of delays to traveling public
- Proposed blocking procedures must be included for mainline and on-ramps
- List of critical equipment for work activity
- Name, Signature, and Work Zone Traffic Control Supervisor Number

KYTC KEPT #: \_\_\_\_\_

GENERAL NOTES

1. ROLLING ROADBLOCK IS A TRAFFIC CONTROL TECHNIQUE TO SLOW TRAFFIC TO FACILITATE SHORT DURATION WORK OPERATIONS WITHOUT AN ELABORATE AND DIFFICULT DETOUR. TRAFFIC CONTROL LAW ENFORCEMENT OFFICERS PACE, OR SLOW, THE TRAFFIC TO A SPEED THAT PROVIDES UP TO 15 MINUTES OF WORK TIME.
2. WHEN MORE THAN ONE ROLLING ROADBLOCK OPERATION IS REQUIRED IN ONE WORK PERIOD, THE CONTRACTOR SHALL ALLOW SUFFICIENT TIME BETWEEN ROLLING ROADBLOCK OPERATIONS TO PERMIT TRAFFIC TO RETURN TO NORMAL SPEEDS AND FLOW.
3. "POLICY AND PROCEDURES FOR THE SAFETY AND MOBILITY OF TRAFFIC THROUGH WORK ZONES FOR PERMIT PROJECTS" MUST BE FOLLOWED FOR ALL WORK. <http://transportation.ky.gov/Permits>
4. TEMPORARY TRAFFIC CONTROL PLAN (TTCP) MUST BE SUBMITTED AND APPROVED BY THE KYTC PRIOR TO ANY WORK ON STATE R/W.
5. LAW ENFORCEMENT PERSONNEL MUST HAVE COMPLETED THE FHWA "WORK ZONE FOR LAW ENFORCEMENT TRAINING" IN ACCORDANCE TO 23 CFR PART 630 SUBPART K.
6. COMMUNICATING DEVICES MUST BE UTILIZED TO PROVIDE CONSTANT COMMUNICATION BETWEEN THE WORK LOCATION, CONTRACTOR VEHICLES AND LAW ENFORCEMENT VEHICLES.
7. CONTRACTOR SHALL PROVIDE ADDITIONAL EQUIPMENT ON-SITE IN CASE OF CRITICAL EQUIPMENT MALFUNCTIONS OR FAILURES.
8. EQUIPMENT LOCATED ON SHOULDERS MUST BE PROTECTED BY CHANNELIZING DEVICES MEETING THE CURRENT MUTCD.
9. TRAFFIC CONTROL DEVICES SHALL BE PLACED AT A TIME AS CLOSE AS PRACTICAL TO THE BEGINNING OF THE ROLLING ROADBLOCK.
10. AFTER ROLLING ROADBLOCK OPERATIONS ARE COMPLETE, THE TRAFFIC CONTROL DEVICES THAT ARE NOT BEING UTILIZED FOR OTHER OPERATIONS SHALL BE IMMEDIATELY REMOVED.
11. ROLLING ROADBLOCKS REQUIRING THE CONTROL OF ON-RAMPS FROM MAJOR CROSSROADS WILL REQUIRE A MORE SPECIFIC/ DETAILED TTCP AND DETAILED DIAGRAMS.
12. A PAYMENT BOND SHALL BE RECEIVED BY THE DEPARTMENT IN AN AMOUNT SUFFICIENT TO COVER LIQUIDATED DAMAGES THAT MAY BE APPLIED IF THE ROADWAY IS CLOSED FOR MORE THAN THE TIME ALLOTTED FOR THE ROLLING ROADBLOCK.
13. LIQUIDATED DAMAGES MAY ALSO BE ASSESSED IF THE APPROVED TEMPORARY TRAFFIC CONTROL PLAN IS NOT FOLLOWED.
14. CONTRACTOR MUST NOTIFY AND RECEIVE APPROVAL FROM THE KYTC 7 DAYS IN ADVANCE OF THE ROLLING ROADBLOCK.


GENERAL SYMBOLS

-  STATIC SIGN (48" X 48")
-  PORTABLE VARIABLE MESSAGE SIGN (VMS)
-  LAW ENFORCEMENT VEHICLE WITH FLASHING BLUE LIGHTS
-  CONTRACTOR VEHICLE WITH FLASHING LIGHTS
-  DIRECTION OF TRAVEL
-  WORK AREA

**KENTUCKY TRANSPORTATION CABINET**  
Department of Highways  
**DIVISION OF MAINTENANCE - PERMITS BRANCH**

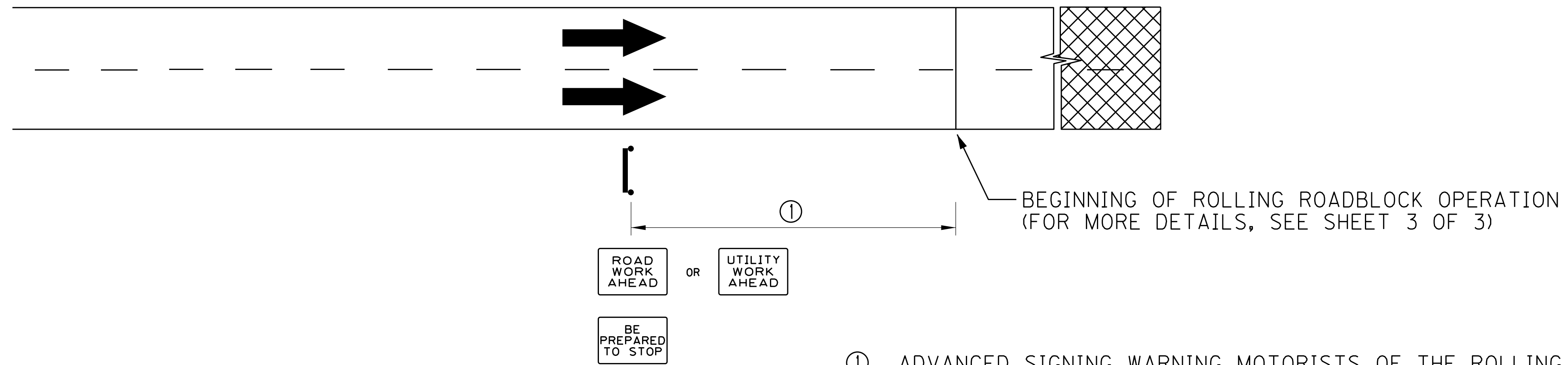
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**ROLLING ROADBLOCK**  
**For Permitted Work on Fully Controlled Highways**

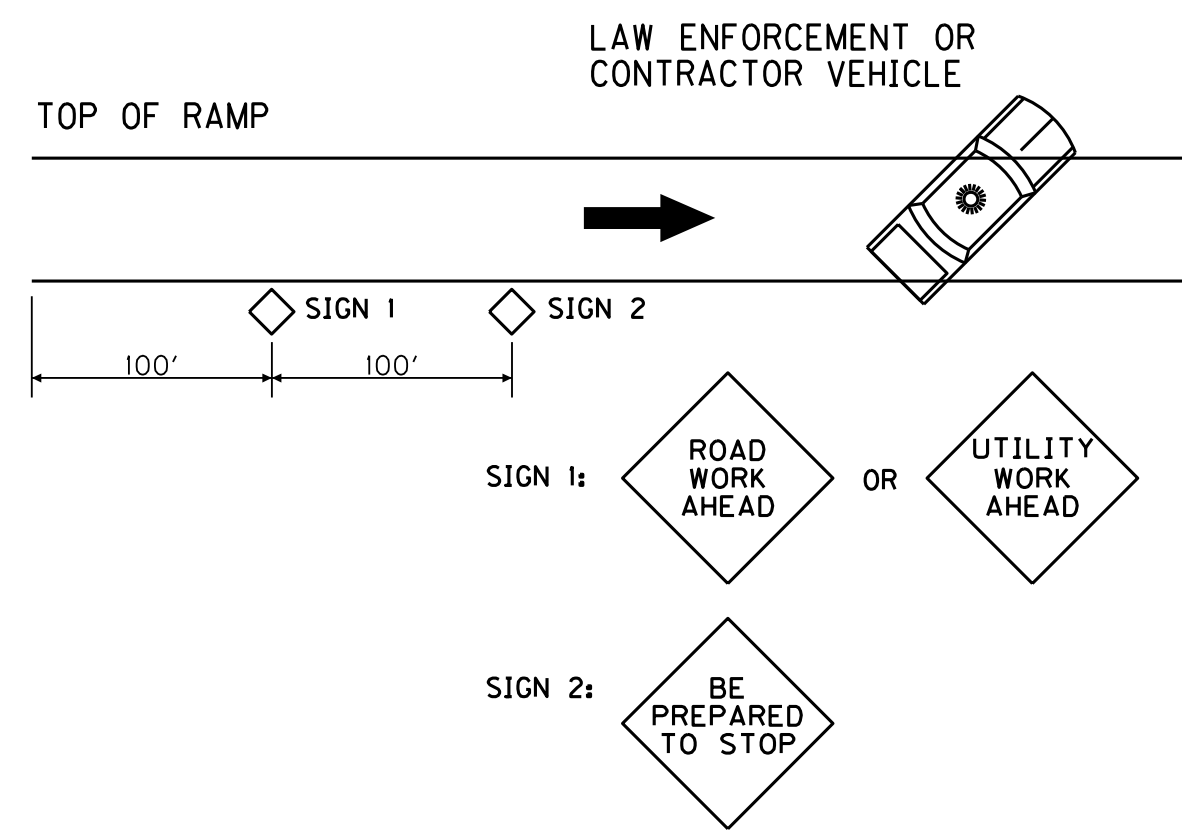


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TYPICAL PLACEMENT OF PCMS



① ADVANCED SIGNING WARNING MOTORISTS OF THE ROLLING ROADBLOCK SHALL BE PROVIDED BY PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS). PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AFTER THE LAST POSSIBLE ENTRY LOCATION PRIOR TO THE WORK AREA BUT NO MORE THAN ONE (1) MILE PRIOR TO THE BEGINNING OF THE ROLLING ROADBLOCK.



TYPICAL RAMP CLOSURE DETAIL

RAMP CLOSURE NOTES

TWO (2) STATIC SIGNS SHALL BE USED ON EACH CONTROLLED ON-RAMP. SIGN ONE (1) SHALL BE "ROAD WORK AHEAD" OR "UTILITY WORK AHEAD" SIGN AND SIGN TWO (2) SHALL BE "BE PREPARED TO STOP".

ONCE NOTIFIED BY THE TRAFFIC CONTROL SUPERVISOR TO BEGIN THE ROLLING ROADBLOCK OPERATION, THE SAFETY VEHICLE AT THE INDICATED RAMP SHALL TURN THEIR FLASHING LIGHTS ON, AND POSITION THE VEHICLE ACROSS THE RAMP LANE(S) TO CLOSE RAMP ACCESS.

ONCE THE ROLLING ROADBLOCK OPERATION PASSES THE CLOSED ON-RAMP, THE SAFETY VEHICLE ON THE RAMP SHALL TURN OFF THEIR FLASHING LIGHTS, AND MOVE FROM THE RAMP LANE(S) TO ALLOW TRAFFIC TO ENTER THE MAINLINE ROLLING ROADBLOCK OPERATION.

SITE SPECIFIC CONDITIONS MAY REQUIRE ADDITIONAL CONTROL DEVICES OR VEHICLES.

KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
**DIVISION OF MAINTENANCE - PERMITS BRANCH**

**ROLLING ROADBLOCK**  
 For Permitted Work on Fully Controlled Highways

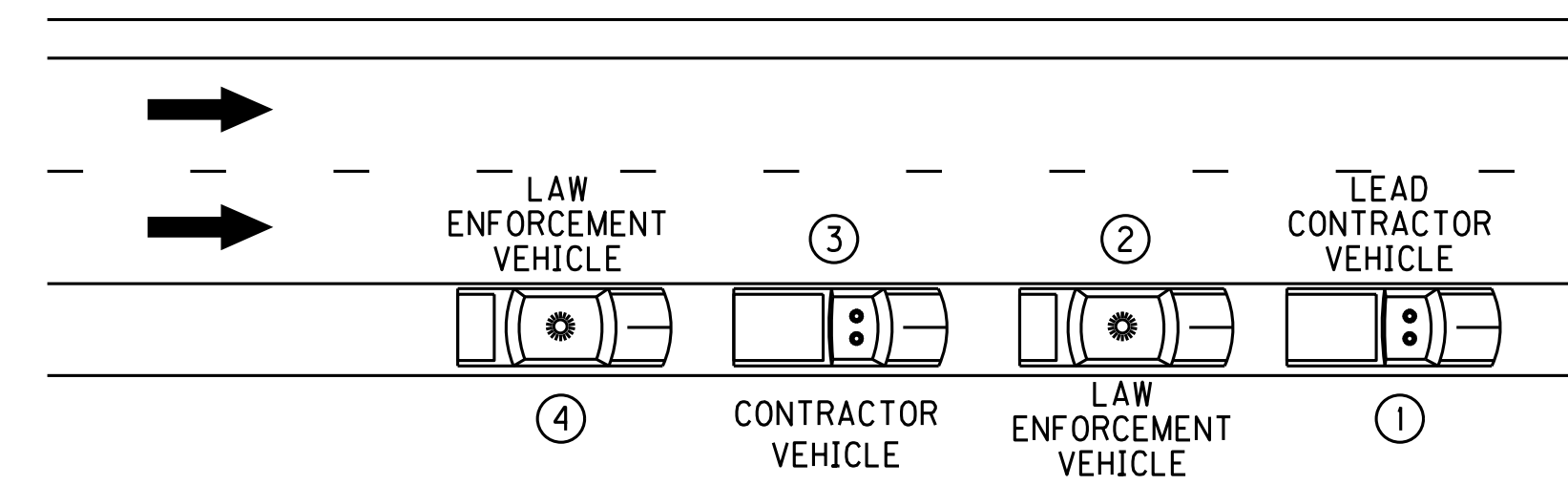


KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
DIVISION OF MAINTENANCE - PERMITS BRANCH

ROLLING ROADBLOCK  
For Permitted Work on Fully Controlled Highways



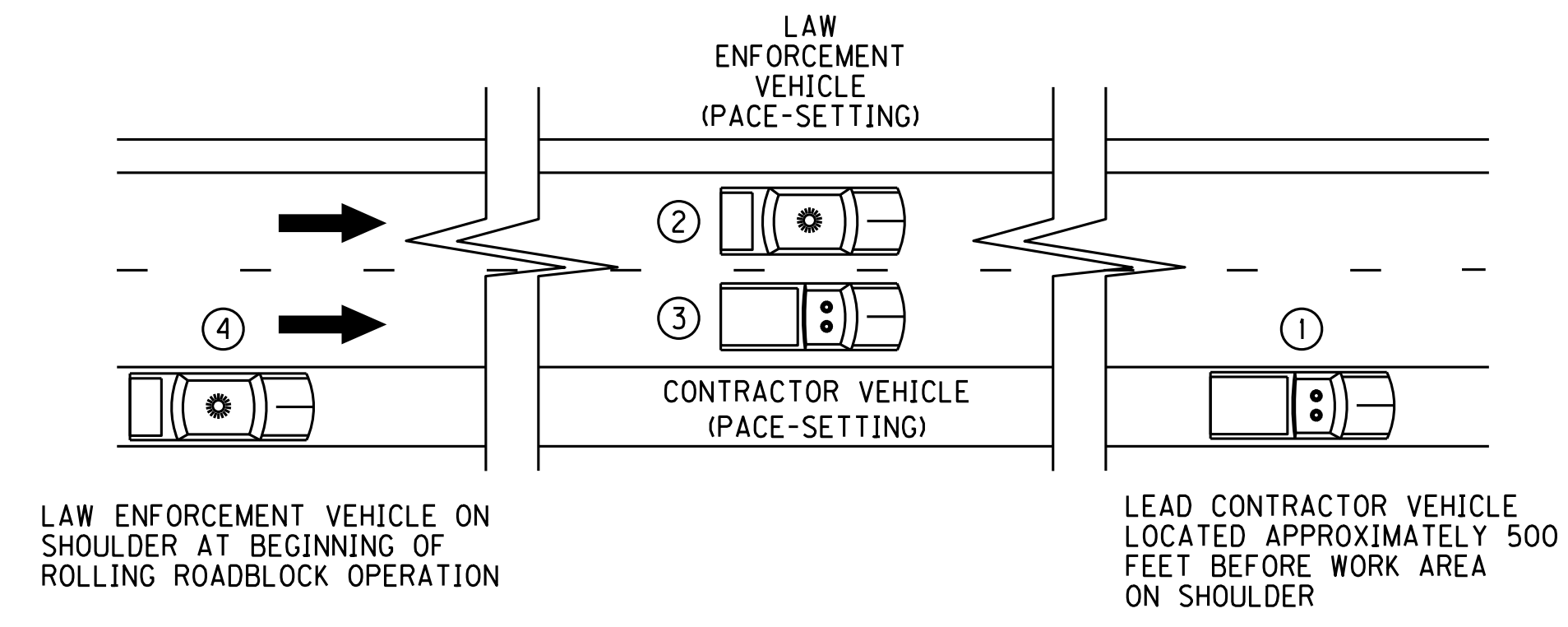
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STAGE 1

STAGE 1 NOTES:

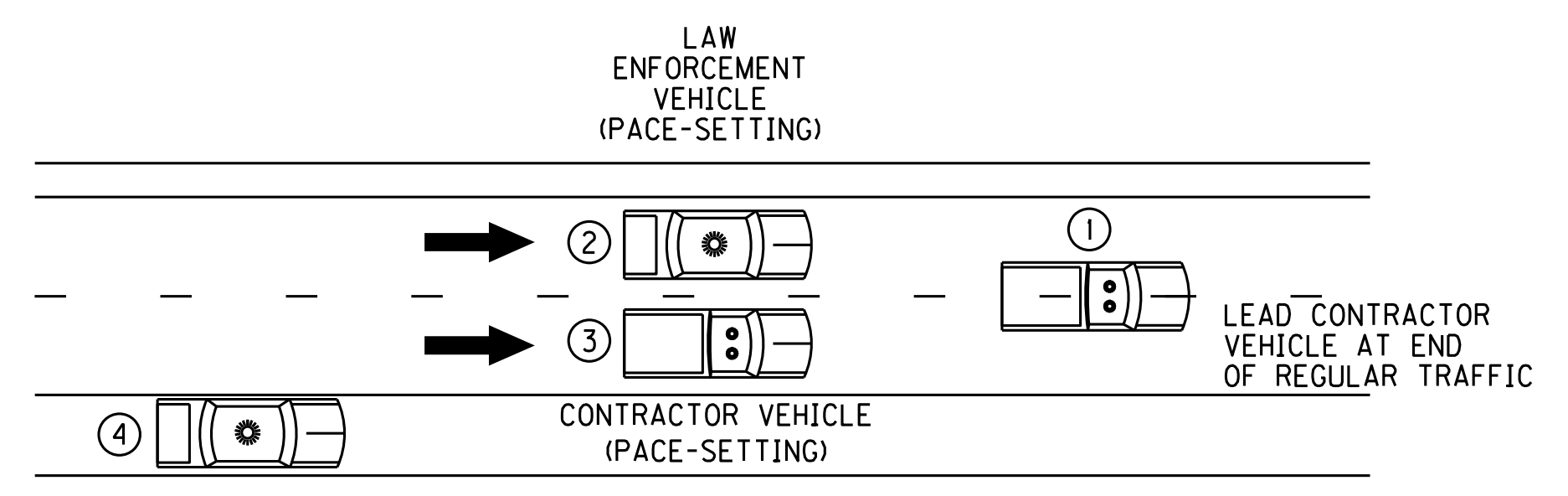
1. MINIMUM OF FOUR SAFETY VEHICLES LOCATED UPSTREAM OF THE ROLLING ROADBLOCK OPERATION WITH FLASHING LIGHTS OFF. A MINIMUM OF TWO (2) LAW ENFORCEMENT VEHICLES ARE REQUIRED. THE LOCATION OF STAGE 1 SHALL BE AT A LOCATION THAT GIVES A SUFFICIENT DISTANCE TO SLOW VEHICLES PRIOR TO THE BEGINNING MILEPOINT OF THE ROLLING ROADBLOCK.
2. THERE SHALL BE A PACE-SETTING VEHICLE IN EACH TRAVEL LANE.



STAGE 3

STAGE 3 NOTES:

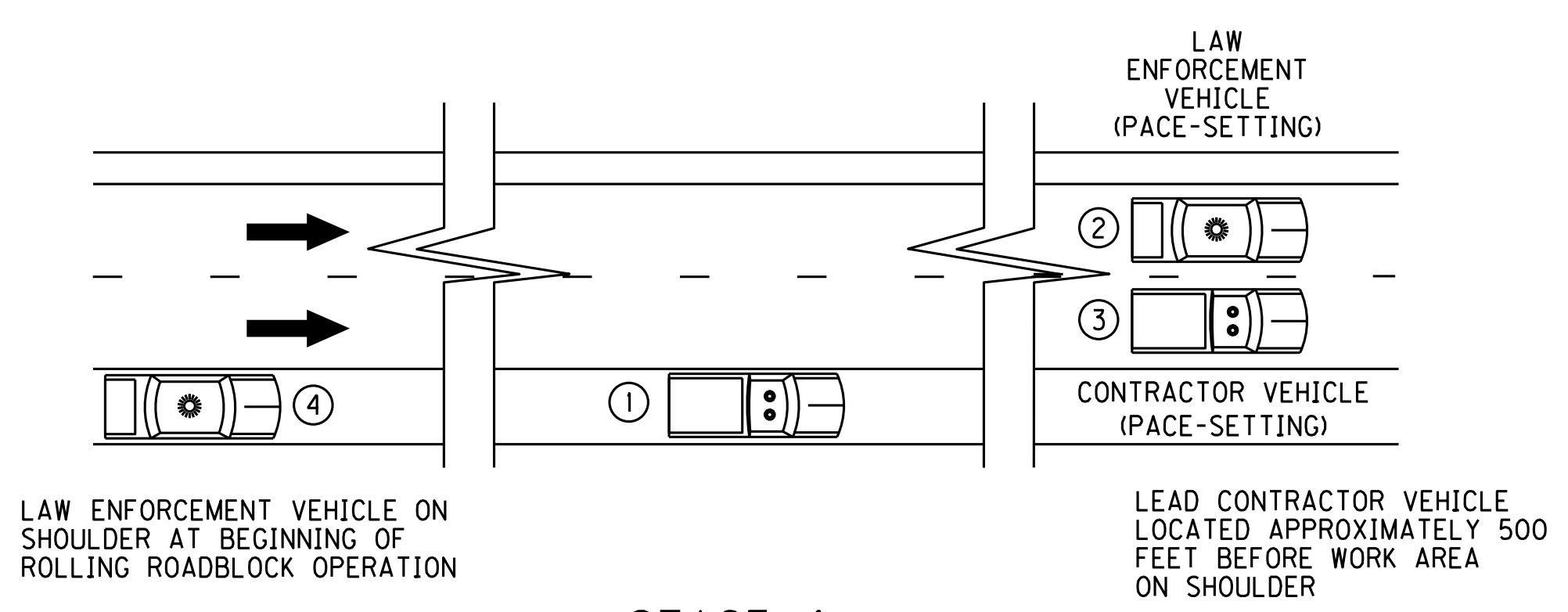
1. THE TWO (2) PACE-SETTING SAFETY VEHICLES NUMBERS TWO (2) AND THREE (3) SHALL BEGIN TO SLOW TO THE PACING SPEED (10 MPH MINIMUM), FOR THE DURATION OF THE ROLLING ROADBLOCK OPERATION.
2. THE LEAD CONTRACTOR VEHICLE NUMBER ONE (1) (FLASHING LIGHTS OFF) SHALL MATCH THE SPEED OF THE LAST VEHICLES AHEAD OF THE PACE-SETTING SAFETY VEHICLE NUMBERS TWO (2) AND THREE (3), AND CONTINUE FOLLOWING TRAFFIC UNTIL A POINT IN ADVANCE OF THE WORK AREA. THE LEAD CONTRACTOR VEHICLE NUMBER ONE (1) SHALL THEN COME TO A COMPLETE STOP ON THE RIGHT SHOULDER, AND THEN TURN ON ITS' FLASHING LIGHTS.



STAGE 2

STAGE 2 NOTE:

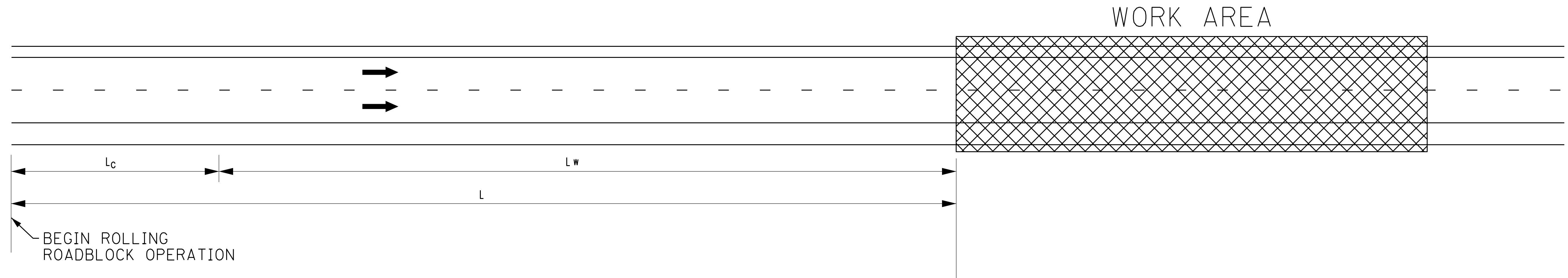
ONCE ALL SAFETY VEHICLES ARE IN PLACE AND THE SUPERVISOR AT THE WORK AREA NOTIFIES ALL SAFETY VEHICLES TO BEGIN THE ROLLING ROADBLOCK OPERATION, ALL SAFETY VEHICLES NUMBERS ONE (1), TWO (2), THREE (3) AND FOUR (4) SHALL TURN ON THEIR FLASHING LIGHTS. VEHICLE NUMBERS ONE (1), TWO (2) AND THREE (3) SHALL ENTER THE TRAVEL LANES, WITH VEHICLE NUMBERS TWO (2) AND THREE (3) IMMEDIATELY FORMING A SIDE-BY-SIDE "PACING OPERATION" OF ALL LANES BEHIND THE LEAD CONTRACTOR VEHICLE NUMBER ONE (1). ONCE VEHICLE NUMBER ONE (1) HAS ENTERED THE TRAVEL LANE FLASHING LIGHTS SHALL BE TURNED OFF.



STAGE 4

STAGE 4 NOTES:

1. THE CONTRACTOR'S ON-SITE SUPERINTENDENT SHALL REMAIN IN CONSTANT CONTACT WITH THE PACE-SETTING SAFETY VEHICLES' NUMBERS TWO (2) AND THREE (3) LOCATION, THE CONTRACTOR SHALL HAVE THE TRAVEL LANES CLEARED, PRIOR TO SAFETY VEHICLES ARRIVAL, OF ALL EQUIPMENT AND DEBRIS IN ORDER TO REOPEN ALL TRAVEL LANES.
2. THE TWO (2) PACE-SETTING SAFETY VEHICLES NUMBERS TWO (2) AND THREE (3) SHALL CLEAR THE WORK AREA AND IMMEDIATELY MOVE TO THE RIGHT SHOULDER OR AN AREA DESIGNATED BY THE TRAFFIC CONTROL SUPERVISOR, AND TURN OFF THE FLASHING LIGHTS. ONCE THE TWO PACE-SETTING SAFETY VEHICLES NUMBERS TWO (2) AND THREE (3) PASS THE WORK AREA, THE TRAFFIC CONTROL SUPERVISOR SHALL INSTRUCT THE LEAD CONTRACTOR VEHICLE NUMBER ONE (1) AND THE LAW ENFORCEMENT VEHICLE NUMBER FOUR(4) TO TURN OFF THEIR FLASHING LIGHTS.



PACING DISTANCES, L (MILES)

Sp = 10 MPH			
SR	T <sub>w</sub> (MIN.) 5	T <sub>w</sub> (MIN.) 10	T <sub>w</sub> (MIN.) 15
70	L= 0.97	1.94	2.92
65	L= 0.98	1.97	2.95
60	L= 1.00	2.00	3.00
55	L= 1.02	2.04	3.06
50	L= 1.04	2.08	3.13

Sp = 15 MPH			
SR	T <sub>w</sub> (MIN.) 5	T <sub>w</sub> (MIN.) 10	T <sub>w</sub> (MIN.) 15
70	L= 1.59	3.18	4.77
65	L= 1.63	3.25	4.88
60	L= 1.67	3.33	5.00
55	L= 1.72	3.44	5.16
50	L= 1.79	3.57	5.36

Sp = 20 MPH			
SR	T <sub>w</sub> (MIN.) 5	T <sub>w</sub> (MIN.) 10	T <sub>w</sub> (MIN.) 15
70	L= 2.33	4.67	7.00
65	L= 2.44	4.81	7.22
60	L= 2.50	5.00	7.50
55	L= 2.62	5.24	7.86
50	L= 2.78	5.56	8.33

S<sub>R</sub> = REGULATORY SPEED, MPH

S<sub>P</sub> = PACING SPEED, MPH

T<sub>W</sub> = WORK DURATION, MINUTES

L = TOTAL PACING DISTANCE, MILES

$$= \frac{T_w}{60} S_p \left( \frac{S_p}{S_R - S_p} + 1 \right)$$

$$= L_c + L_w$$

L<sub>C</sub> DISTANCE PACE-SETTING LAW ENFORCEMENT VEHICLES SHALL TRAVEL BEFORE THE VEHICLES AT REGULATORY SPEED HAVE CLEARED THE WORK AREA

$$= \frac{\left( \frac{T_w}{60} \times S_p^2 \right)}{S_R - S_p}$$

L<sub>W</sub> DISTANCE PACE-SETTING LAW ENFORCEMENT VEHICLES TRAVEL WHILE CONSTRUCTION WORK IS PERFORMED

$$= \left( \frac{T_w}{60} \times S_p \right)$$

KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
**DIVISION OF MAINTENANCE - PERMITS BRANCH**

**ROLLING ROADBLOCK**  
**For Permitted Work on Fully Controlled Highways**



KYTC KEPT #: \_\_\_\_\_

SITE SPECIFIC ROLLING ROADBLOCK INFORMATION

APPLICANT COMPANY: Kentucky Utilities Company  
 CONTACT NAME: Brandon Crook  
 CONTACT PHONE#: (270) 841-7590

COUNTY: Hardin  
 ROUTE#: KY - 9001 (Western Kentucky Parkway)  
 MILE POINT OF WORK AREA: 132.2

NUMBER OF ROLLING ROADBLOCKS NEEDED (PER DIRECTION):  
 EASTBOUND/SOUTHBOUND 5 WESTBOUND/NORTHBOUND 5  
 WORK TIME REQUIRED FOR EACH ROLLING ROADBLOCK (T<sub>R</sub>): 15 MINUTES  
 PACING SPEED OF ROLLING ROADBLOCK (S<sub>P</sub>): 10 MPH  
 WORK DAY OF THE WEEK: Sunday  
 WORK STARTING TIME: 7:00 AM/PM

TRAFFIC CONTROL SUPERVISOR NAME: Hunter McKinney  
 CERTIFICATION#: S18100056  
 24 HR PHONE#: (606) 669-9071  
 LAW ENFORCEMENT CONTACT NAME: Scotty Sharp  
 COMPANY/DEPARTMENT NAME: Kentucky State Police Post 4  
 PHONE #: (270) 766-5078 CELL #: \_\_\_\_\_

MAINLINE ROLLING ROADBLOCK  
 BEGIN MP EASTBOUND/SOUTHBOUND: 129.3  
 PCMS POSITIONED AT MP EASTBOUND/SOUTHBOUND: 128.3  
 BEGIN MP WESTBOUND/NORTHBOUND: 135.1  
 PCMS POSITIONED AT MP WESTBOUND/NORTHBOUND: 136.1  
 LIST OF ALL ON-RAMPS AND MP EACH:  
Exit 133 westbound on ramp

LIST OF CRITICAL EQUIPMENT REQUIRED TO COMPLETE WORK:  
Digger Derrick, Bucket Truck, Pickup Truck, Traffic Control Vehicles

**APPLICANT TRAFFIC CONTROL SUPERVISOR:**

Hunter McKinney  
 Printed Name

S18100056  
 Work Zone Traffic Control Supervisor #:

Hunter McKinney  
 Approval Signature

08/21/2022  
 Date

**KYTC TRAFFIC CONTROL SUPERVISOR:**

\_\_\_\_\_  
 Printed Name

\_\_\_\_\_  
 Work Zone Traffic Control Supervisor #:

\_\_\_\_\_  
 Approval Signature

\_\_\_\_\_  
 Date

KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
**DIVISION OF MAINTENANCE - PERMITS BRANCH**  
**ROLLING ROADBLOCK**  
 For Permitted Work on Fully Controlled Highways



TC 99-210  
 Rev. 08/2018  
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3/8/22, 2:22 PM

Permits

# Kentucky Encroachment Permit Tracking

## Maintenance Division



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### PERMIT QUICK SEARCH

Search Criteria: **Permit / Tracking #**

Search Value:

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### PERMIT GENERAL INFORMATION

Permit/Tracking #: A04-2022-00059

[Follow](#)

Permit Status: Approved

Status Date: 2/28/2022

Created By: Steve Peace

Created Date: 2/28/2022

District: 04 - Elizabethtown

Filing Date: 2/28/2022

Cancellation Date: 9/28/2023

Completion Date: 8/28/2023

Permit Type: Utilities

Permit Subtype: Electric

**Kentucky Utilities : Applicant**

Address: 1 Quality St.  
Lexington, KY 40507

Phone: (859)310-3230  
Email: Michael.kern@lge-ku.com

Company

[Maintain Contacts](#)

[Application](#) [Attachments](#) [Routing](#) [Comments](#) [Locations/Inspections](#) [Indemnity](#) [Approvals](#)

### LOCATION DATA

Description:

#### MOST RECENT INSPECTION

There are no inspections for this location.

County: Hardin

Route: Multiple

BMP/EMP: Multiple

Activity: New Install

Location: Overhead

Contract Agr #:

Bridge #:

Description:

Applicant to install new 345KV transmission line which will cross KY 9001, 222, and 1136. Overhead wire shall meet KYTC minimum clearance standards and all structures shall be off KYTC ROW.

[Edit](#)

[View on Map](#)

[Edit Permit](#)

v1.1.7969.26696



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Andy Beshear  
Governor

COMMONWEALTH OF KENTUCKY  
TRANSPORTATION CABINET  
Department of Highways, District 4 Office  
634 East Dixie Highway  
Elizabethtown, Kentucky 42701  
(270) 766-5066  
[www.transportation.ky.gov/](http://www.transportation.ky.gov/)

Jim Gray  
Secretary

May 12, 2022

Kentucky Utilities  
Mike Kern  
1 Quality St.  
Lexington, Kentucky 40509

Subject: Permit #: 04-2022-00206  
Permit Type: Utilities - Electric  
Approval

Dear Applicant:

Attached is your permit approval and documentation for the subject permit.

Be advised that all work must be done in conformity with permit and application conditions. If you have any questions, please contact the Permits Section at this office.

Sincerely,

A handwritten signature in cursive script that reads "Jacob Riggs".

Jacob Riggs  
D4 Permits - Supervisor

Attachments



An Equal Opportunity Employer M/F/D





Kentucky Transportation Cabinet  
Department of Highways  
Division of Maintenance  
Permits Branch

TC 99-1 (B)  
07/2018  
Page 1 of 1

**ENCROACHMENT PERMIT**

**KYTC KEPT #:** 04-2022-00206

**Permittee:** Kentucky Utilities

**Permit Type / Subtype:** Utilities / Electric

**Work Completion Date:** 8/28/2023

INDEMNITIES		
Type	Amount Required	Tracking Number
Performance Bond	\$0.00	
Cash / Check	\$0.00	
Self-Insured	\$0.00	
Payment Bond	\$0.00	
Liability Insurance	\$0.00	

This permit has been:      **APPROVED**       **DENIED**

Jacob Riggs	D4 Permits - Supervisor	5/12/2022
<b>SIGNATURE</b>	<b>TITLE</b>	<b>DATE</b>

The TC 99-1(B), including the application TC-99 1(A) and all related and accompanying documents and drawings make up the permit. It is not a permit unless both the TC 99-1(A) and TC 99-1(B) are both present.

LOCATION(S)			
Description	County - Route	Latitude	Longitude
	Hardin - KY 3005	37.664293	-85.900141



To Submit a Locate Request  
24 Hours a Day, Seven Days a Week:  
Call 811 or 800-752-6007



KENTUCKY TRANSPORTATION CABINET  
 Department of Highways  
 PERMITS BRANCH

TC 99-1A  
 Rev. 10/2020  
 Page 1 of 4

### APPLICATION FOR ENCROACHMENT PERMIT

KYTC KEPT #: **04-2022-00206**

#### SECTION 1: APPLICANT CONTACT INFORMATION

<b>APPLICANT</b> Kentucky Utilities		<b>ADDRESS</b> 1 Quality Street		
<b>EMAIL</b> Michael.kern@lge-ku.com		<b>CITY</b> Lexington	<b>STATE</b> KY	<b>ZIP</b> 40507
<b>CONTACT NAME 1</b> Mike Kern		<b>EMAIL</b> Michael.kern@lge-ku.com		<b>PHONE #</b> 859-367-5674
				<b>CELL #</b> 859-310-3230
<b>CONTACT NAME 2 (if applicable)</b> Addam Gooch		<b>EMAIL</b> addam.gooch@lge-ku.com		<b>PHONE #</b> 859-367-5416
				<b>CELL #</b> 859-613-4682

#### SECTION 2: PROPOSED WORK LOCATION

<b>ADDRESS</b> KY-3005		<b>CITY</b> Glendale		<b>STATE</b> Kentucky	<b>ZIP</b> 42740
<b>COUNTY</b> Hardin	<b>ROUTE #</b> KY-3005	<b>MILE POINT</b> 1.1	<b>LONGITUDE (X)</b> -85.900141°	<b>LATITUDE (Y)</b> 37.664293°	

#### ADDITIONAL LOCATION INFORMATION:

#### FOR KYTC USE ONLY

**PERMIT TYPE:**  Air Right  Entrance  Utilities  Vegetation Removal  Other: \_\_\_\_\_

**ACCESS:**  Full  Partial  by Permit **LOCATION:**  Left  Right  Crossing

#### SECTION 3: GENERAL DESCRIPTION OF WORK

Installing a new 0kV AFL DNO-1168 OPGE wire on an existing transmission line. The wire heights above the road shall be at or above KYTC minimum clearance and the structures will be off of KYTC R/W. The project will require a temporary closure of KY - 3005. See "Exhibit A" for a plan view drawing of the encroachment location. Also see the traffic control plan attached.

THE UNDERSIGNED APPLICANT(s), being duly authorized representative(s) or owner(s), DO AGREE TO ALL ORIGINAL UNEDITED TERMS AND CONDITIONS ON THE TC 99-1A, pages 1-4.

*Michael J. Kern*

SIGNATURE

11 May 2022

DATE

This is not a permit unless and until the applicant(s) receives an approved TC 99-1B from KYTC. This application shall become void if not approved by the cancellation date. The cancellation date shall be a minimum of one year from the date the applicant submits their application.



KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
PERMITS BRANCH

TC 99-1A  
Rev. 10/2020  
Page 2 of 4

## APPLICATION FOR ENCROACHMENT PERMIT

### TERMS AND CONDITIONS

1. The permit, including this application and all related and accompanying documents and drawings making up the permit, remains in effect and is binding upon the Applicant/Permittee, its successors and assigns, as long as the encroachment(s) exists and also until the permittee is finally relieved by the Department of Highways from all its obligations.
2. Applicant shall meet all requirements of the Clean Water Act if the project will disturb one acre or more, the applicant shall obtain a KPDES KYR10 Permit from the Kentucky Division of Water. All disturbed areas shall meet the requirements of the Department of Highway's Standard Specifications, Sections 212 and 213, as amended.
3. **INDEMNITY:**
  - A. **PERFORMANCE BOND:** The permittee shall provide to the Department a performance bond according to the Permits Manual, Section PE-203 as a guarantee of conformance with the Department's Encroachment Permit requirements.
  - B. **PAYMENT BOND:** At the discretion of the department, a payment bond shall be required of the permittee to ensure payment of liquidated damages assessed to the permittee.
  - C. **LIABILITY INSURANCE:** Liability insurance shall be required of the permittee (in an amount approved by the department) to cover all liabilities associated with the encroachment.
  - D. It shall be the responsibility of the permittee, its successors and assigns, to maintain all indemnities in full force and effect until the permittee is authorized to release the indemnity by the Department.
4. A copy of this application and all related documents making up the approved permit shall be given to the applicant and shall be made readily available for review at the work site at all times.
5. Perpetual maintenance of the encroachment is the responsibility of the permittee, its successors and assigns, with the approval of the Department as required, unless otherwise stated.
6. Permittee, its successors and assigns, shall comply with and agree to be bound by the requirements and terms of (a) this application and all related documents making up the approved permit, (b) by the Department's Permits Manual, and (c) by the Manual on Uniform Traffic Control Devices, both manuals as revised to and in effect on the date of issuance of the permit, all of which documents are made a part thereof by this reference. Compliance by the permittee, its successors and assigns, with subsequent revisions to applicable provisions of either manual or other policy of the Department may be made a condition of allowing the encroachment to persist under the permit.
7. Permittee agrees that this and any encroachment may be ordered removed by the Department at any time, and for any reason, upon thirty days written notice to the last known address of the applicant or to the address at the location of the encroachment. The permittee agrees that the cost of removing and of restoring the associated right-of-way is the responsibility of the permittee, its successors and assigns.
8. Permittee, its successors and assigns, agree that if the Department determines that motor vehicular safety deficiencies develop as a result of the installation or use of the encroachment, the permittee, its successors and assigns, shall provide and bear the expenses to adjust, relocate, or reconstruct the facilities, add signs, auxiliary lanes, or other corrective measures reasonably deemed necessary by the Department within a reasonable time after receipt of a written notice of such deficiency. The period within which such adjustments, relocations, additions, modifications, or other corrective measures must be completed will be specified in the notice.
9. Where traffic signals are required as a condition of granting the requested permit or are thereafter required to correct motor vehicular safety deficiencies, as determined by the Department, the costs for signal equipment and installation(s) shall be borne by the permittee, its successors and assigns and the Department in its reasonable discretion and only in accordance with the Department's current policy set forth in the Traffic Operations Manual and Permits Manual. Any modifications to the permittee's entrance necessary to accommodate signalization (including necessary easement(s) on private property) shall be the responsibility of the permittee, its successors and assigns, at no expense to the Department.



KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
PERMITS BRANCH

TC 99-1A  
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### APPLICATION FOR ENCROACHMENT PERMIT

10. The requested encroachment shall not infringe on the frontage rights of an abutting owner without their written consent as hereinafter described. Each abutting owner shall express their consent, which shall be binding on their successors and assigns, by the submission of a notarized statement as follows, "I (we), \_\_\_\_\_, hereby consent to the granting of the permit requested by the applicant along Route \_\_\_\_\_, which permit does affect frontage rights along my (our) adjacent real property." By signature(s) \_\_\_\_\_, subscribed and sworn by \_\_\_\_\_, on this date \_\_\_\_\_.
11. The permit, if approved, is subject to the agreement that it shall not interfere with any similar rights or permit(s) previously granted to any other party, except as otherwise provided by law.
12. Permittee shall include documentation which describes the facilities to be constructed. Permittee, its successors and assigns, agree as a condition of the granting of the permit to construct and maintain any and all permitted facilities or other encroachments in strict accordance with the submitted and approved permit documentation and the policies and procedures of the Department. Permittee, its successors and assigns, shall not use facilities authorized herein in any manner contrary to that prescribed by the approved permit. Only normal usage as contemplated by the parties and by this application and routine maintenance are authorized by the permit.
13. Permittee, its successors and assigns, at all times from the date permitted work is commenced until such time as all permitted facilities or other encroachments are removed from the right-of-way and the right-of-way restored, **shall defend, protect, indemnify and save harmless** the Department from any and all liability claims and demands arising out of the work, encroachment, maintenance, or other undertaking by the permittee, its successors and assigns, related or undertaken pursuant to the granted permit, due to any claimed act or omission by the permittee, its servants, agents, employees, or contractors. This provision shall not inure to the benefit of any third party nor operate to enlarge any liability of the Department beyond that existing at common law or otherwise if this right to indemnity did not exist.
14. Upon a violation of any provision of the permit, or otherwise in its reasonable discretion, the Department may require additional action by the permittee, its successors and assigns, up to and including the removal of the encroachment and restoration of the right-of-way. In the event additional actions required by the Department under the permit are not undertaken as ordered and within a reasonable time, the Department may in its discretion cause those or other additional corrective actions to be undertaken and the Department shall recover the reasonable costs of those corrective actions from the permittee, its successors and assigns.
15. Permittee, its successors and assigns, shall use the encroachment premises in compliance with all requirements of federal law and regulation, including those imposed pursuant to Title VI of the Civil Right Act of 1964 (42 U.S.C. § 2000d et seq.) and the related regulations of the U.S. Department of Transportation in Title 49 C.F.R. Part 21, all as amended.
16. Permittee, its successors and assigns, agree that if the Department determines it is necessary for the facilities or other encroachment authorized by the permit to be removed, relocated or reconstructed in connection with the reconstruction, relocation or improvement of a highway, the Department may revoke permission for the encroachment to remain under the permit and may order its removal, relocation or reconstruction by the permittee, its successors and assigns, at the expense of the permittee, except where the Department is required by law to pay any or all of those costs.



KENTUCKY TRANSPORTATION CABINET  
Department of Highways  
PERMITS BRANCH

TC 99-1A  
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**APPLICATION FOR ENCROACHMENT PERMIT**

17. Permittee agrees that the authorized permit is personal to the permittee and shall remain in effect until such time as (a) the permittee's rights to the adjoining real property to have benefitted from the requested encroachment have been relinquished, (b) until all permit obligations have been assumed by appropriate successors and assigns, and (c) unless and until a written release from permit obligations has been granted by the Department. The permit and its requirements shall also bind the real property to have benefitted from the requested encroachment to the extent permitted by law. The permit and the related encroachment become the responsibility of the successors and assigns of the permittee and the successors and assigns of each property owner benefitting from the encroachment, or the encroachment may not otherwise permissibly continue to be maintained on the right-of-way. (Does not apply to utility encroachments serving the general public.)
18. If work authorized by the permit is within a highway construction project in the construction phase, it shall be the responsibility of the permittee to make personal contact with the Department's Engineer on the project in order to coordinate all permitted work with the Department's prime contractor on the project.
19. This permit is not intended to, nor shall it, affect, alter or alleviate any requirement imposed upon the permittee, its successors and assigns, by any other agency.
20. Permittee, its successors and assigns, agree to contain and maintain all dirt, mud, and other debris emanating from the encroachment away from the surrounding right-of-way and the travel way of the highway hereafter and at all times that its obligations under the permit remain in effect.
21. Before You Dig: The contractor is instructed to call 1-800-752-6007 to reach KY 811, the One-Call system for information on the location of existing underground utilities. The call is to be placed a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor should be aware that the owners of underground facilities are not required to be members of the KY 811 One-Call Before U-Dig (BUD) service. The contractor must coordinate excavation with the utility owners, including those whom do not subscribe to KY 811. It may be necessary for the contractor to contact the County Clerk to determine what utility companies have facilities in the area.
22. The undersigned Utility acknowledges ownership and control of the facilities proposed to be installed, modified, or extended by the Applicant/Permittee and agrees to be bound by the requirements and terms of this application and all related documents making up the approved permit, by the Department's Permits Guidance Manual, and by all applicable regulations and statutes in effect on the date of issuance of the permit. This information and application is certified correct to the best knowledge and belief of the undersigned Utility.

Kentucky Utilities

UTILITY

Michael Kern

NAME (Utility Representative)

SIGNATURE (Utility Representative)

Project Coordinator

TITLE (Utility Representative)

11 May 2022

DATE



To Submit a Locate Request  
24 Hours a Day, Seven Days a Week:  
Call 811 or 800-752-6007

## NOTICE OF COMPLETION OF ENCROACHMENT PERMIT WORK

---

### PERMITTEE

Name: Kentucky Utilities  
Contact Person: Mike Kern  
Address: 1 Quality St.  
City: Lexington  
State: Kentucky  
Zip: 40509  
Telephone: (859) 310-3230

### PROJECT IDENTIFICATION

Permit Number: 04-2022-00206

---

I wish to notify the Department of Highways that the above mentioned permit work and any necessary right-of-way restoration have been completed and are ready for final inspection.

---

Permittee

---

Please return this form to the address below when work is completed and ready for final inspection.

Please Return to: Permit Engineer  
Department of Highways, District 4 Office  
634 East Dixie Highway  
Elizabethtown, Kentucky 42701  
(270) 766-5066  
[www.transportation.ky.gov/](http://www.transportation.ky.gov/)

LOCATION(S)			
Description	County - Route	Latitude	Longitude
	Hardin - KY 3005	37.664293	-85.900141

5/12/22, 10:42 AM

Permits

# Kentucky Encroachment Permit Tracking

## Maintenance Division



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**PERMIT QUICK SEARCH**

Search Criteria: Permit / Tracking # Search Value:  [Search](#)

**PERMIT GENERAL INFORMATION**

Permit/Tracking #: A04-2022-00206 [Follow](#) Permit Status: Approved Status Date: 5/12/2022  
 Created By: Steve Peace Created Date: 5/12/2022

District: 04 - Elizabethtown

Filing Date: 5/12/2022

Cancellation Date: 8/28/2023

Completion Date: 8/28/2023

Permit Type: Utilities

Permit Subtype: Electric

**Kentucky Utilities : Applicant**

Address: 1 Quality St.  
 Lexington, KY 40509

Phone: (859)310-3230  
 Email: michael.kern@lge-ku.com

Company [Maintain Contacts](#)

[Application](#) [Attachments](#) [Routing](#) [Comments](#) [Locations/Inspections](#) [Indemnity](#) [Approvals](#)

**LOCATION DATA**

Description: **MOST RECENT INSPECTION**  
 There are no inspections for this location.

County: **Hardin**

Route: **3005**

BMP/EMP: **1.076 / 1.076**

Activity: New Install

Location: Overhead

Contract Agr #:

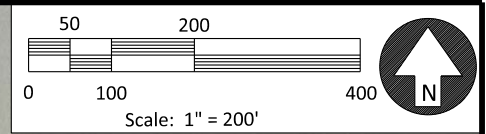
Bridge #:

Description:  
 Installing a new 0kV AFL DNO-1168 OPGE wire on an existing transmission line. The wire heights above the road shall be at or above KYTC minimum clearance and the structures will be off of KYTC R/W. The project will require a temporary closure of KY - 3005. See "Exhibit A" for a plan view drawing of the encroachment location. Also see the traffic control plan attached.

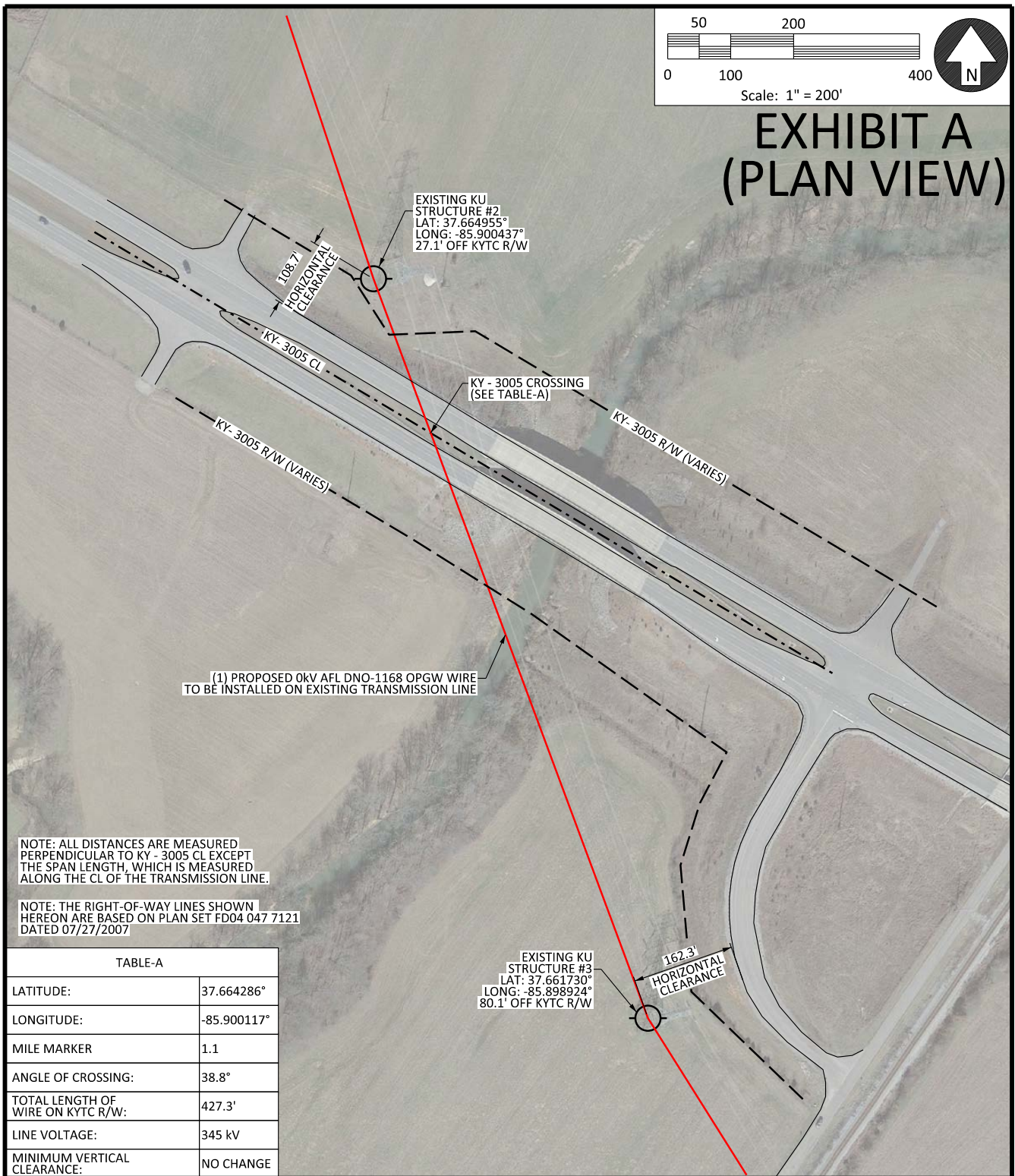
[Edit](#)

[View on Map](#) [Edit Permit](#)

v1.1.7969.26696



# EXHIBIT A (PLAN VIEW)



(1) PROPOSED 0kV AFL DNO-1168 OPGW WIRE TO BE INSTALLED ON EXISTING TRANSMISSION LINE

NOTE: ALL DISTANCES ARE MEASURED PERPENDICULAR TO KY - 3005 CL EXCEPT THE SPAN LENGTH, WHICH IS MEASURED ALONG THE CL OF THE TRANSMISSION LINE.

NOTE: THE RIGHT-OF-WAY LINES SHOWN HEREON ARE BASED ON PLAN SET FD04 047 7121 DATED 07/27/2007

TABLE-A	
LATITUDE:	37.664286°
LONGITUDE:	-85.900117°
MILE MARKER	1.1
ANGLE OF CROSSING:	38.8°
TOTAL LENGTH OF WIRE ON KYTC R/W:	427.3'
LINE VOLTAGE:	345 kV
MINIMUM VERTICAL CLEARANCE:	NO CHANGE

EXISTING KU STRUCTURE #3  
 LAT: 37.661730°  
 LONG: -85.898924°  
 80.1' OFF KYTC R/W

162.3'  
 HORIZONTAL CLEARANCE



P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097

PLAN VIEW  
 KENTUCKY UTILITIES COMPANY  
 KY - 3005  
 HARDIN COUNTY, KENTUCKY

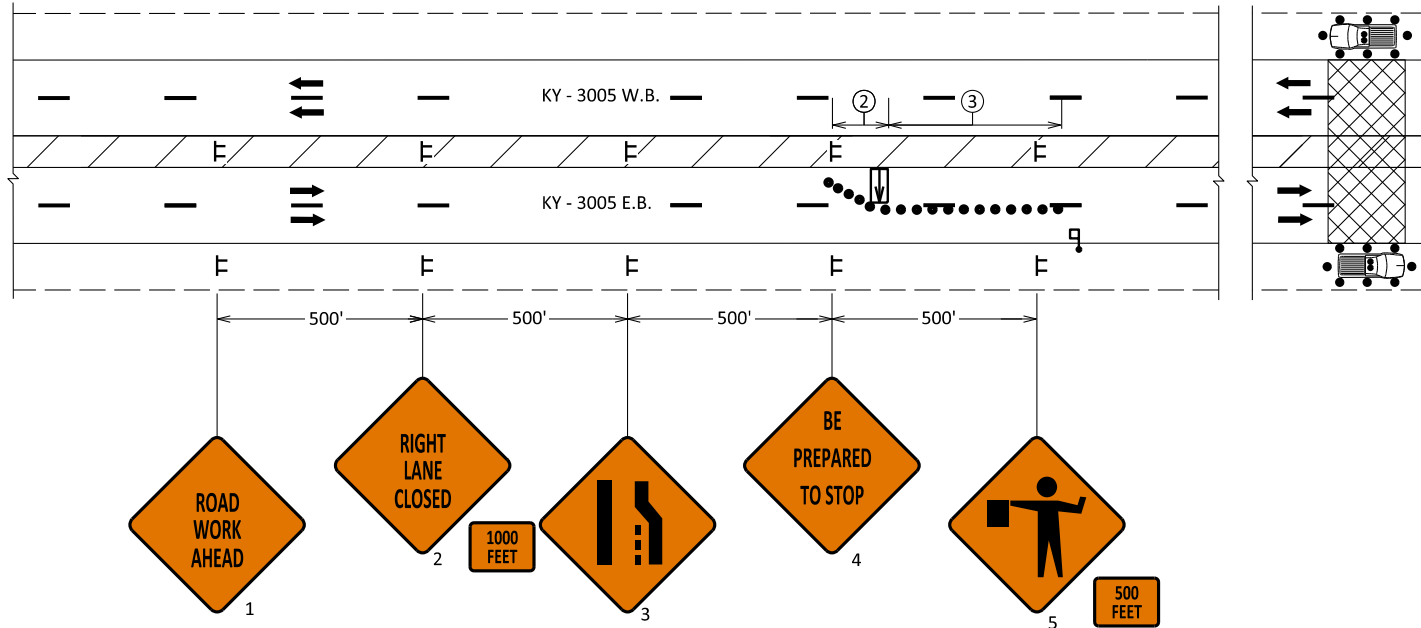
DATE: 05/11/2022  
 SCALE: 1" = 200'  
 DRAWN BY: HSM  
 APPROVED BY: HSM  
 FILENAME: 21568 HWY



KY - 3005 TEMPORARY CLOSURE

PAGE 1 OF 2

DRAWING NOT TO SCALE



~ NOTES ~

- 1.) THE MINIMUM SIZE OF SIGNS 1 THROUGH 5 SHALL BE 48" X 48".
- 2.) THE MERGING TAPER SHALL BE 660' IN LENGTH. SPACING OF CHANNELIZING DEVICES SHOULD BE 55' THROUGH THE TAPER AREAS.
- 3.) THE SPACING OF CHANNELIZING DEVICES SHALL BE 55' THROUGH THE WORK AREA.
- 4.) THERE SHALL BE AN ARROW BOARD POSITIONED AT THE INDICATED AREAS.
- 5.) SIGNS SHALL BE DOUBLE POSTED WHERE THE MEDIAN ALLOWS.
- 6.) THE TRUCKS SHALL BE PLACED AS TO ALLOW A ROW OF BARRELS BETWEEN THE TRUCKS AND THE EDGE OF THE DRIVING LANES. BARRELS SHALL BE PLACED COMPLETELY AROUND THE TRUCKS TO INCREASE VISIBILITY.

APPLICATION

THIS DRAWING APPLIES TO WORK ON KU POLES WHICH WILL REQUIRE A LANE CLOSURE ALONG KY - 3005.

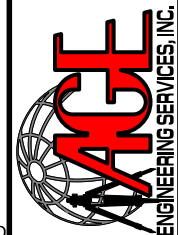
TRAFFIC CONTROL SUPERVISOR	
HUNTER MCKINNEY	PRINTED NAME
S18100056	WORK ZONE TRAFFIC CONTROL SUPERVISOR NUMBER
<i>Hunter McKinney</i>	APPROVAL SIGNATURE
05/11/2022	DATE

- LEGEND
- WORK ZONE
  - SIGN
  - ARROW BOARD
  - CHANNELIZING DEVICES CONES
  - FLAGGER
  - BUCKET TRUCK WITH EXTENDED BOOM AND FLASHING LIGHTS

DATE: 05/11/2022  
 SCALE: NTS  
 DRAWN BY: HSM  
 APPROVED BY: HSM  
 FILENAME: 21568TCP

TRAFFIC CONTROL PLAN  
 KENTUCKY UTILITIES  
 KY-3005  
 HARDIN COUNTY, KENTUCKY

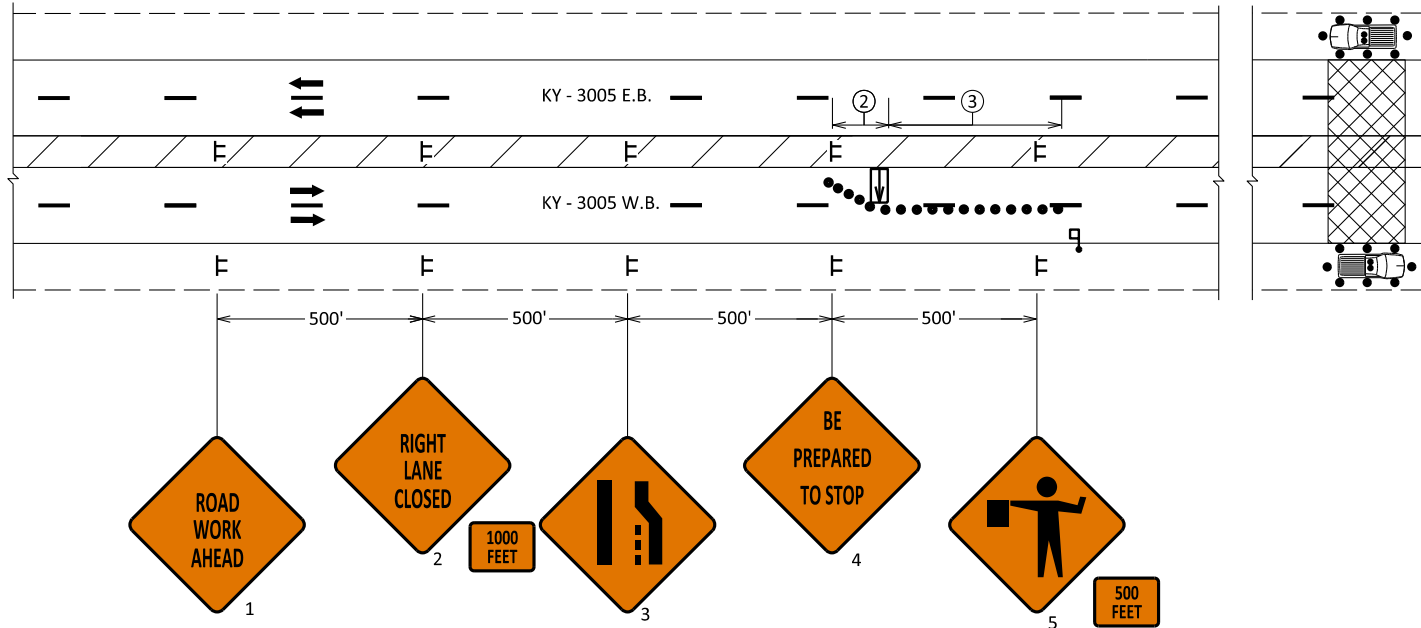
P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097



**KY - 3005 TEMPORARY CLOSURE**

PAGE 2 OF 2

DRAWING NOT TO SCALE



DATE: 05/11/2022  
 SCALE: NTS  
 DRAWN BY: HSM  
 APPROVED BY: HSM  
 FILENAME: 21568TCP

TRAFFIC CONTROL PLAN  
 KENTUCKY UTILITIES  
 KY-3005  
 HARDIN COUNTY, KENTUCKY

P.O. BOX 204  
 165 FOSTER LANE  
 STANFORD, KY 40484  
 PHONE (606) 365-8362  
 FAX (606) 365-1097



~ NOTES ~

- 1.) THE MINIMUM SIZE OF SIGNS 1 THROUGH 5 SHALL BE 48" X 48".
- 2.) THE MERGING TAPER SHALL BE 660' IN LENGTH. SPACING OF CHANNELIZING DEVICES SHOULD BE 55' THROUGH THE TAPER AREAS.
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- 6.) THE TRUCKS SHALL BE PLACED AS TO ALLOW A ROW OF BARRELS BETWEEN THE TRUCKS AND THE EDGE OF THE DRIVING LANES. BARRELS SHALL BE PLACED COMPLETELY AROUND THE TRUCKS TO INCREASE VISIBILITY.

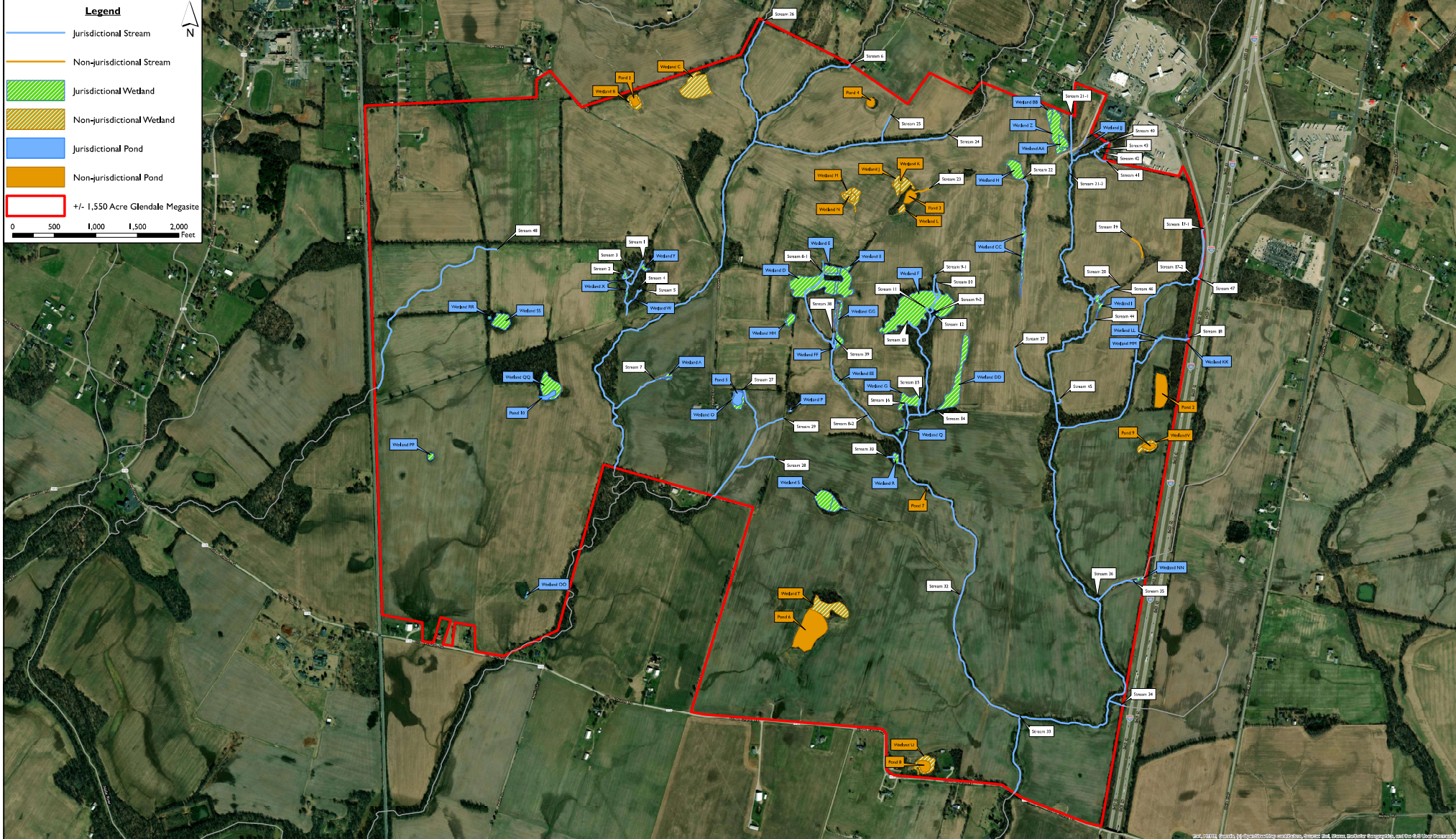
APPLICATION

THIS DRAWING APPLIES TO WORK ON KU POLES WHICH WILL REQUIRE A LANE CLOSURE ALONG KY - 3005.

TRAFFIC CONTROL SUPERVISOR	
HUNTER MCKINNEY	
PRINTED NAME	
S18100056	
WORK ZONE TRAFFIC CONTROL SUPERVISOR NUMBER	
	05/11/2022
APPROVAL SIGNATURE	DATE

- LEGEND**
- WORK ZONE
  - SIGN
  - ARROW BOARD
  - CHANNELIZING DEVICES  
CONES
  - FLAGGER
  - BUCKET TRUCK WITH EXTENDED  
BOOM AND FLASHING LIGHTS

Document Path: P:\Project\_Files\Kentucky\21-049\_ford\_Megsite\4 Working Documents\Maping\GIS\URACE\2021-01-21\_All Waters\_Fishba\_McFarland Data Swath\1/21/2022 11:45:39 AM User Name: korazon



Prepared by:  
Third Rock Consultants, LLC  
2526 Regency Road, Suite 180  
Lexington, Kentucky 40503

# All Waters Delineated Elizabethtown-Hardin County Industrial Foundation Megaside Glendale, Hardin County, Kentucky

Blue Oval SK Battery Park  
LRL-2021-443-sea  
37.588248° / -85.886739°



Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3957-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 4
Location:	Elizabethtown, KY
Latitude:	37-39-41.30N NAD 83
Longitude:	85-53-55.42W
Heights:	670 feet site elevation (SE) 152 feet above ground level (AGL) 822 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3957-OE.

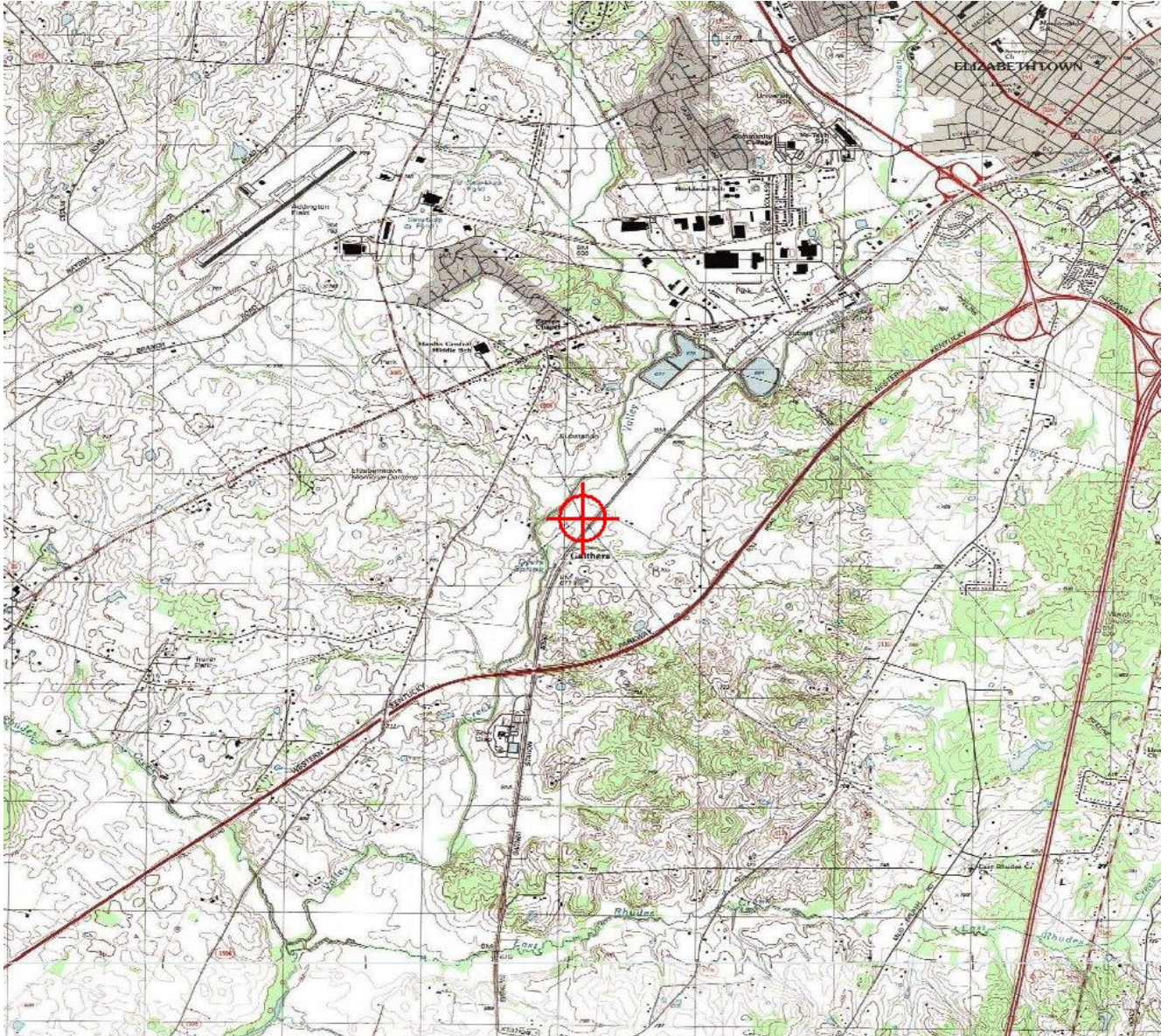
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( DNE )

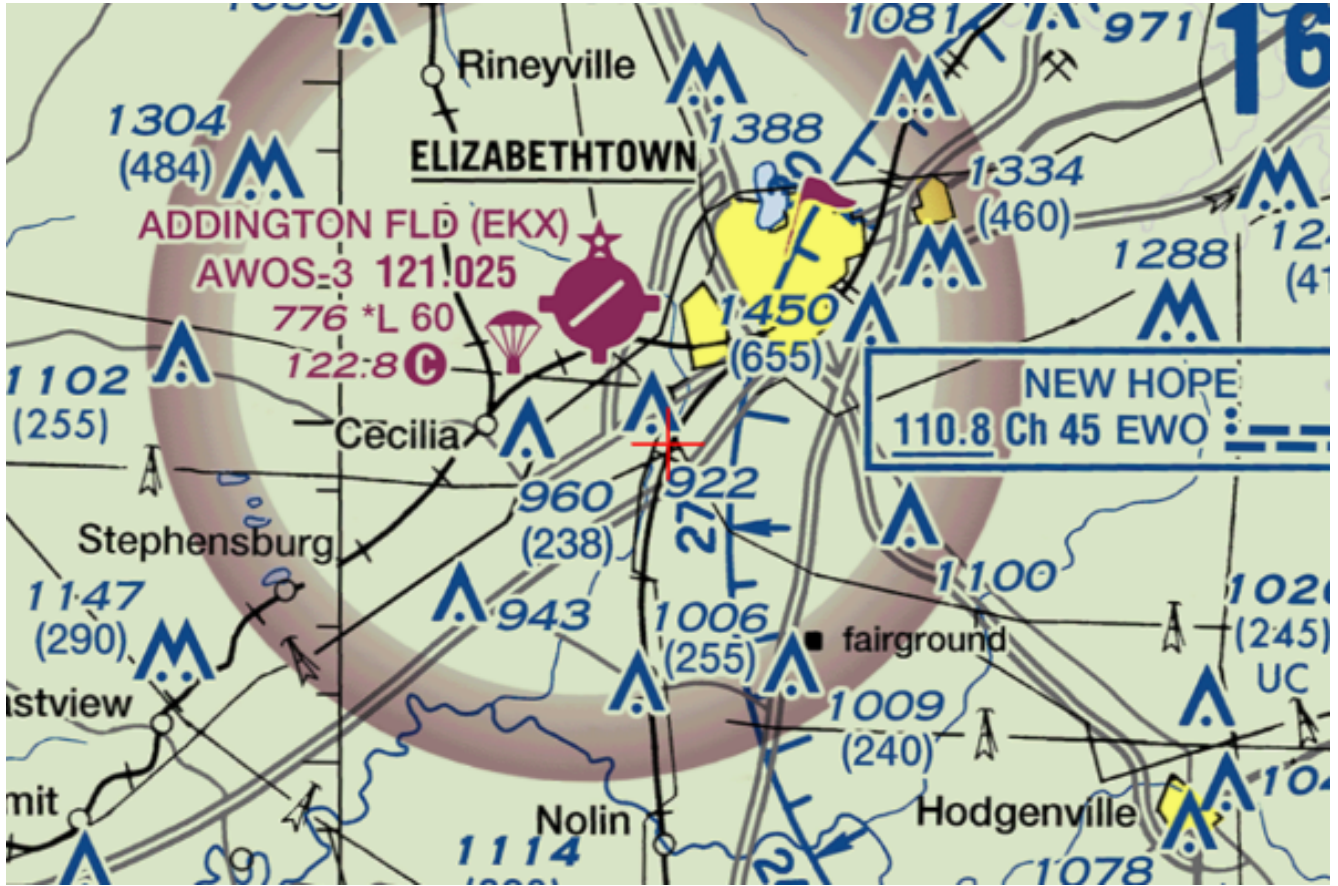
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3957-OE



Sectional Map for ASN 2022-ASO-3957-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3958-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 5
Location:	Elizabethtown, KY
Latitude:	37-39-35.15N NAD 83
Longitude:	85-54-02.63W
Heights:	668 feet site elevation (SE) 115 feet above ground level (AGL) 783 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- extended, revised, or terminated by the issuing office.
- the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



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This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3958-OE.

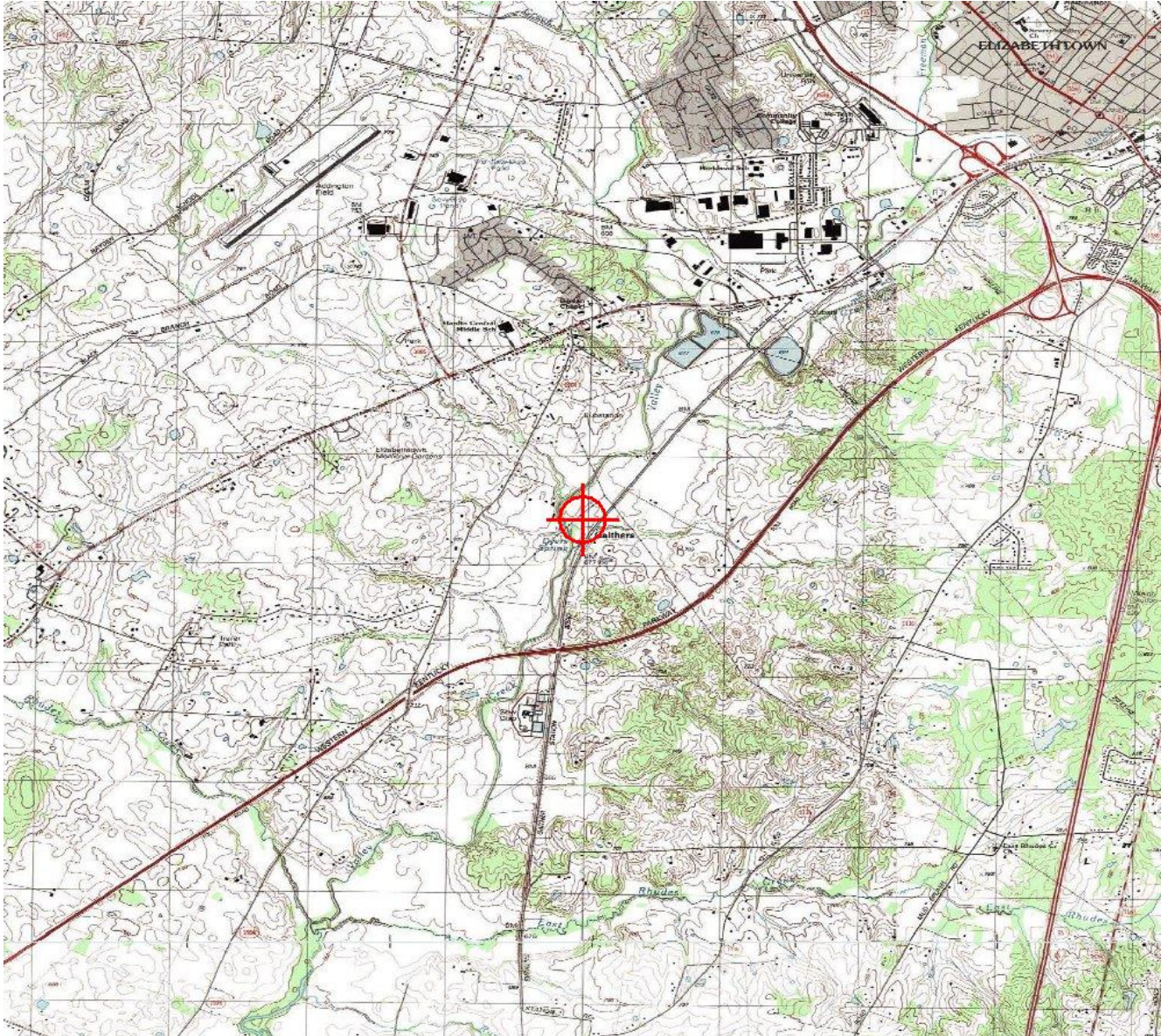
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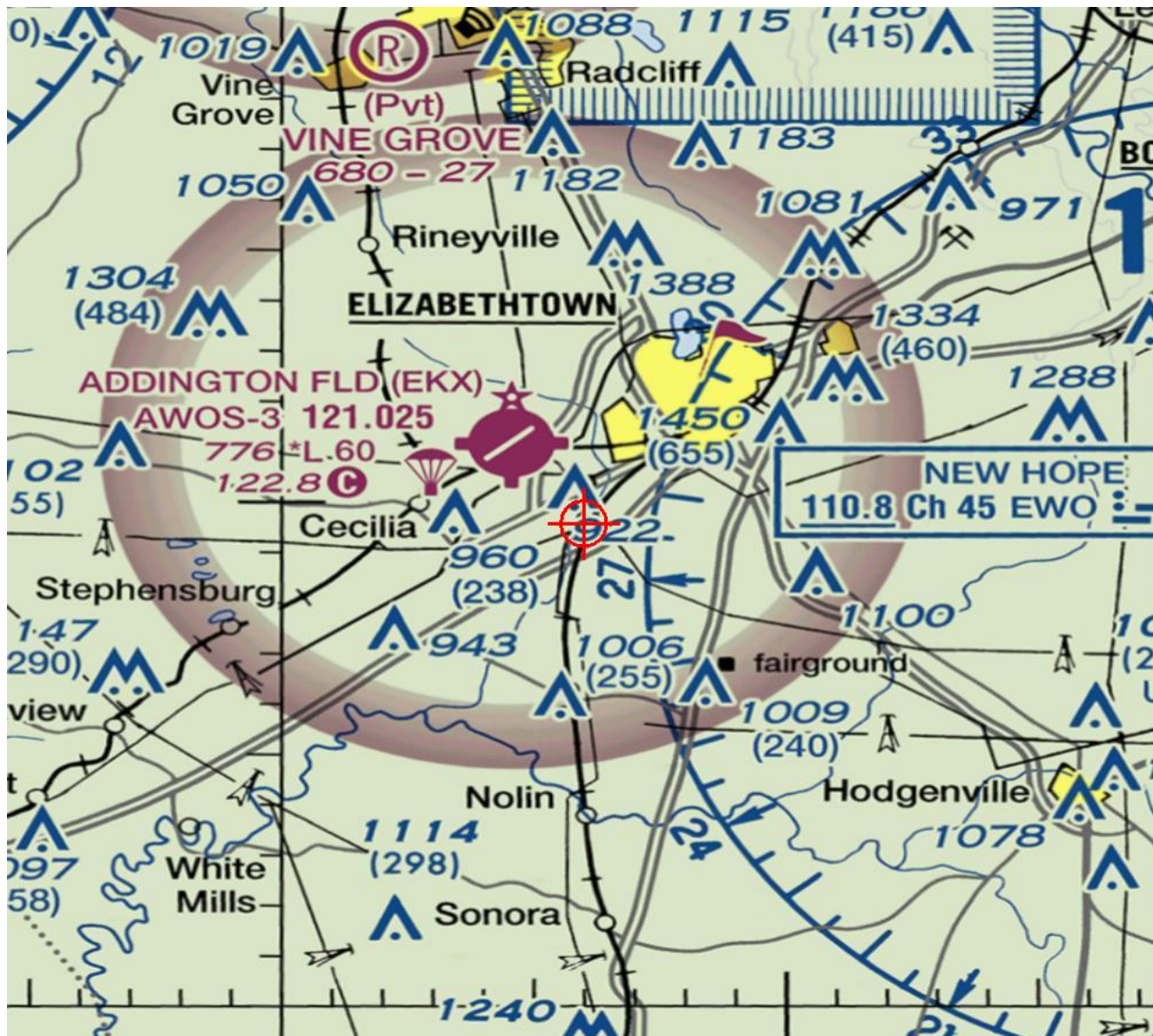
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3958-OE



Sectional Map for ASN 2022-ASO-3958-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3959-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 6
Location:	Elizabethtown, KY
Latitude:	37-39-29.36N NAD 83
Longitude:	85-54-09.43W
Heights:	671 feet site elevation (SE) 82 feet above ground level (AGL) 753 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3959-OE.

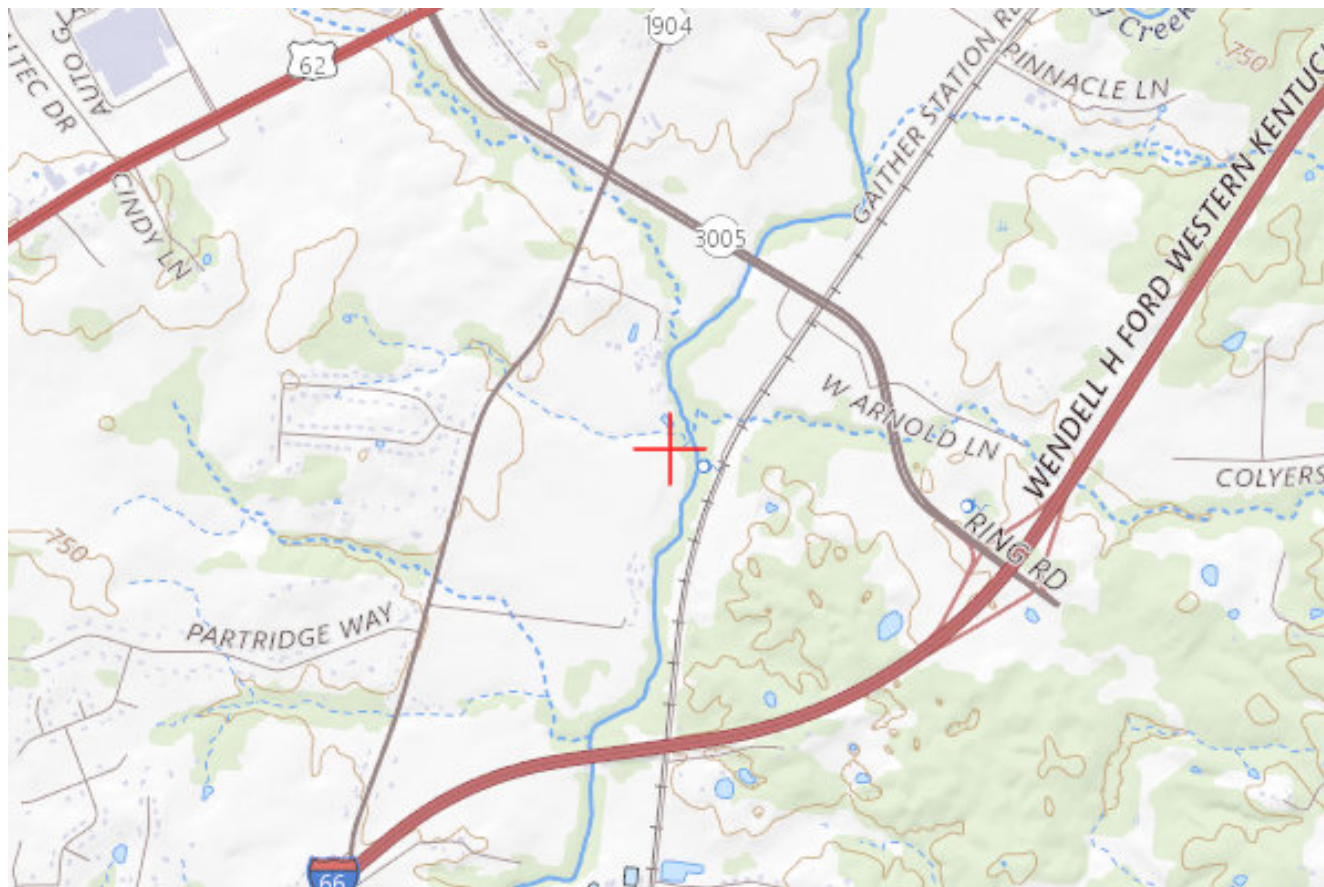
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( DNE )

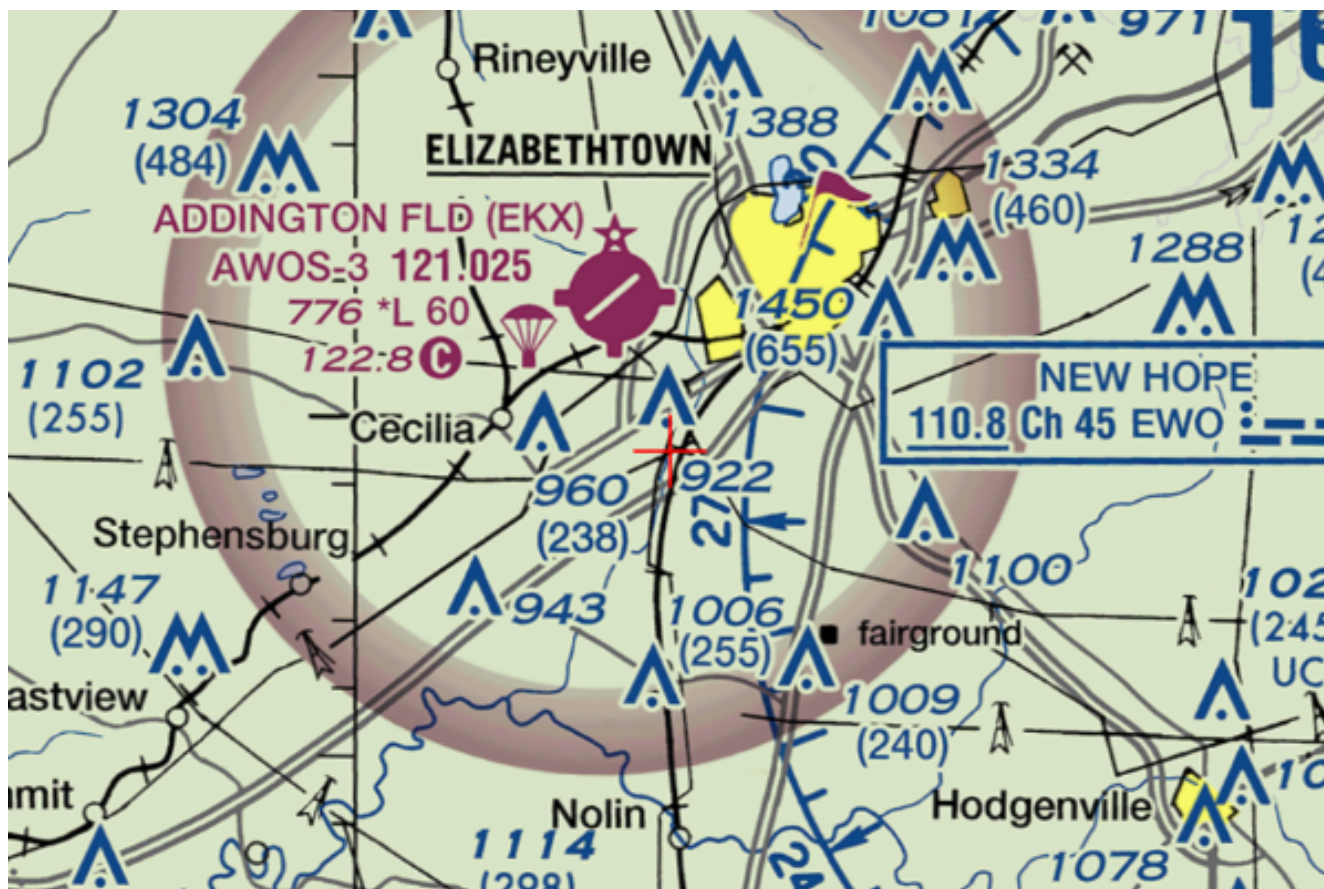
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3959-OE



Sectional Map for ASN 2022-ASO-3959-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3960-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 7
Location:	Elizabethtown, KY
Latitude:	37-39-29.36N NAD 83
Longitude:	85-54-14.25W
Heights:	685 feet site elevation (SE) 77 feet above ground level (AGL) 762 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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- (b) extended, revised, or terminated by the issuing office.
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If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3960-OE.

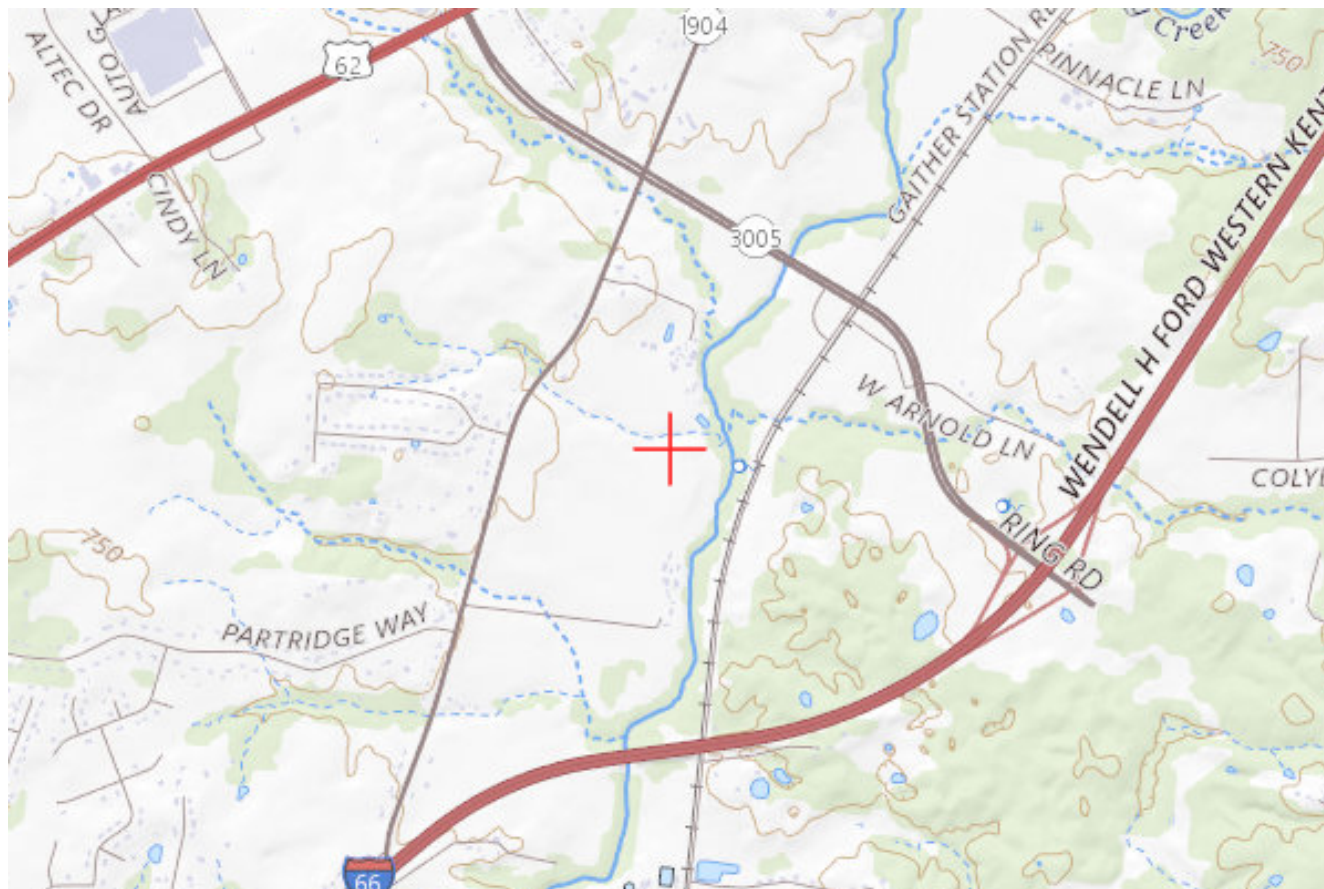
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( DNE )

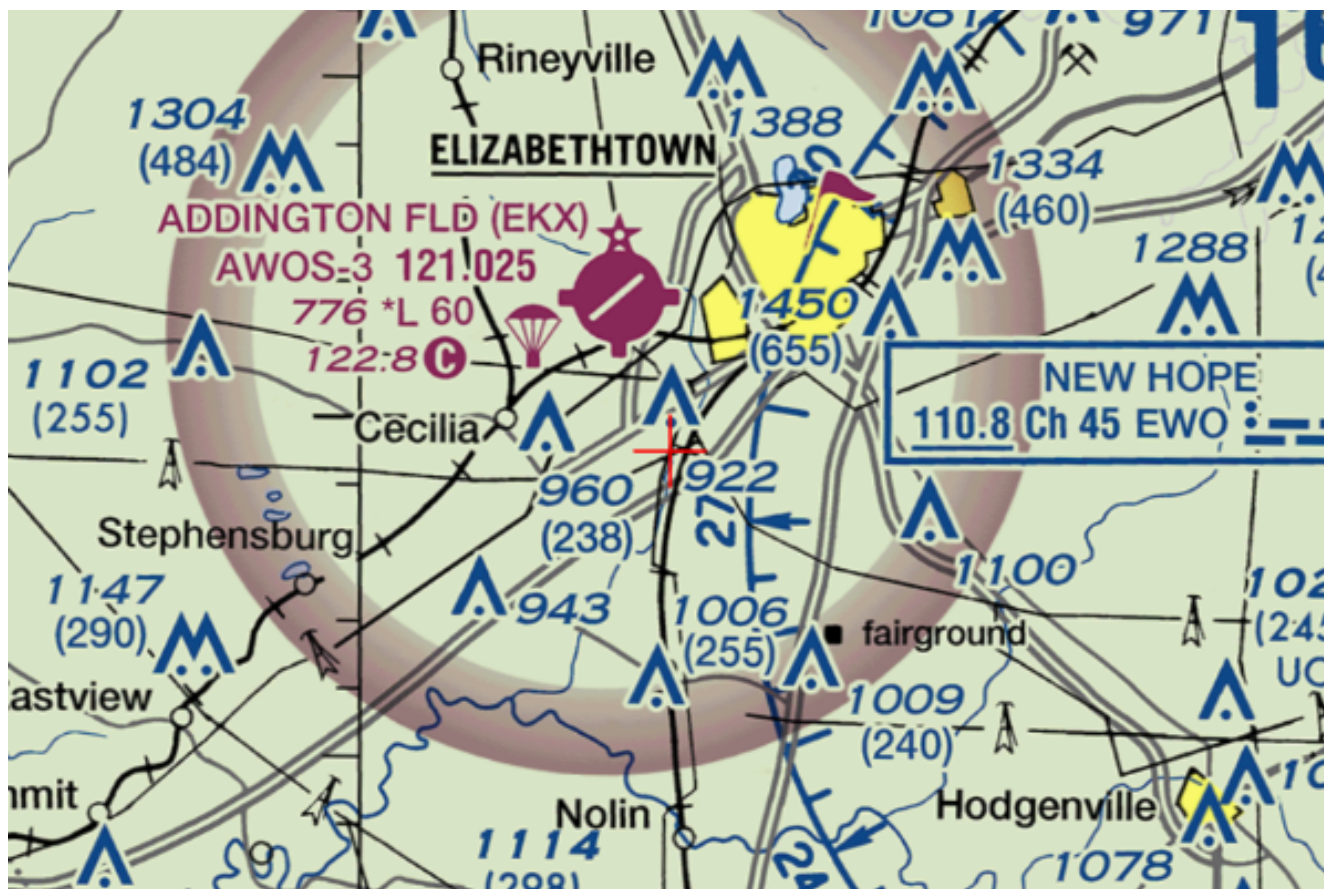
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3960-OE



Sectional Map for ASN 2022-ASO-3960-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3961-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 8
Location:	Elizabethtown, KY
Latitude:	37-39-19.99N NAD 83
Longitude:	85-54-20.42W
Heights:	678 feet site elevation (SE) 86 feet above ground level (AGL) 764 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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- extended, revised, or terminated by the issuing office.
- the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3961-OE.

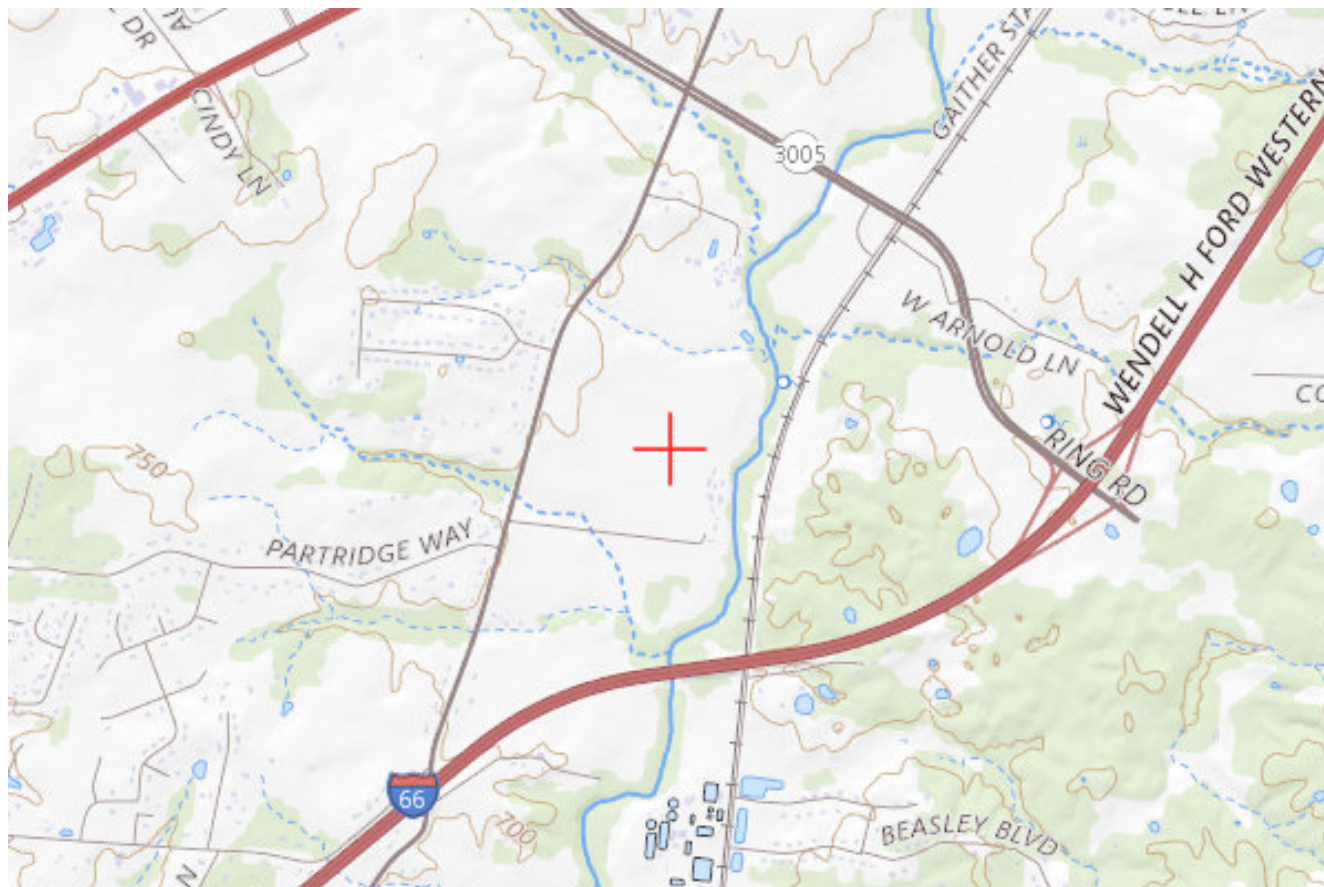
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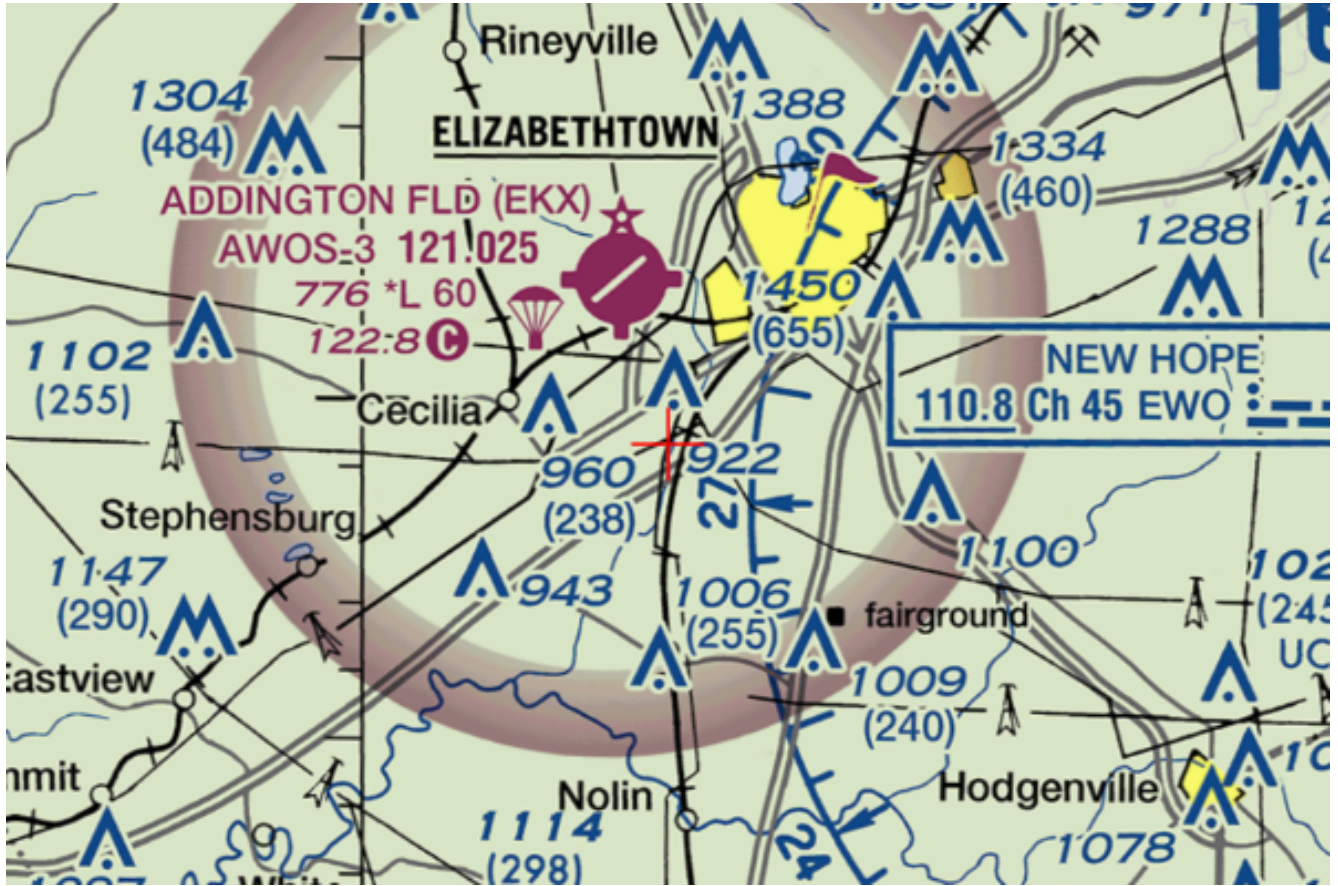
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3961-OE



Sectional Map for ASN 2022-ASO-3961-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3962-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 9
Location:	Elizabethtown, KY
Latitude:	37-39-14.11N NAD 83
Longitude:	85-54-27.31W
Heights:	678 feet site elevation (SE) 95 feet above ground level (AGL) 773 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3962-OE.

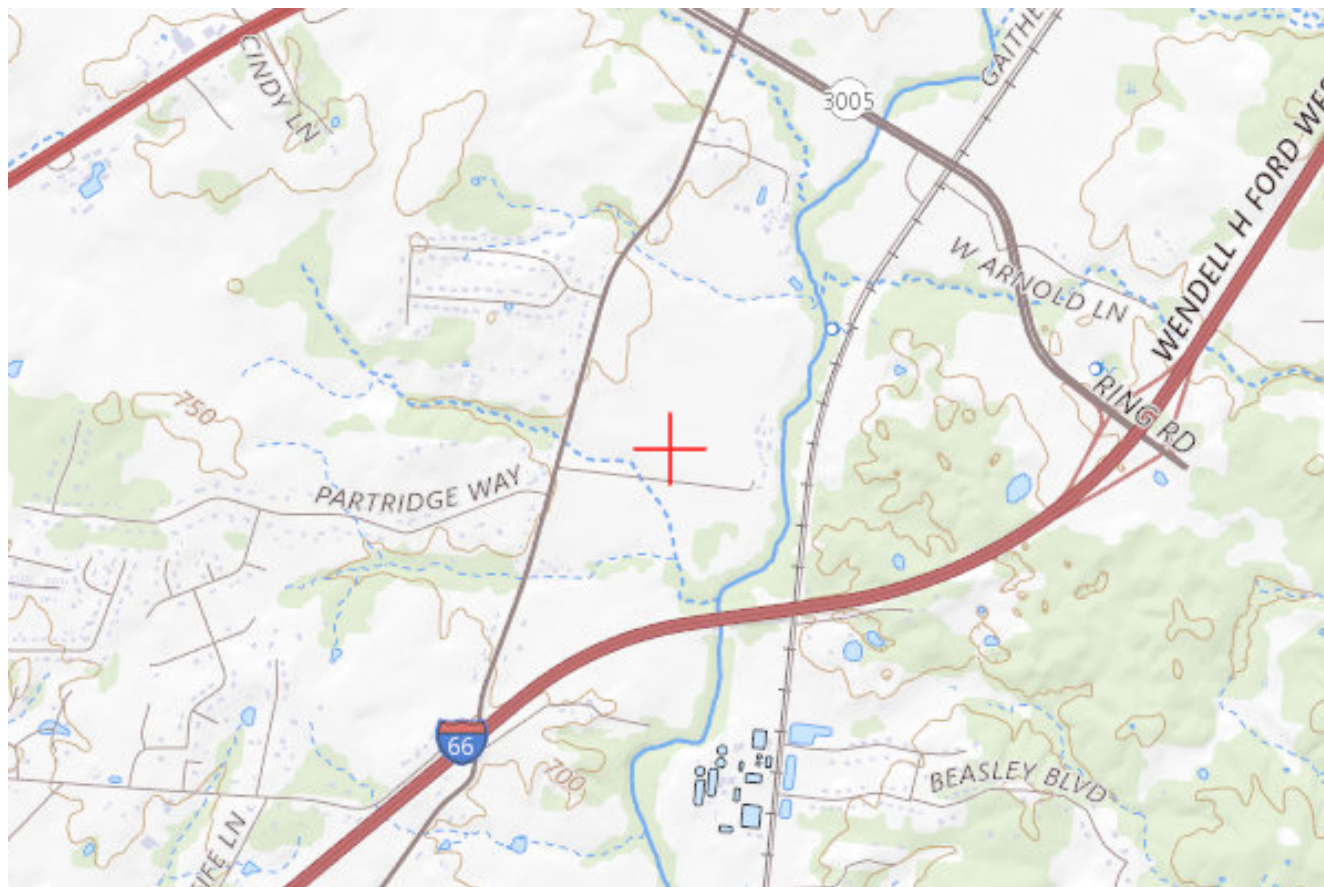
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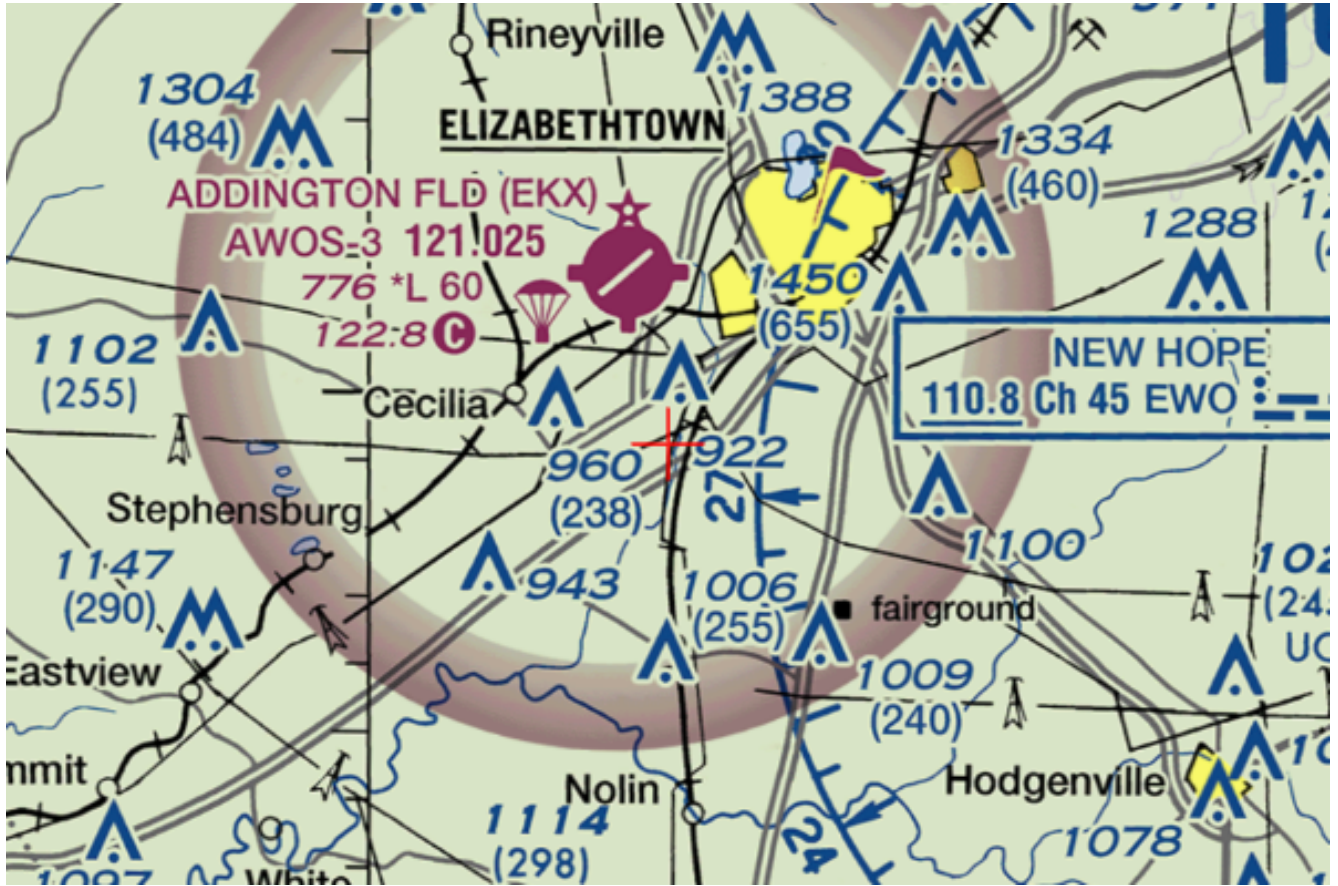
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3962-OE



Sectional Map for ASN 2022-ASO-3962-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3963-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 10
Location:	Elizabethtown, KY
Latitude:	37-39-08.40N NAD 83
Longitude:	85-54-34.01W
Heights:	678 feet site elevation (SE) 115 feet above ground level (AGL) 793 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

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If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3963-OE.

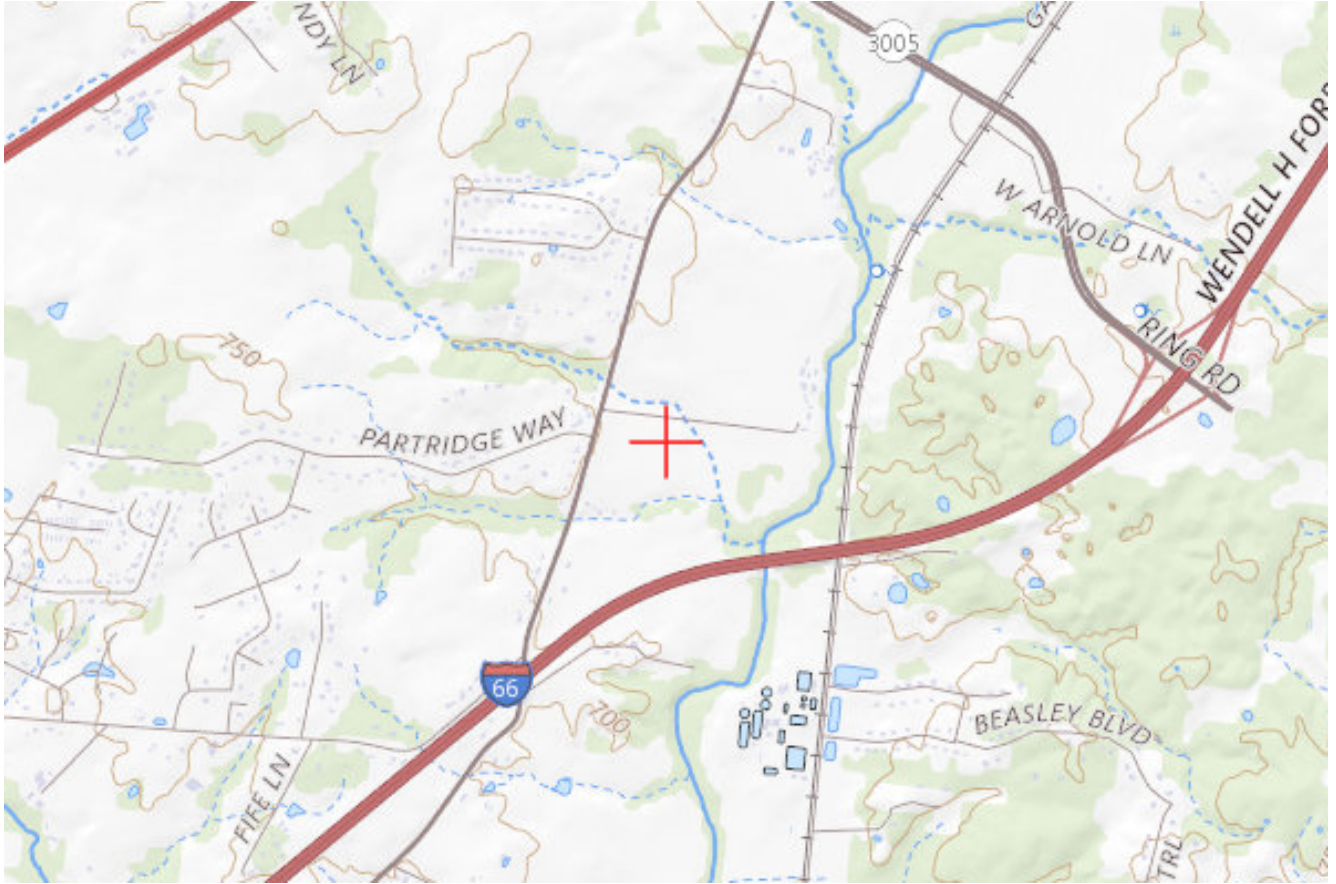
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( DNE )

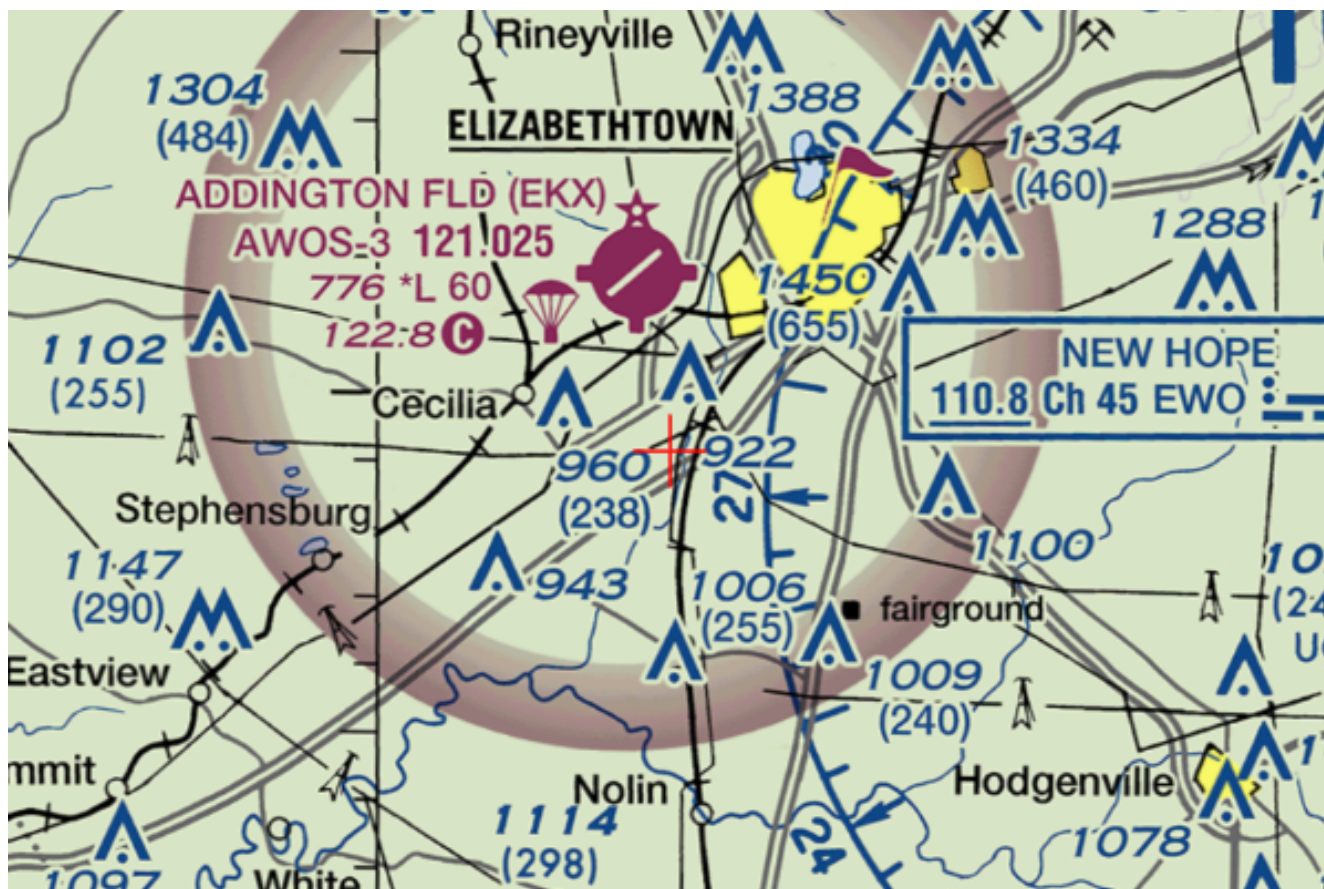
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3963-OE



Sectional Map for ASN 2022-ASO-3963-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3964-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 11
Location:	Elizabethtown, KY
Latitude:	37-39-00.03N NAD 83
Longitude:	85-54-34.46W
Heights:	680 feet site elevation (SE) 82 feet above ground level (AGL) 762 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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- the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



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If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3964-OE.

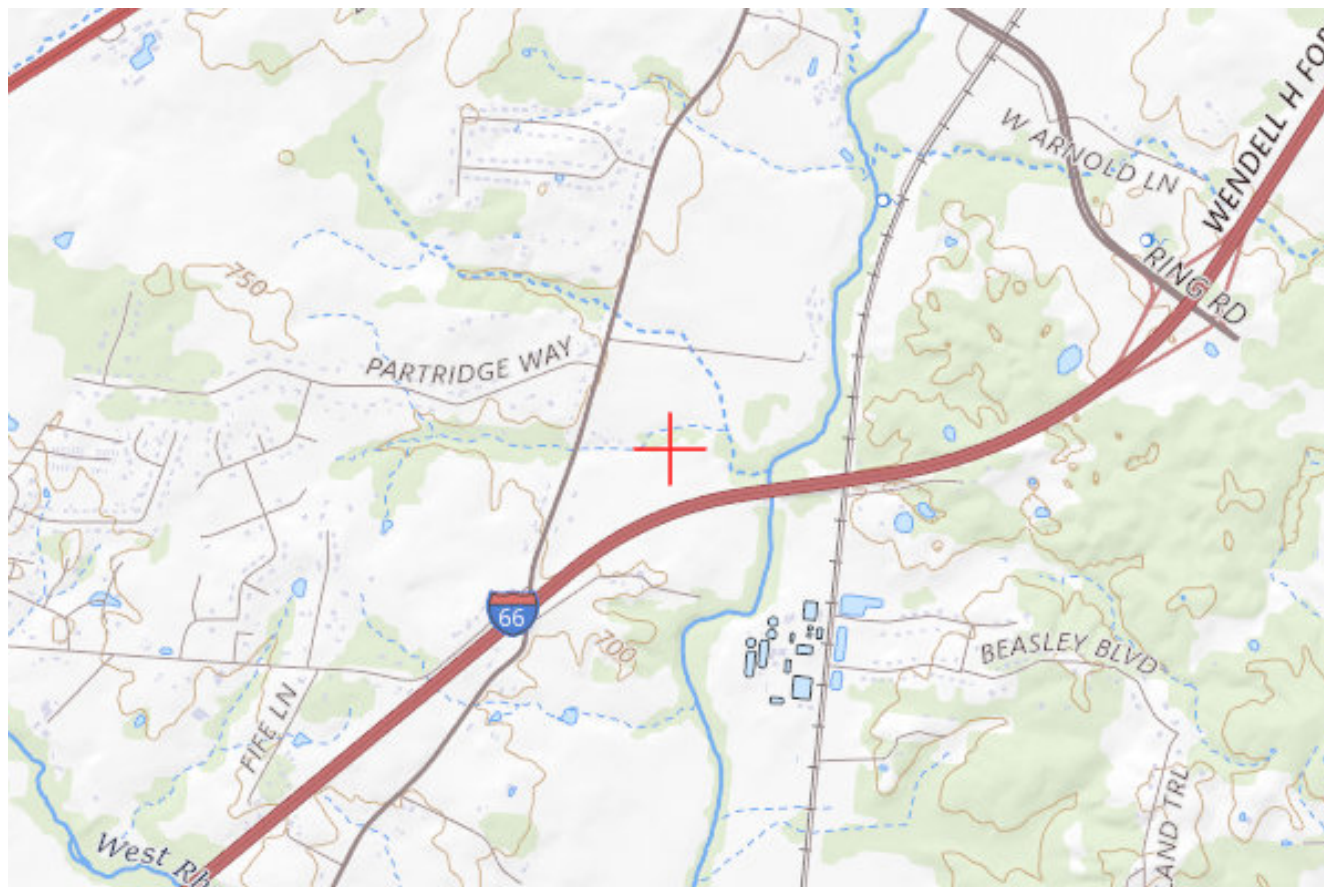
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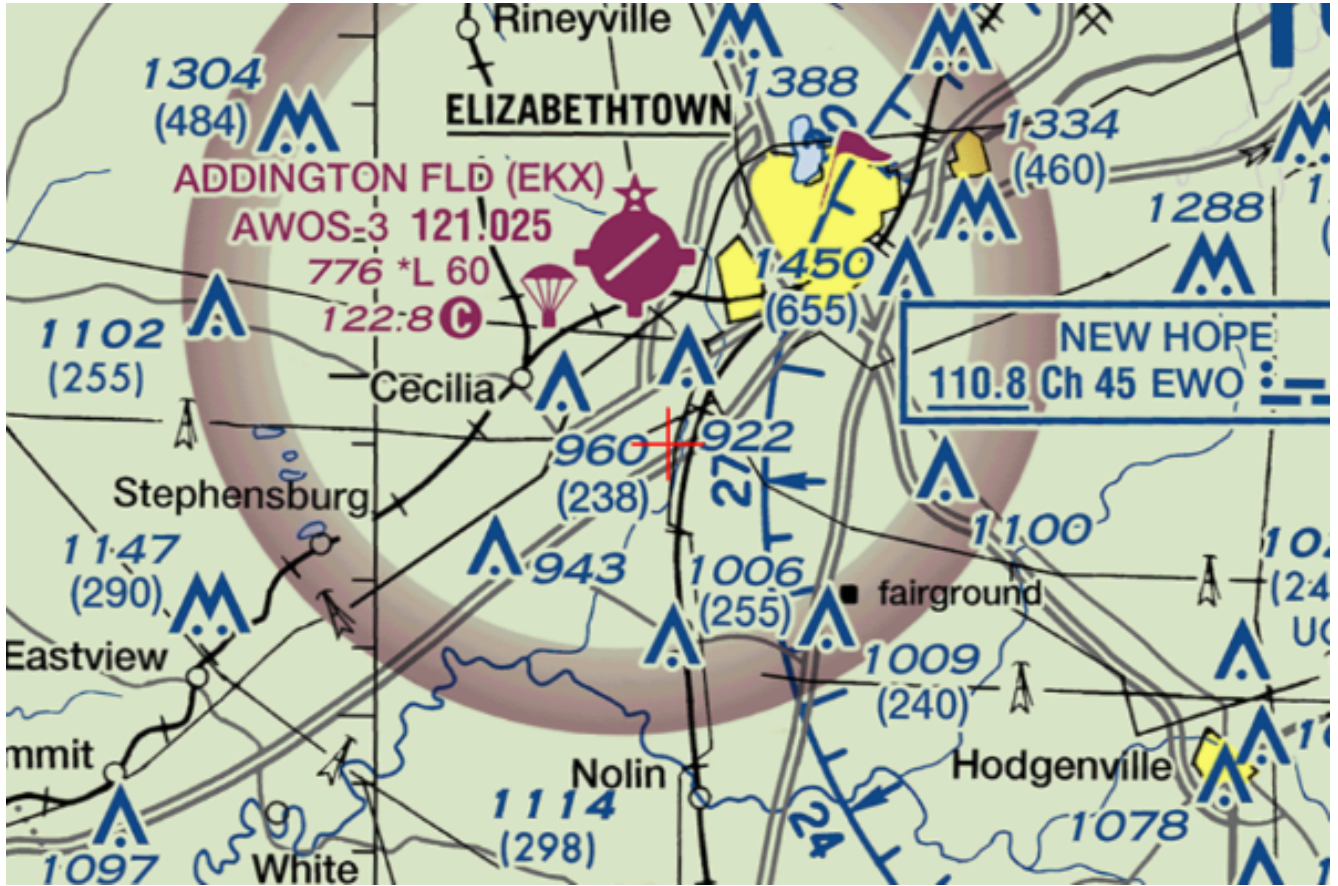
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3964-OE



Sectional Map for ASN 2022-ASO-3964-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3965-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 12
Location:	Elizabethtown, KY
Latitude:	37-38-55.35N NAD 83
Longitude:	85-54-34.71W
Heights:	679 feet site elevation (SE) 82 feet above ground level (AGL) 761 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

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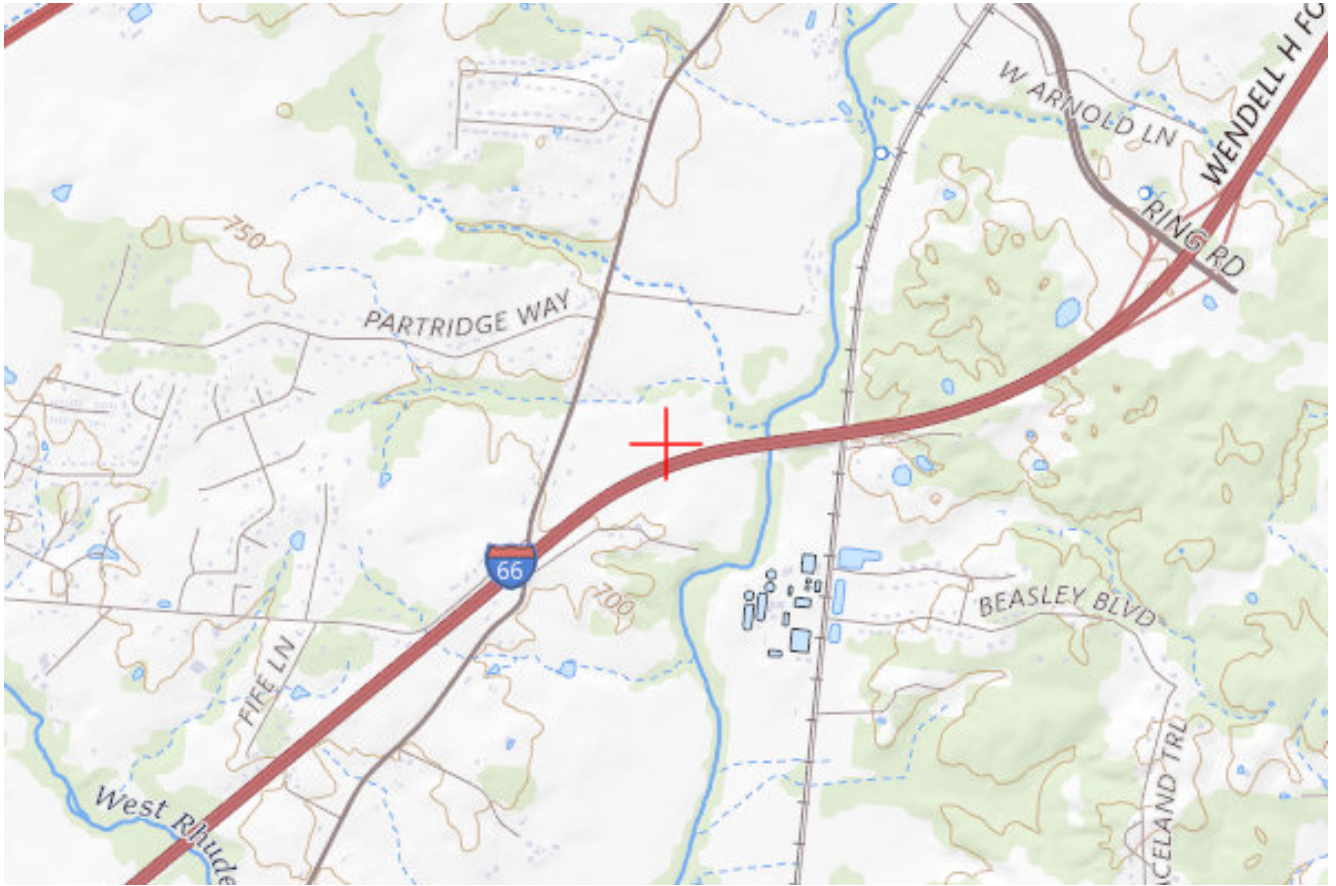
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( DNE )

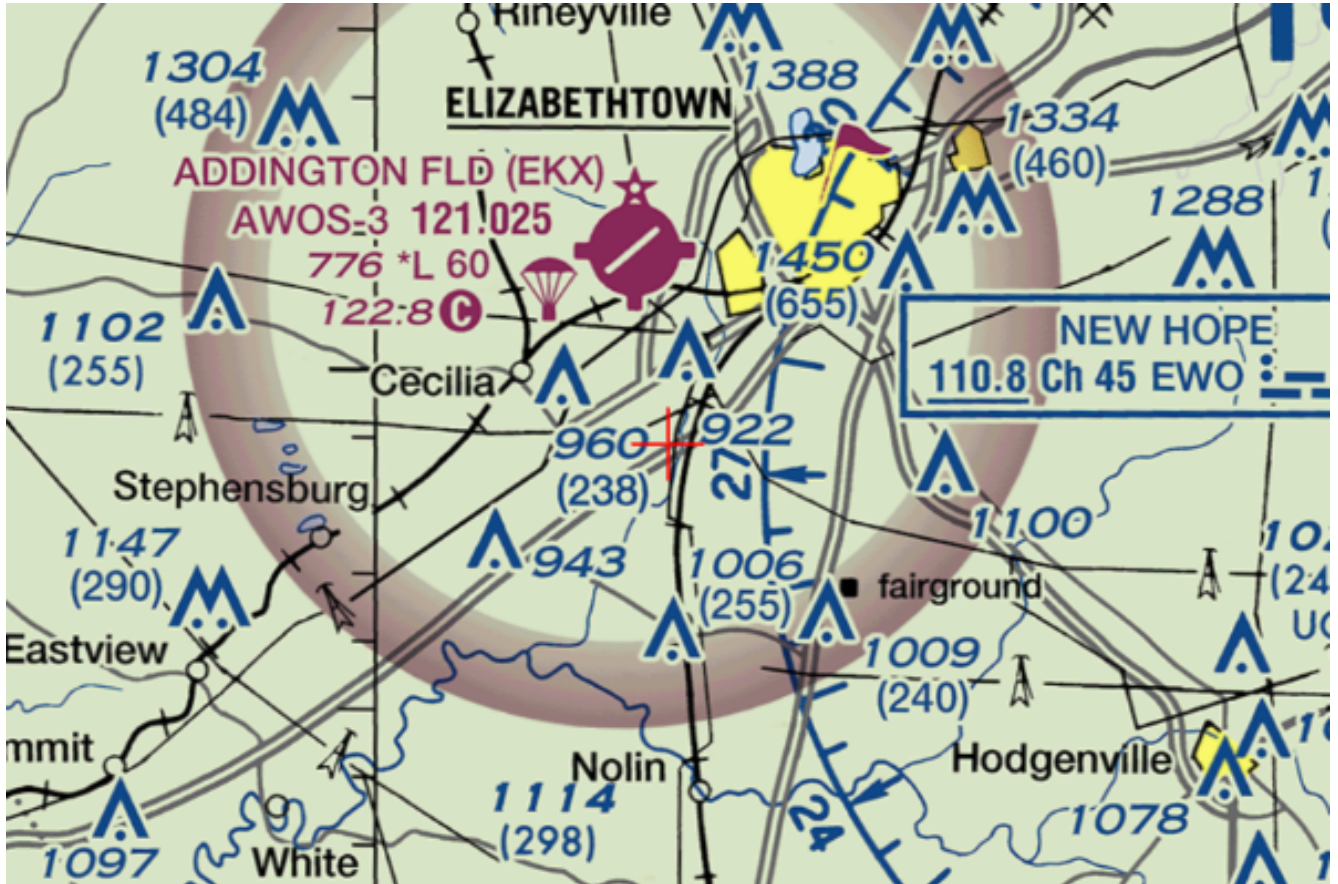
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3965-OE



Sectional Map for ASN 2022-ASO-3965-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3966-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 13
Location:	Elizabethtown, KY
Latitude:	37-38-50.13N NAD 83
Longitude:	85-54-34.99W
Heights:	679 feet site elevation (SE) 82 feet above ground level (AGL) 761 feet above mean sea level (AMSL)

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3966-OE.

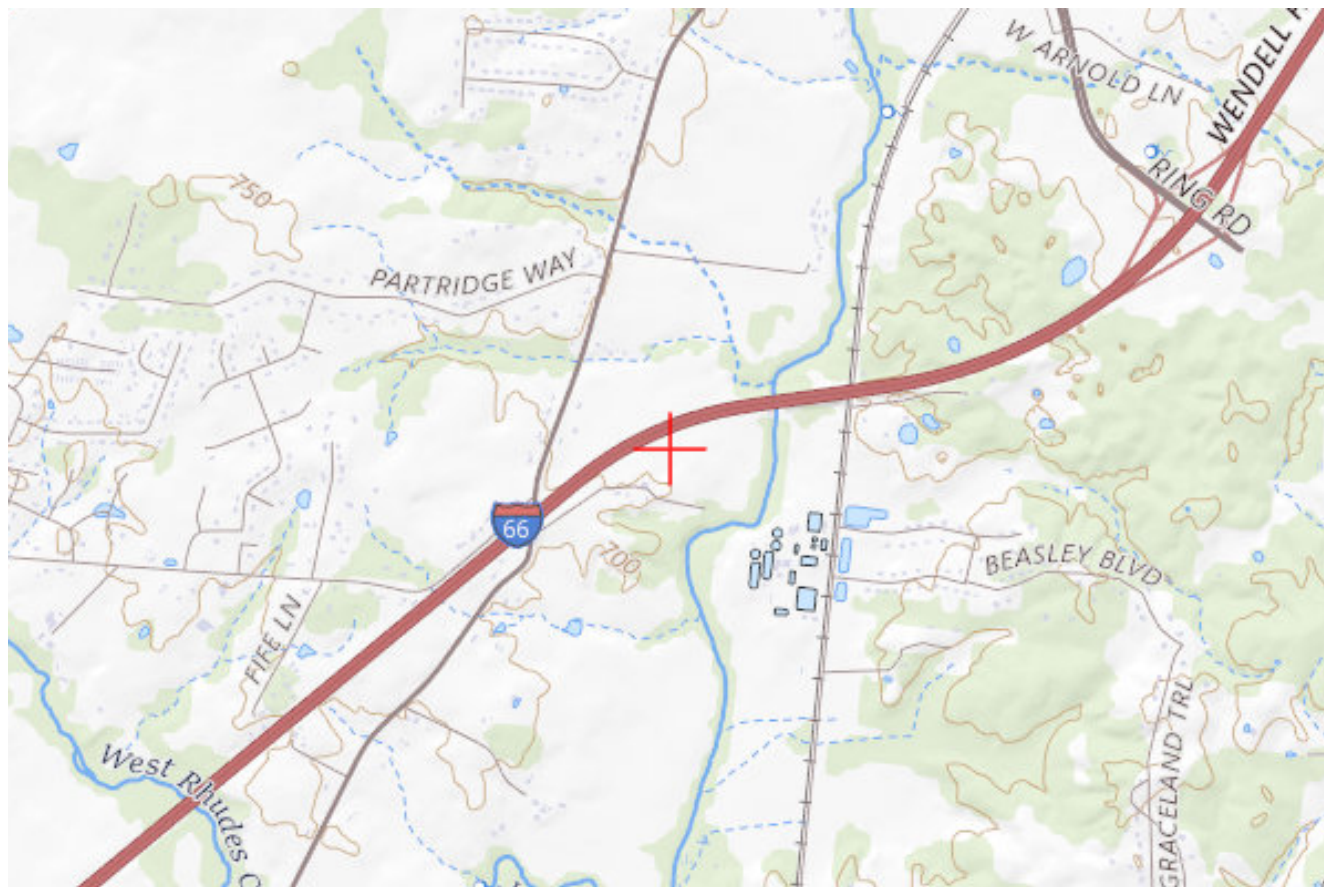
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( DNE )

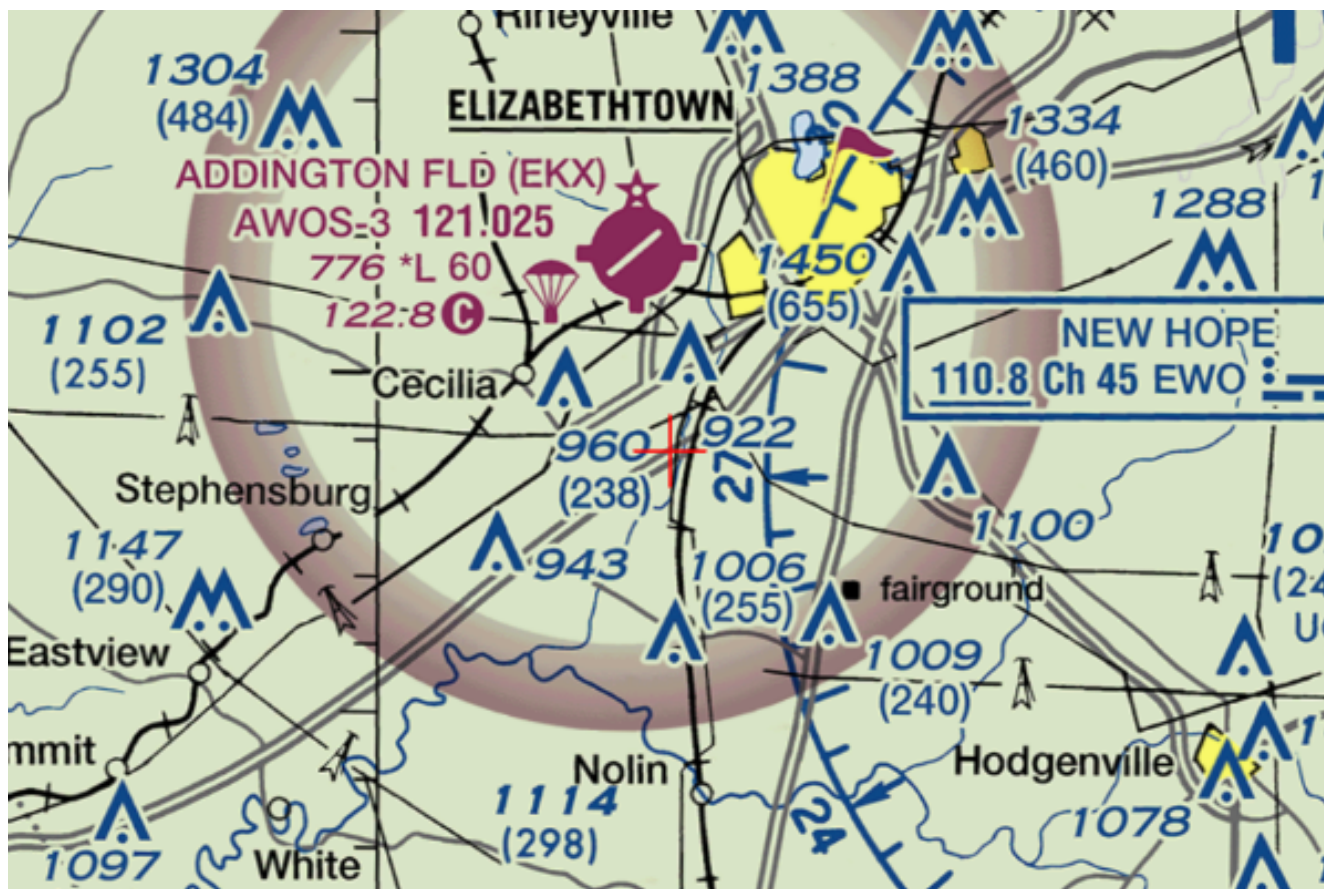
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3966-OE



Sectional Map for ASN 2022-ASO-3966-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3967-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 14
Location:	Elizabethtown, KY
Latitude:	37-38-45.09N NAD 83
Longitude:	85-54-35.27W
Heights:	698 feet site elevation (SE) 73 feet above ground level (AGL) 771 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3967-OE.

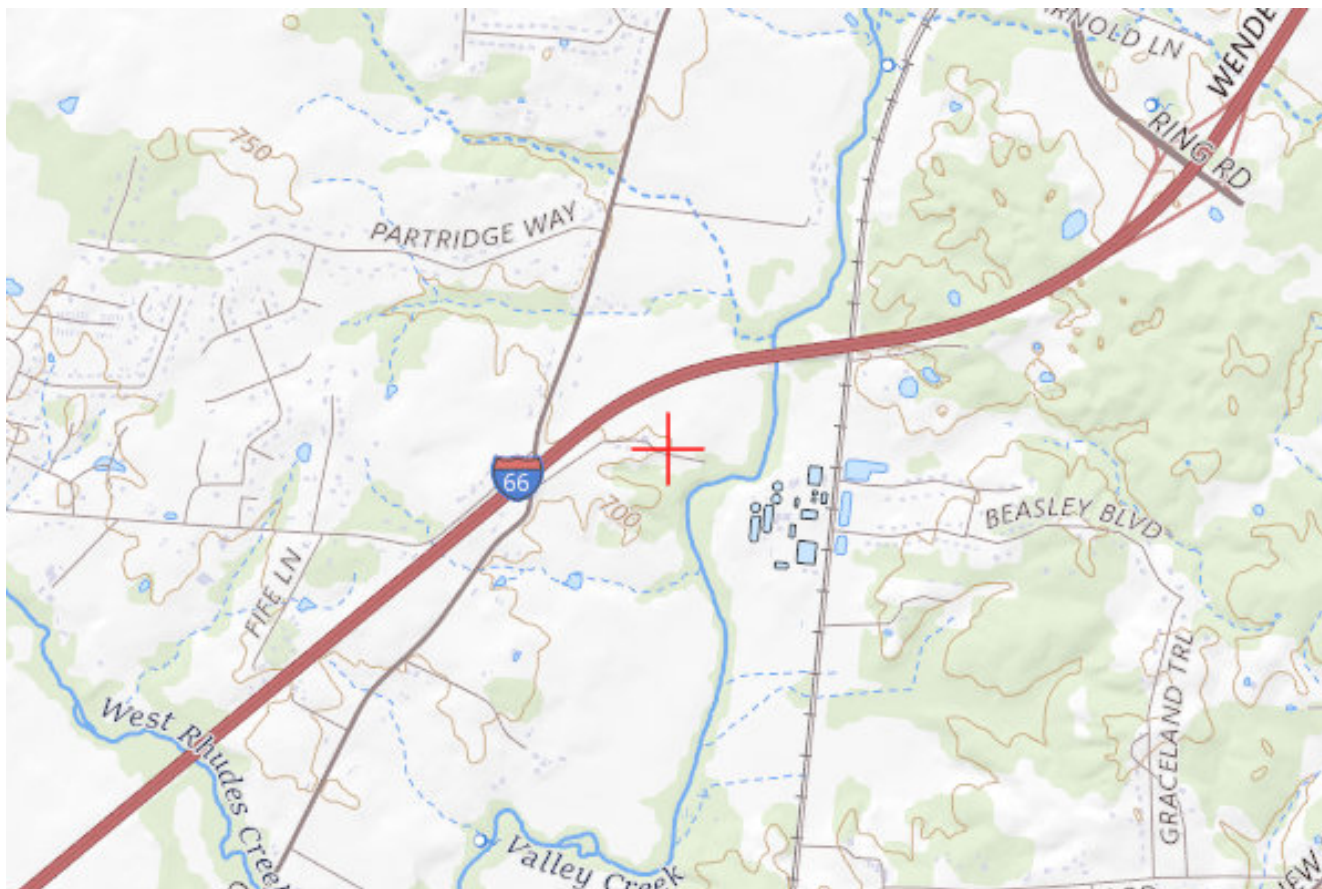
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( DNE )

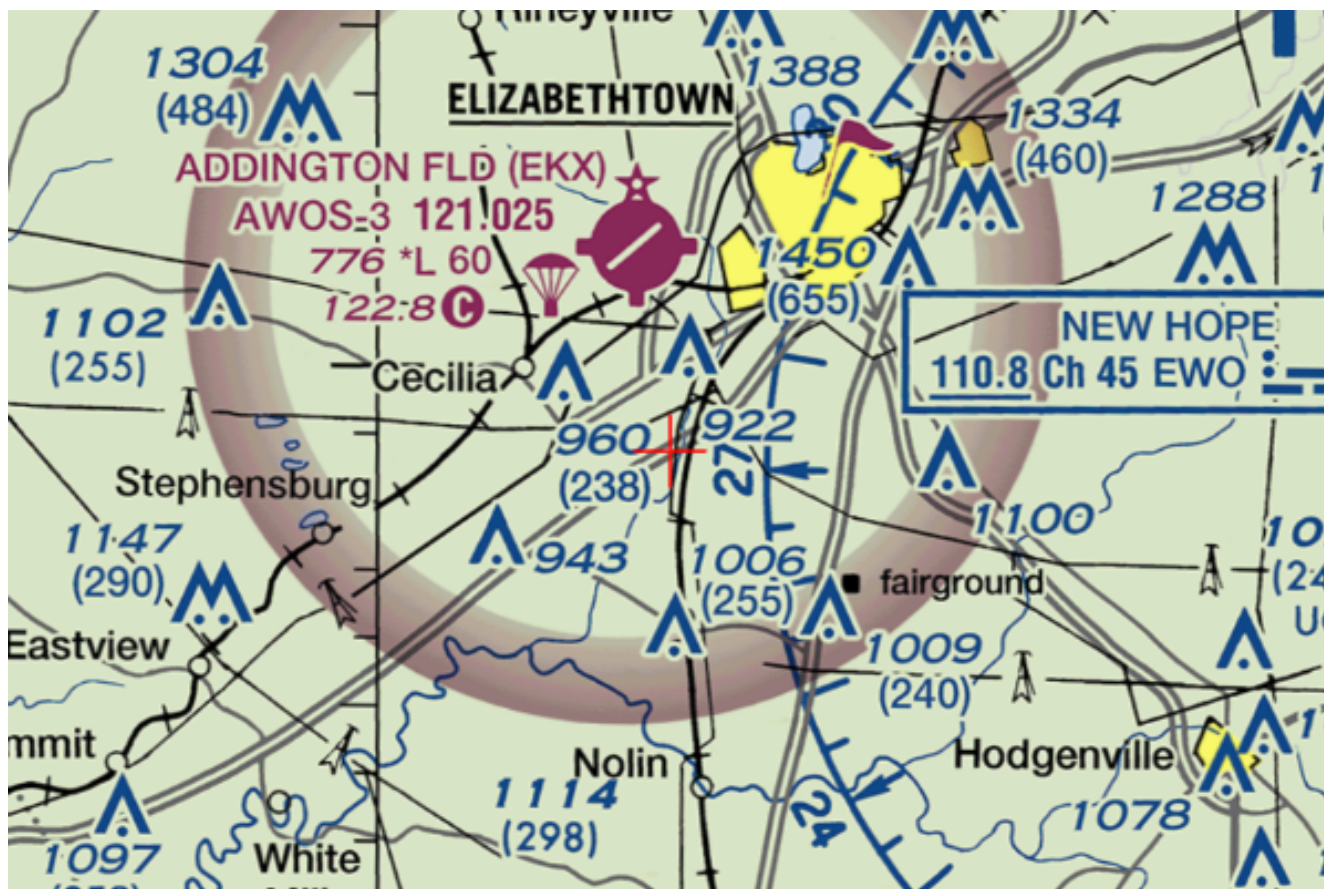
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3967-OE



Sectional Map for ASN 2022-ASO-3967-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3968-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 15
Location:	Elizabethtown, KY
Latitude:	37-38-38.77N NAD 83
Longitude:	85-54-35.61W
Heights:	692 feet site elevation (SE) 82 feet above ground level (AGL) 774 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- extended, revised, or terminated by the issuing office.
- the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3968-OE.

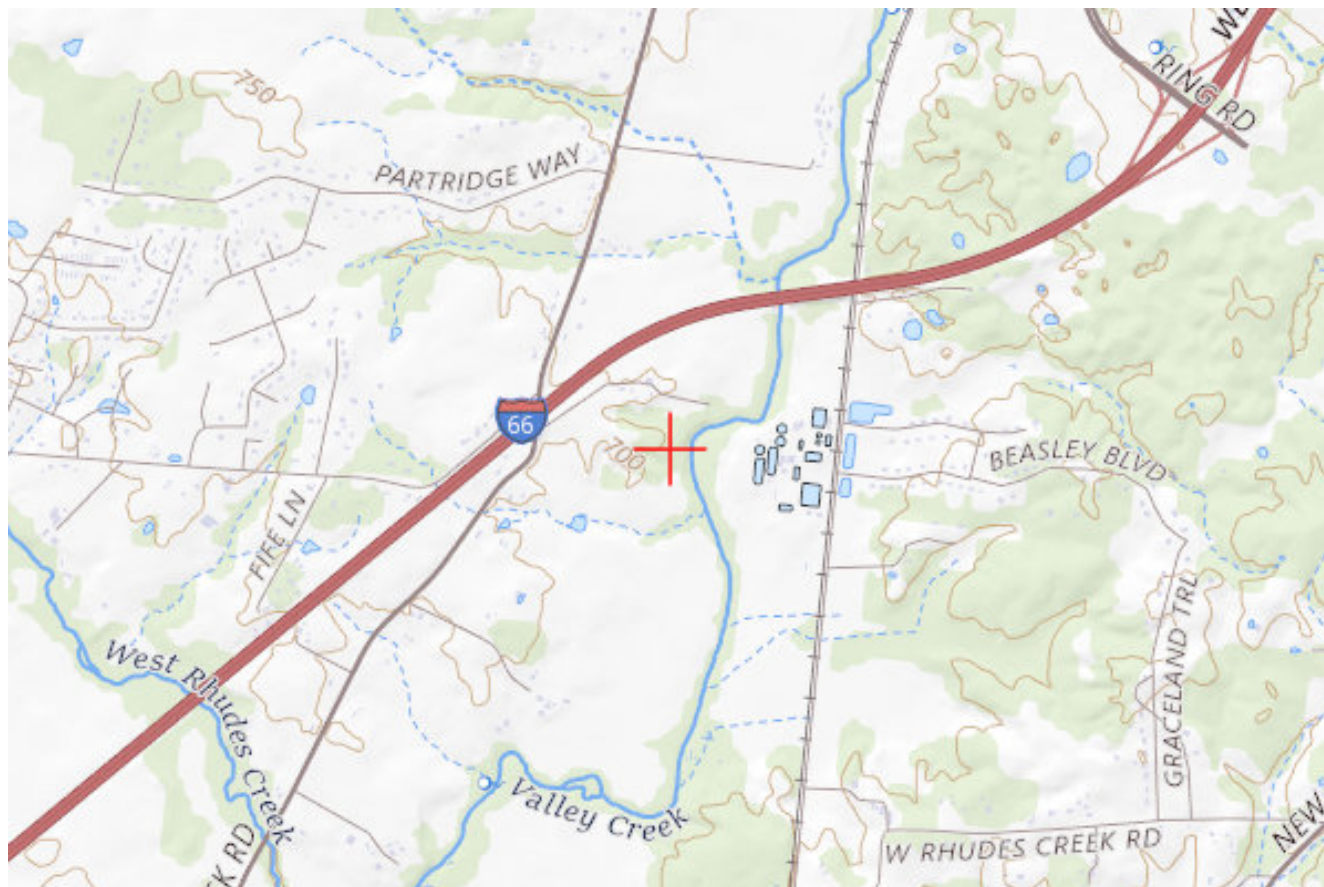
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( DNE )

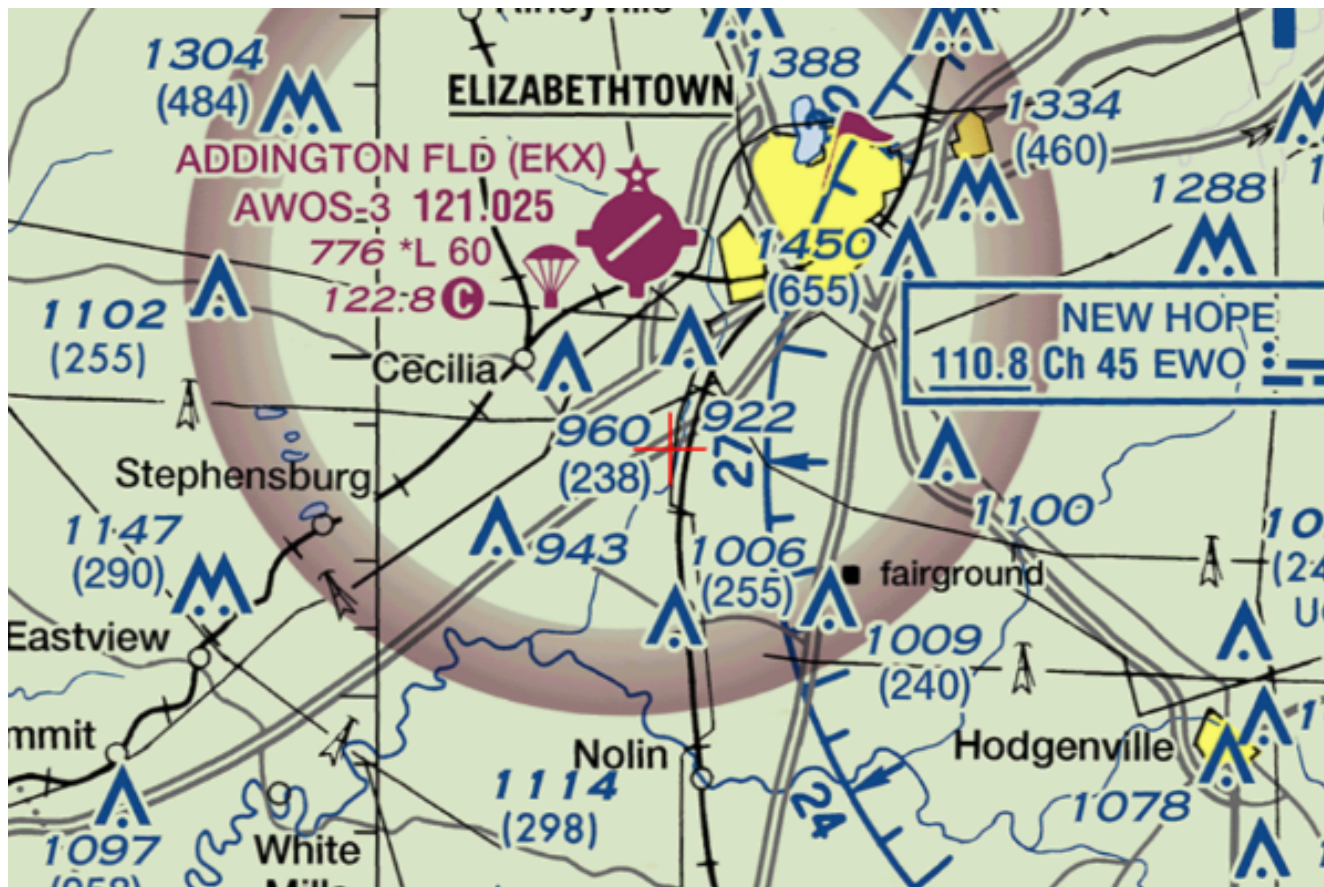
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3968-OE



Sectional Map for ASN 2022-ASO-3968-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3969-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 16
Location:	Elizabethtown, KY
Latitude:	37-38-31.30N NAD 83
Longitude:	85-54-36.01W
Heights:	673 feet site elevation (SE) 86 feet above ground level (AGL) 759 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3969-OE.

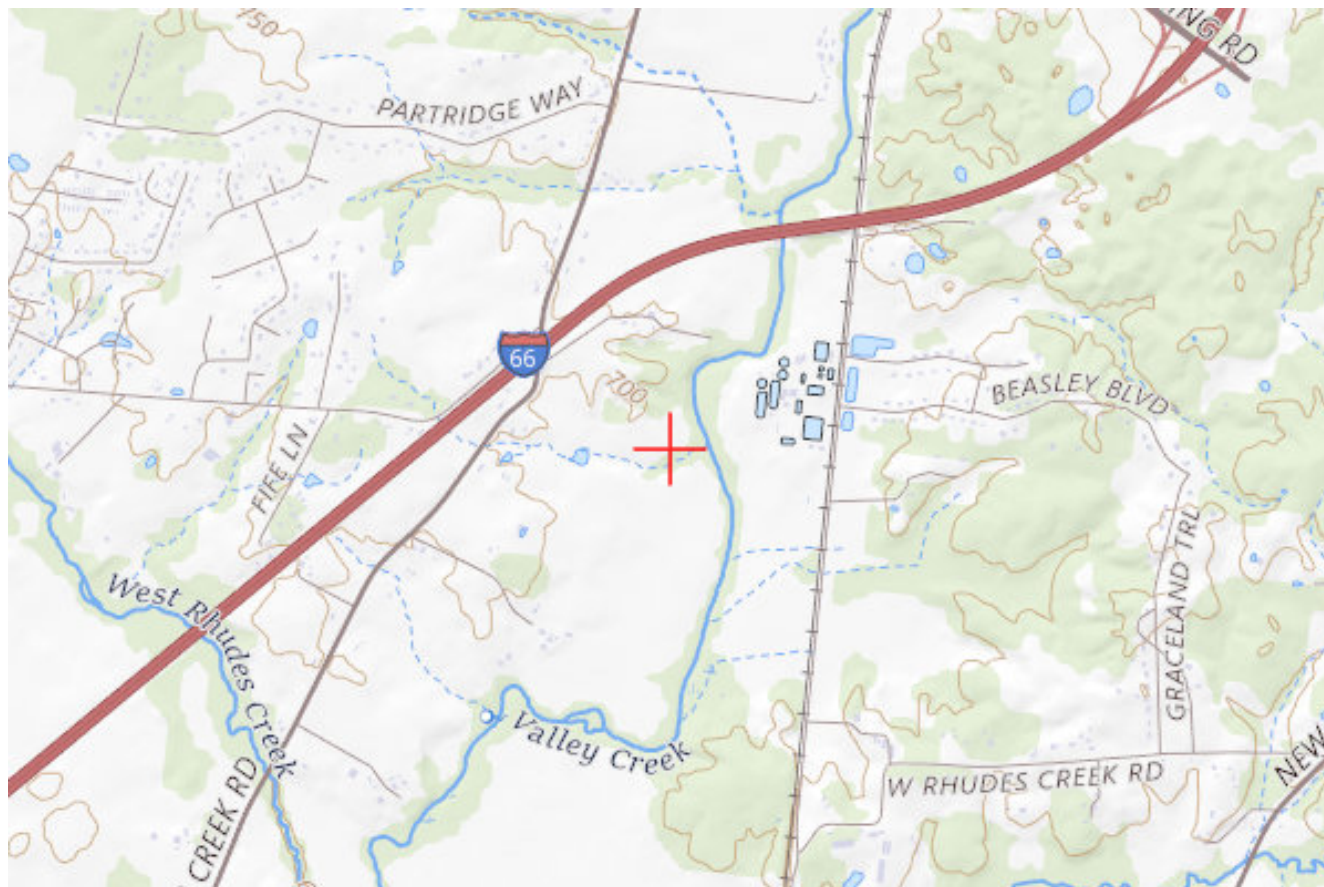
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( DNE )

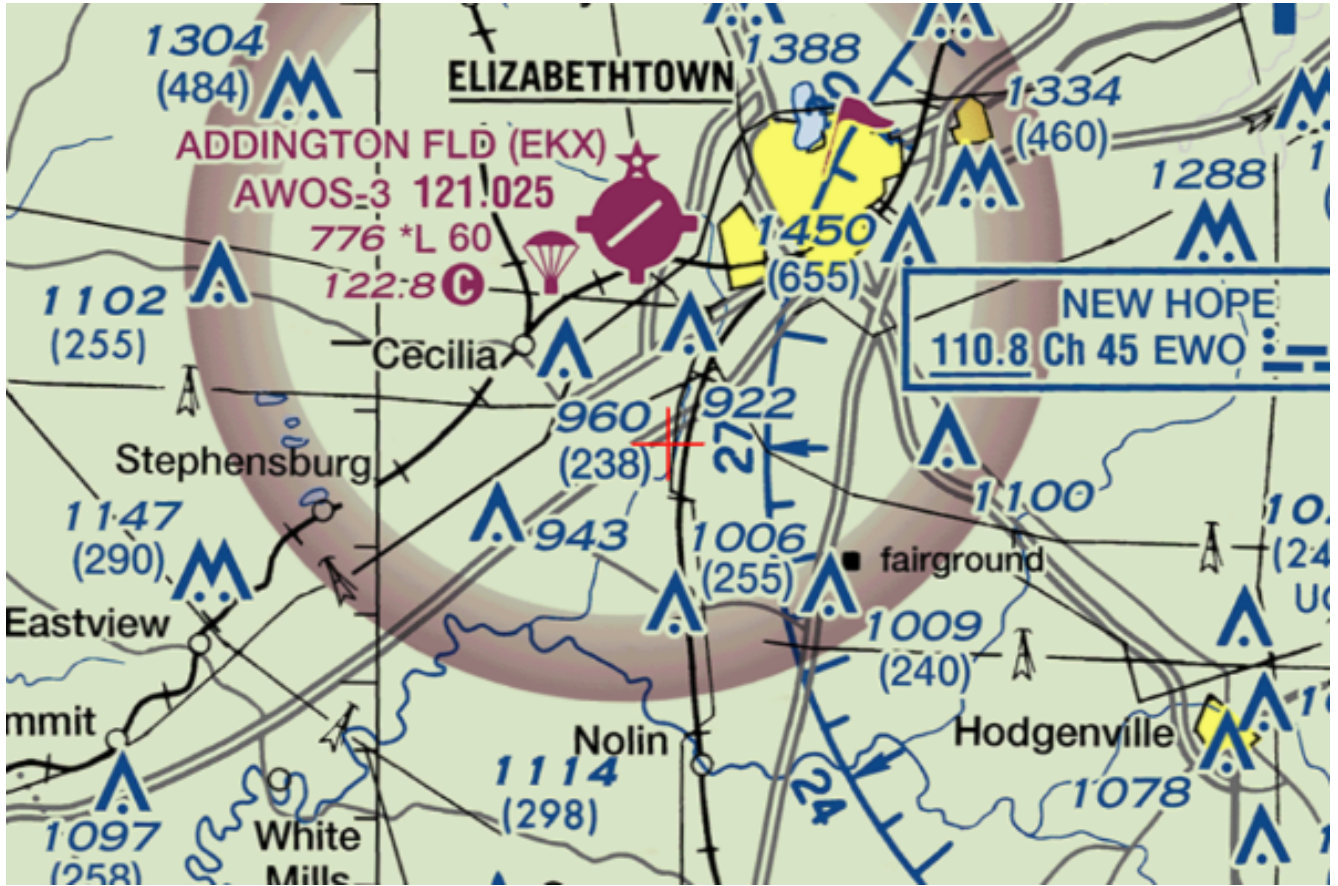
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3969-OE



Sectional Map for ASN 2022-ASO-3969-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3970-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 17
Location:	Elizabethtown, KY
Latitude:	37-38-23.67N NAD 83
Longitude:	85-54-36.42W
Heights:	671 feet site elevation (SE) 86 feet above ground level (AGL) 757 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



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If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3970-OE.

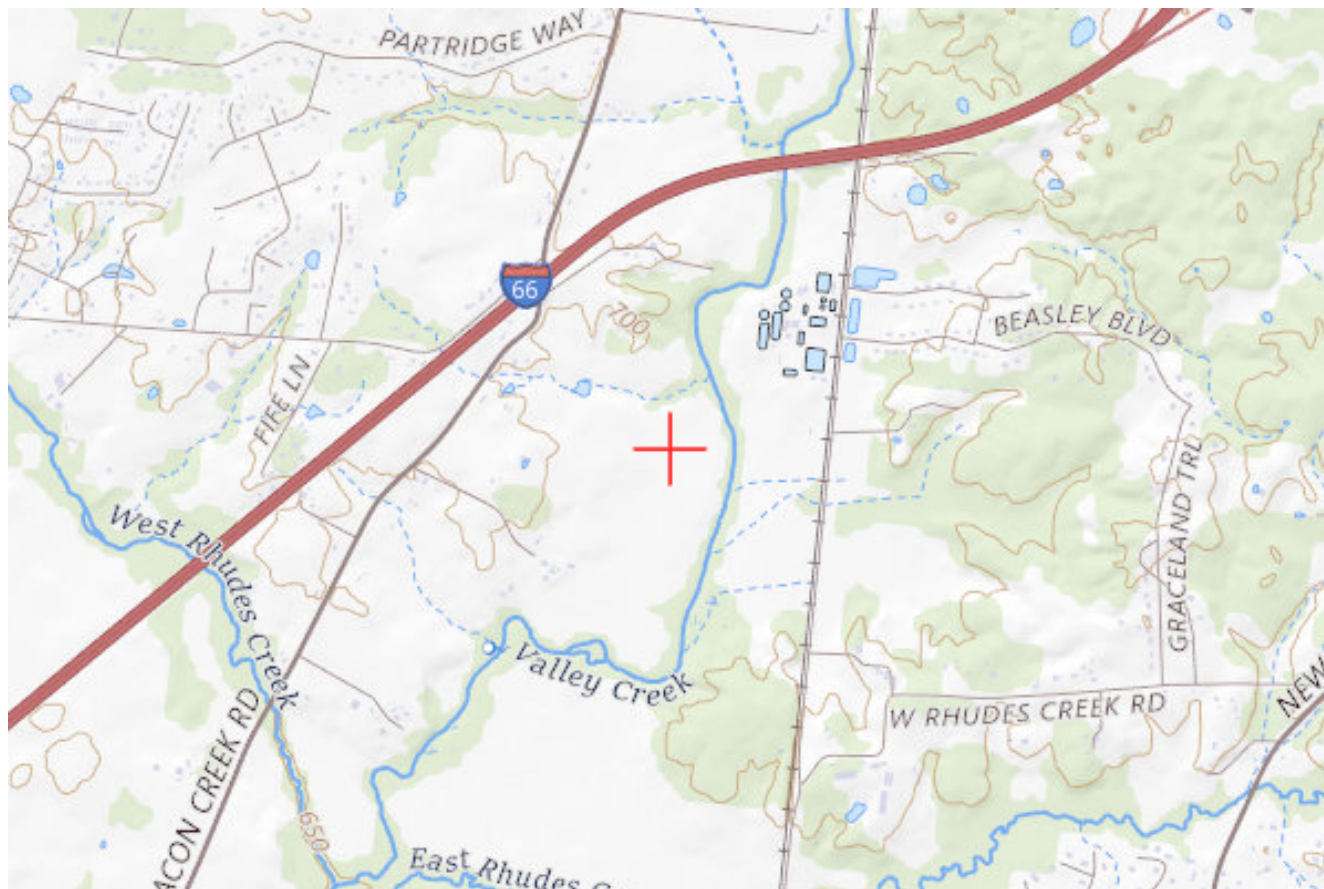
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( DNE )

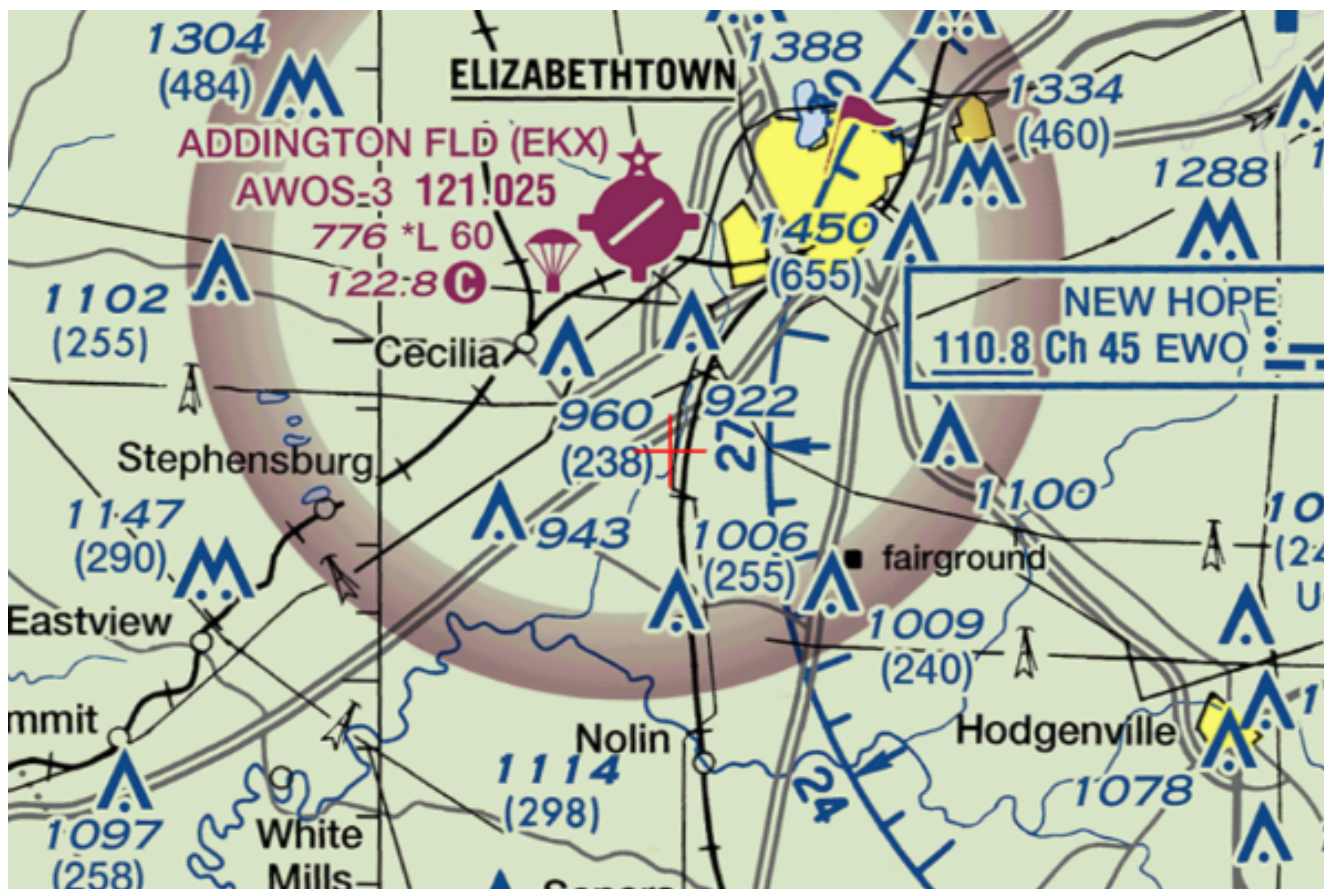
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3970-OE



Sectional Map for ASN 2022-ASO-3970-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3971-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 18
Location:	Elizabethtown, KY
Latitude:	37-38-16.75N NAD 83
Longitude:	85-54-36.79W
Heights:	667 feet site elevation (SE) 86 feet above ground level (AGL) 753 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3971-OE.

**Signature Control No: 510156548-520362052**

( DNE )

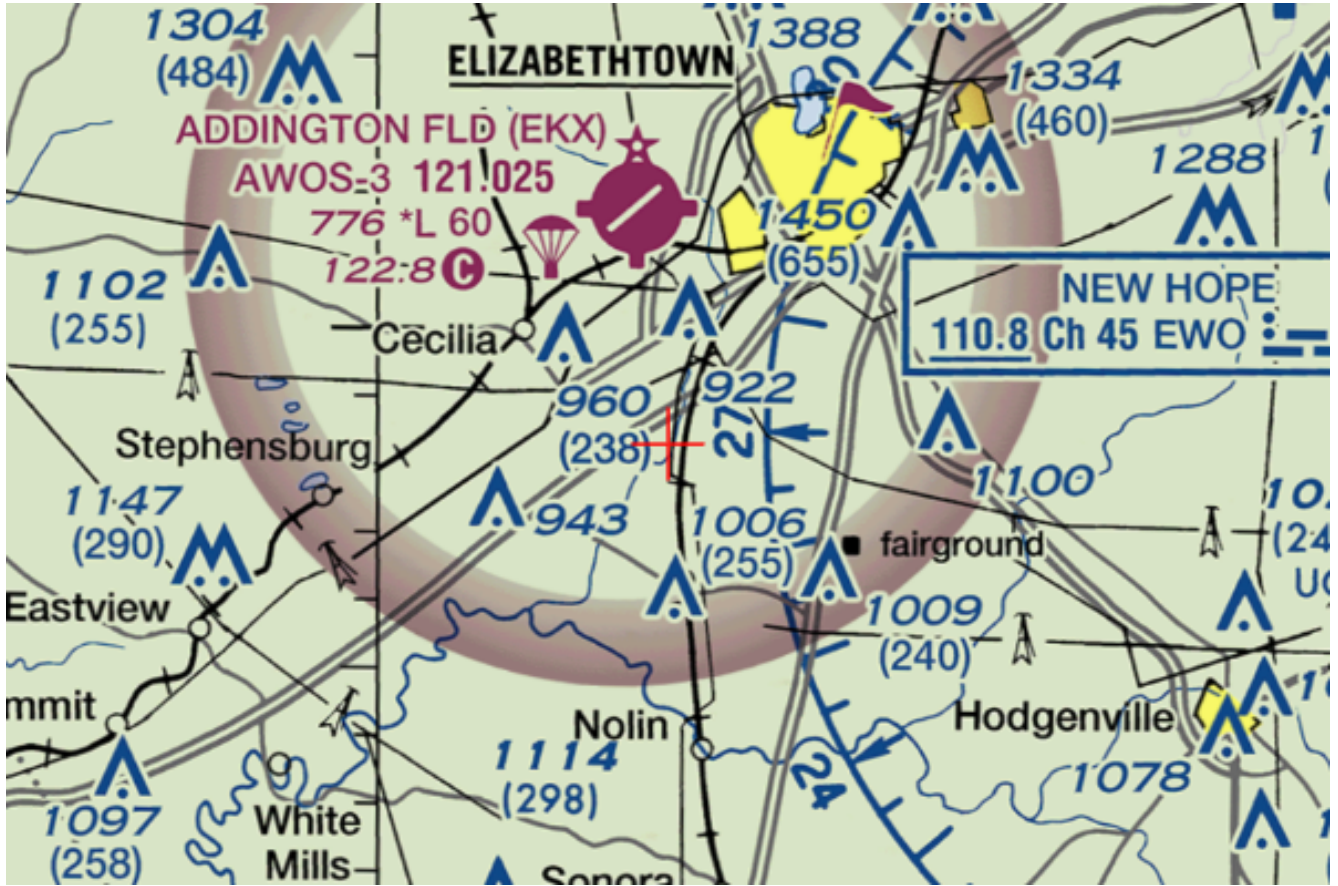
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3971-OE



Sectional Map for ASN 2022-ASO-3971-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3972-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 19
Location:	Elizabethtown, KY
Latitude:	37-38-09.83N NAD 83
Longitude:	85-54-37.16W
Heights:	661 feet site elevation (SE) 86 feet above ground level (AGL) 747 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3972-OE.

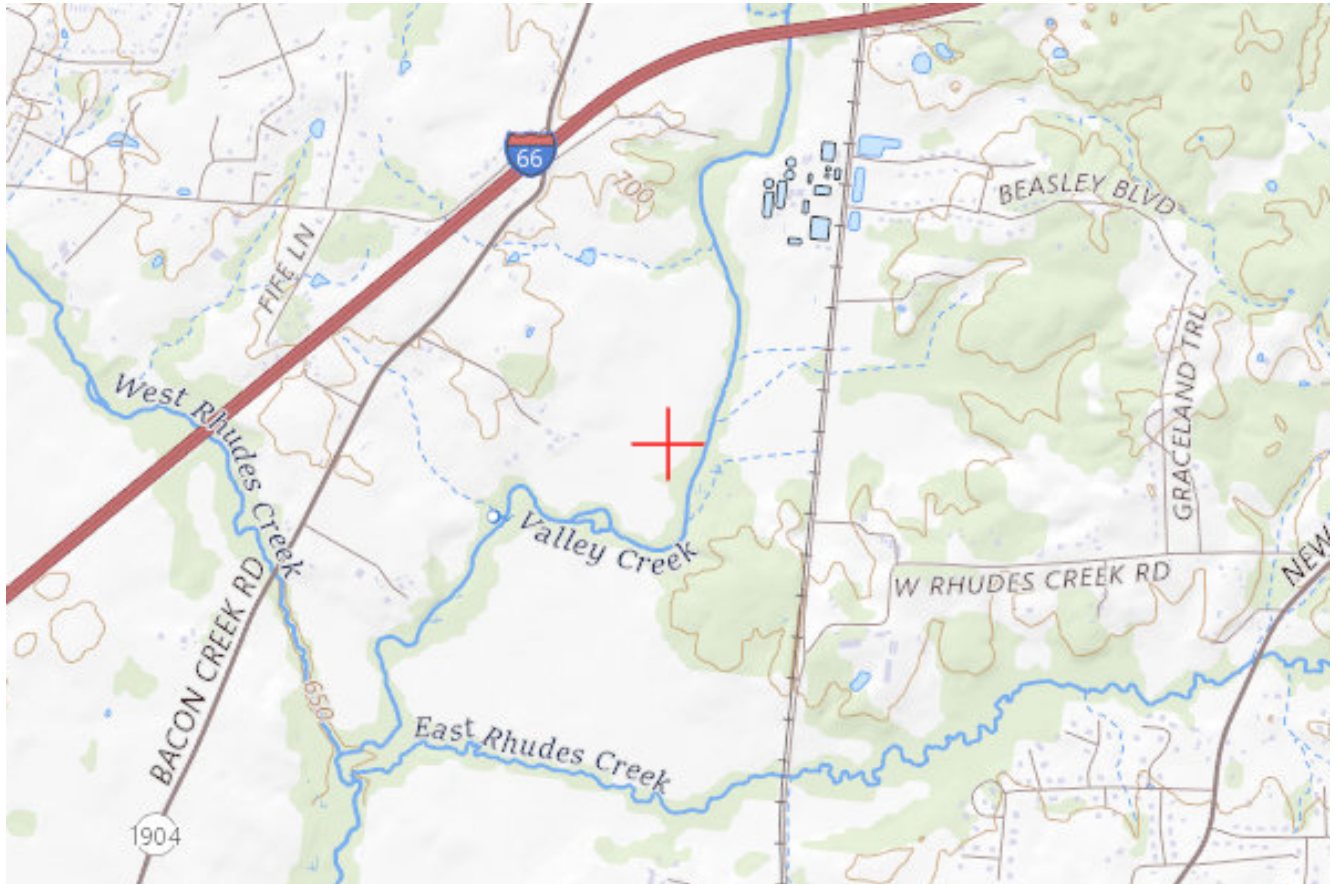
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( DNE )

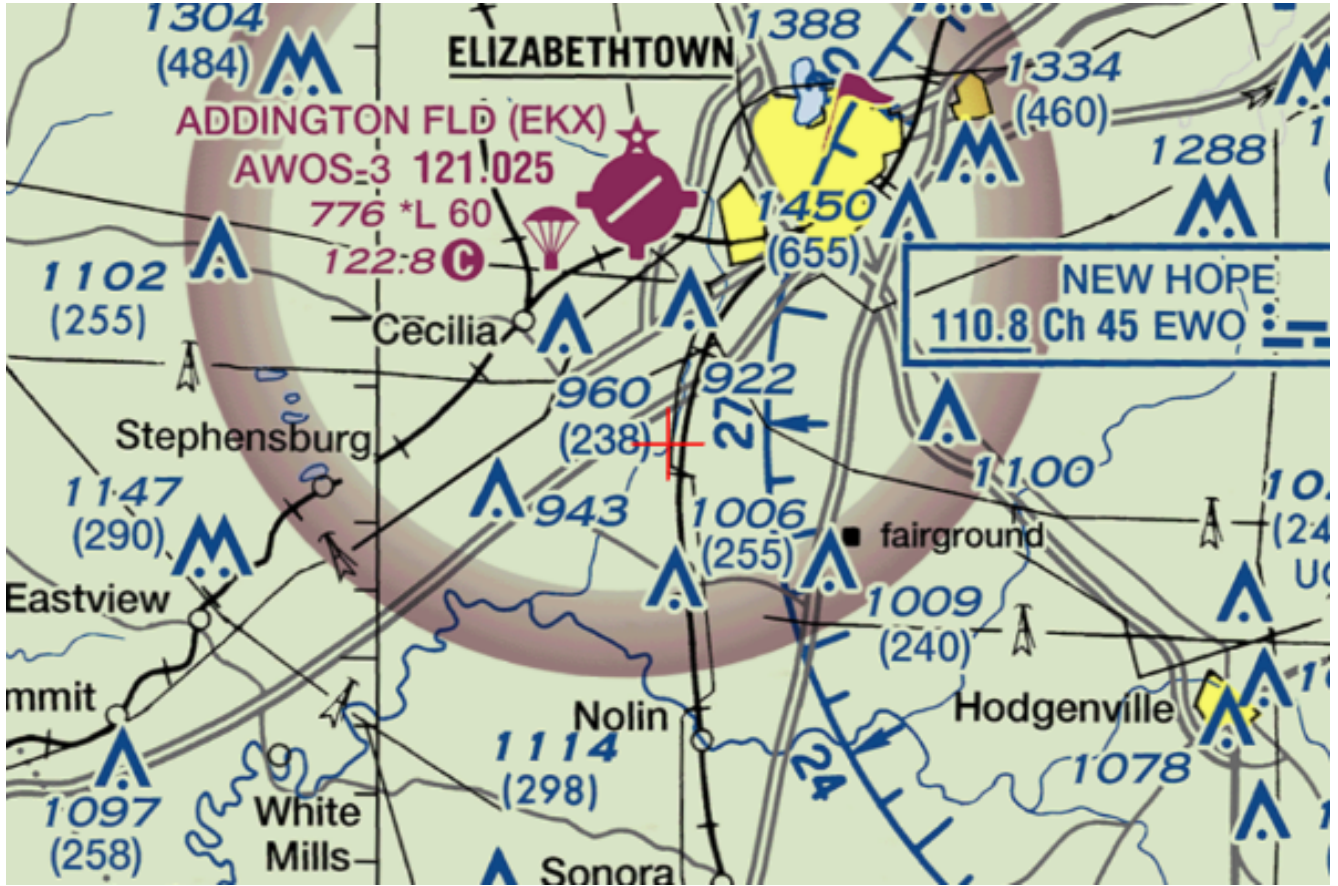
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3972-OE



Sectional Map for ASN 2022-ASO-3972-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3973-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 20
Location:	Elizabethtown, KY
Latitude:	37-38-02.20N NAD 83
Longitude:	85-54-37.57W
Heights:	660 feet site elevation (SE) 86 feet above ground level (AGL) 746 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3973-OE.

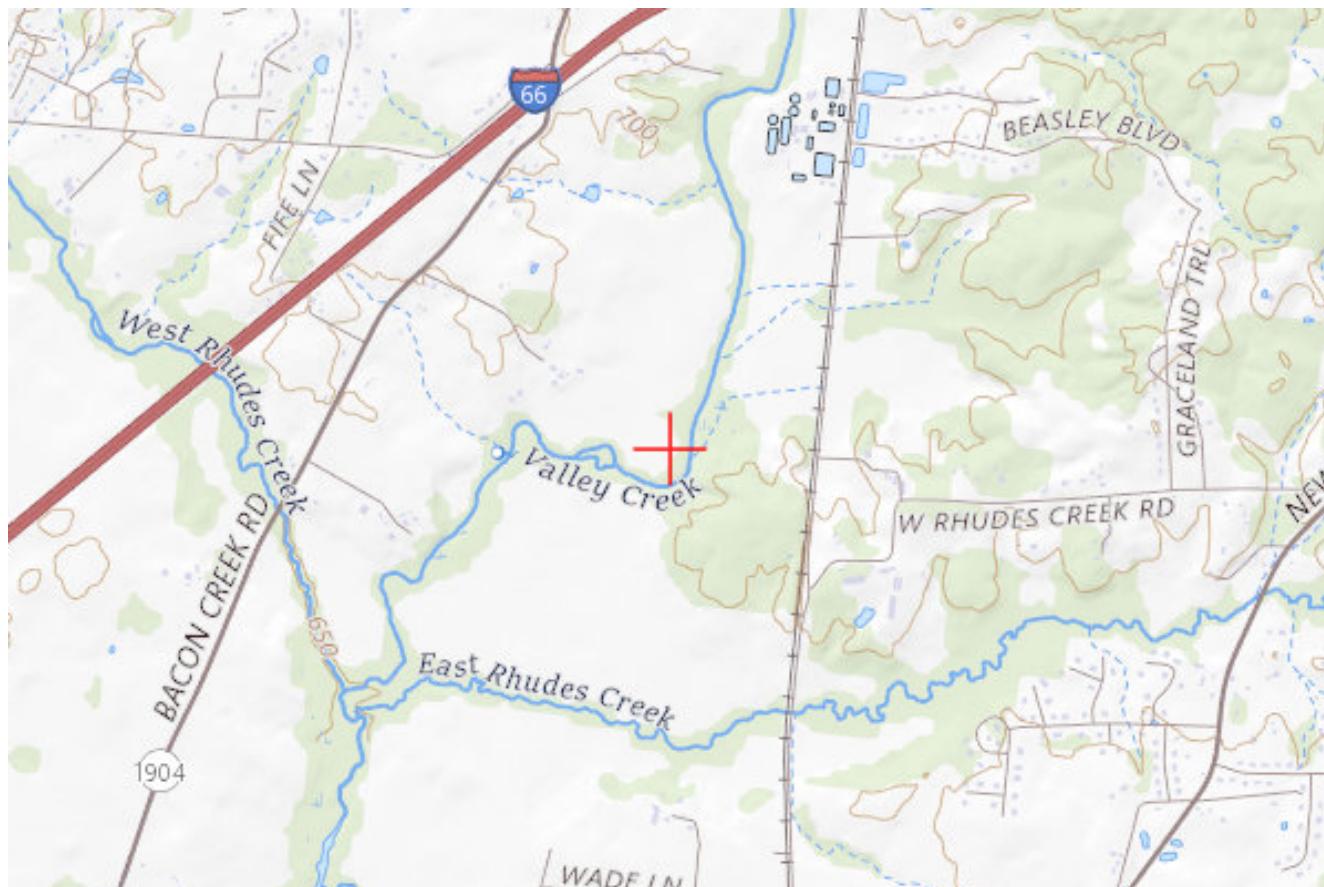
**Signature Control No: 510156550-520362069**

( DNE )

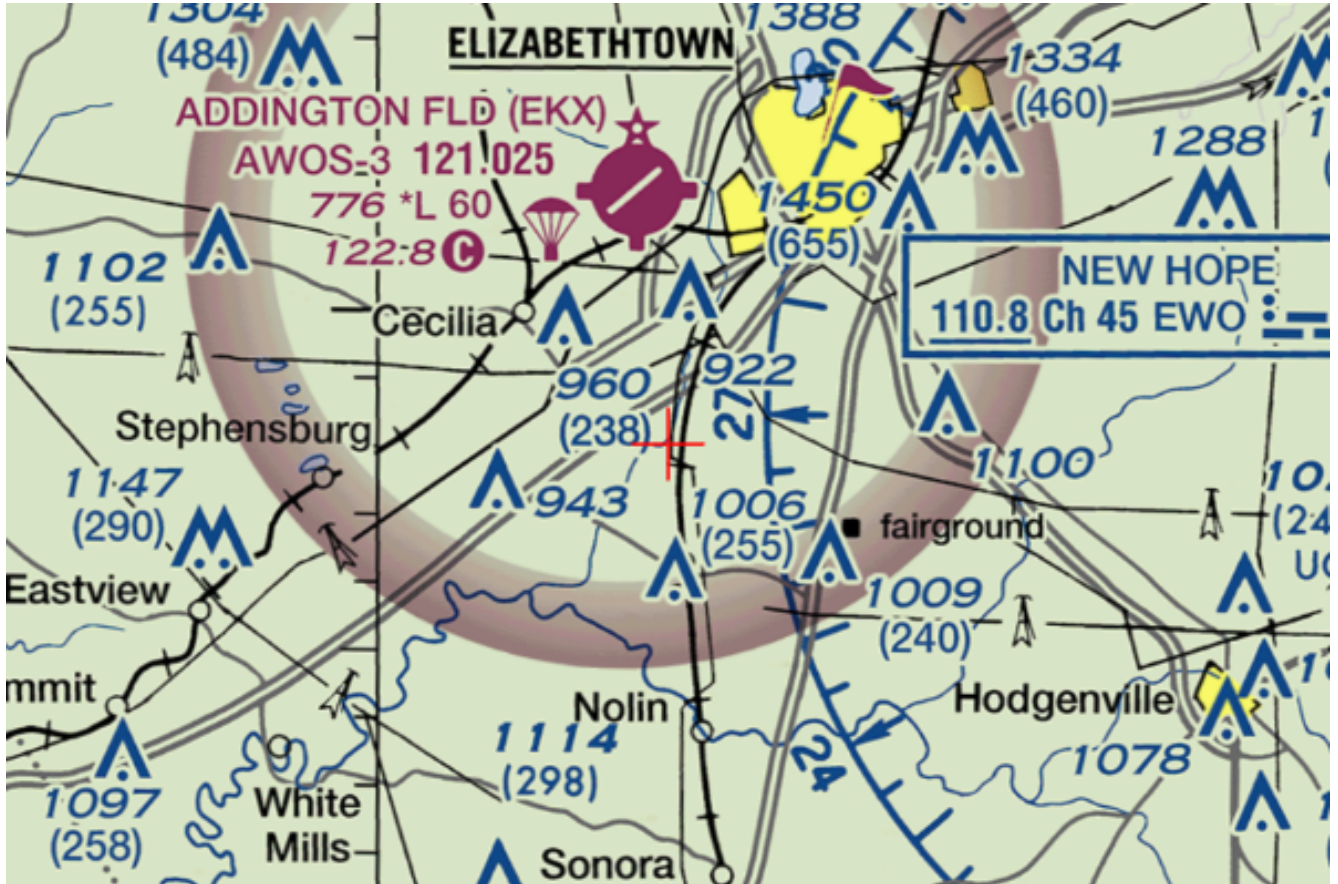
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3973-OE



Sectional Map for ASN 2022-ASO-3973-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3977-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 21
Location:	Elizabethtown, KY
Latitude:	37-37-54.45N NAD 83
Longitude:	85-54-37.99W
Heights:	689 feet site elevation (SE) 86 feet above ground level (AGL) 775 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3977-OE.

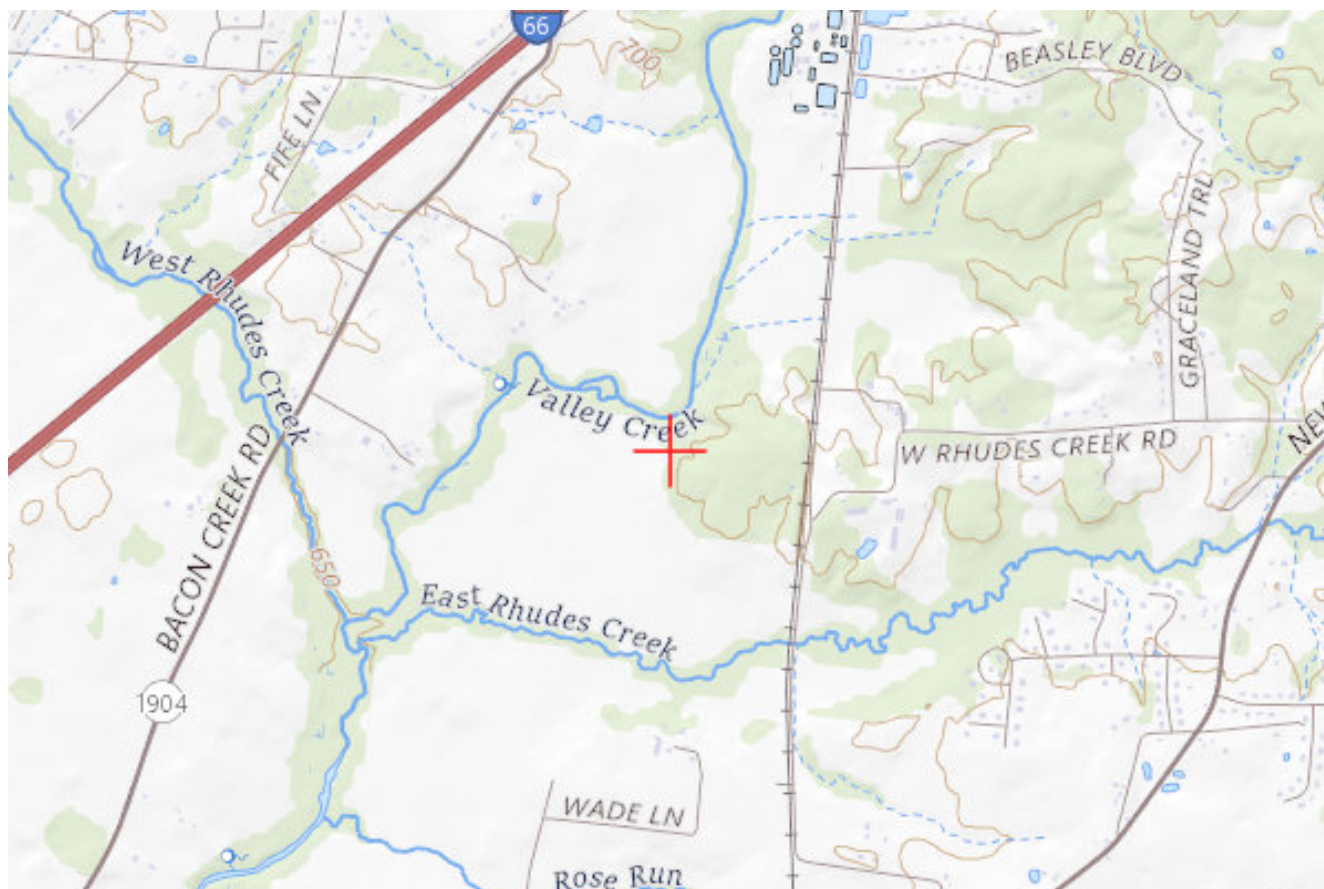
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( DNE )

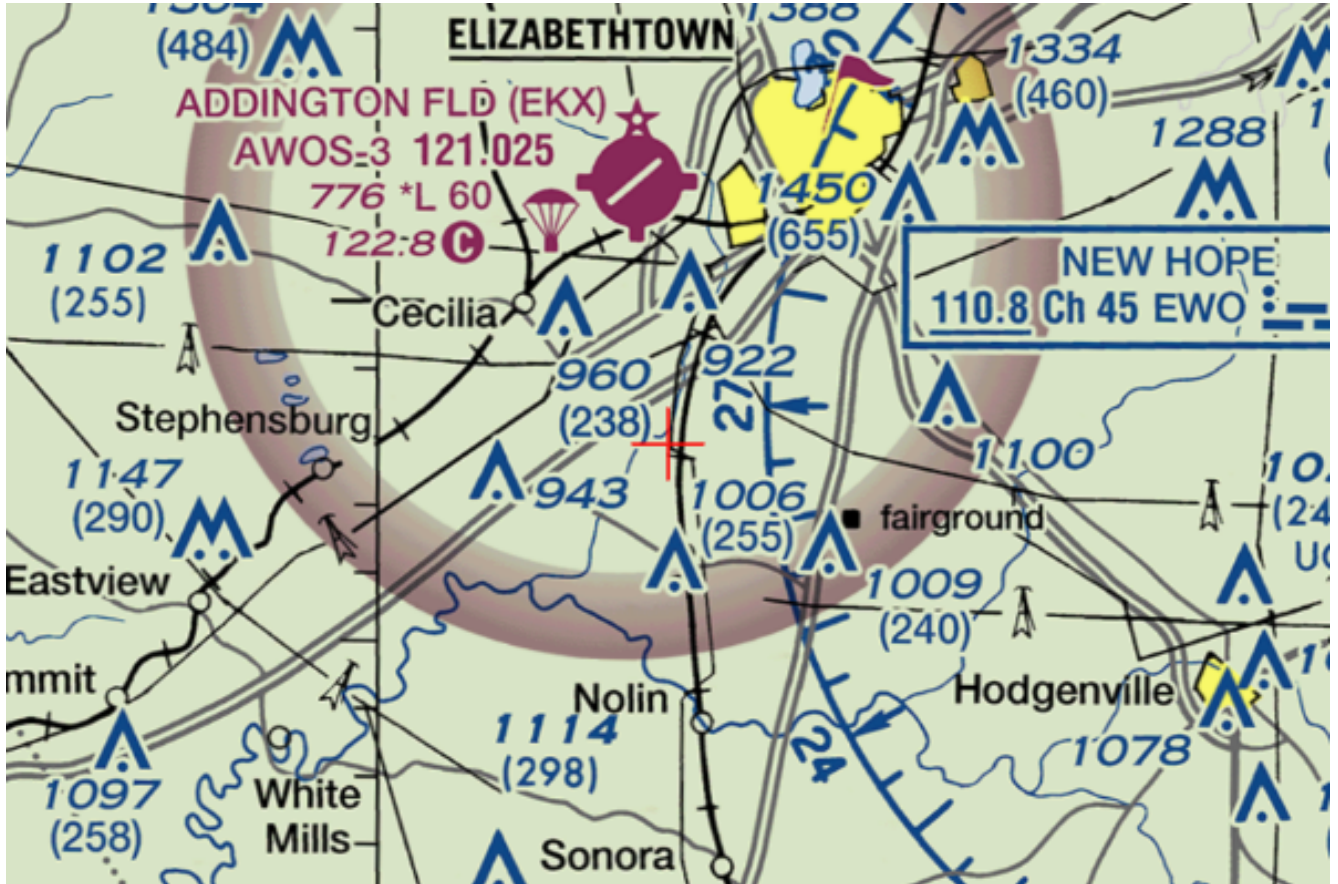
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3977-OE



Sectional Map for ASN 2022-ASO-3977-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3978-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 22
Location:	Elizabethtown, KY
Latitude:	37-37-50.53N NAD 83
Longitude:	85-54-38.20W
Heights:	690 feet site elevation (SE) 95 feet above ground level (AGL) 785 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3978-OE.

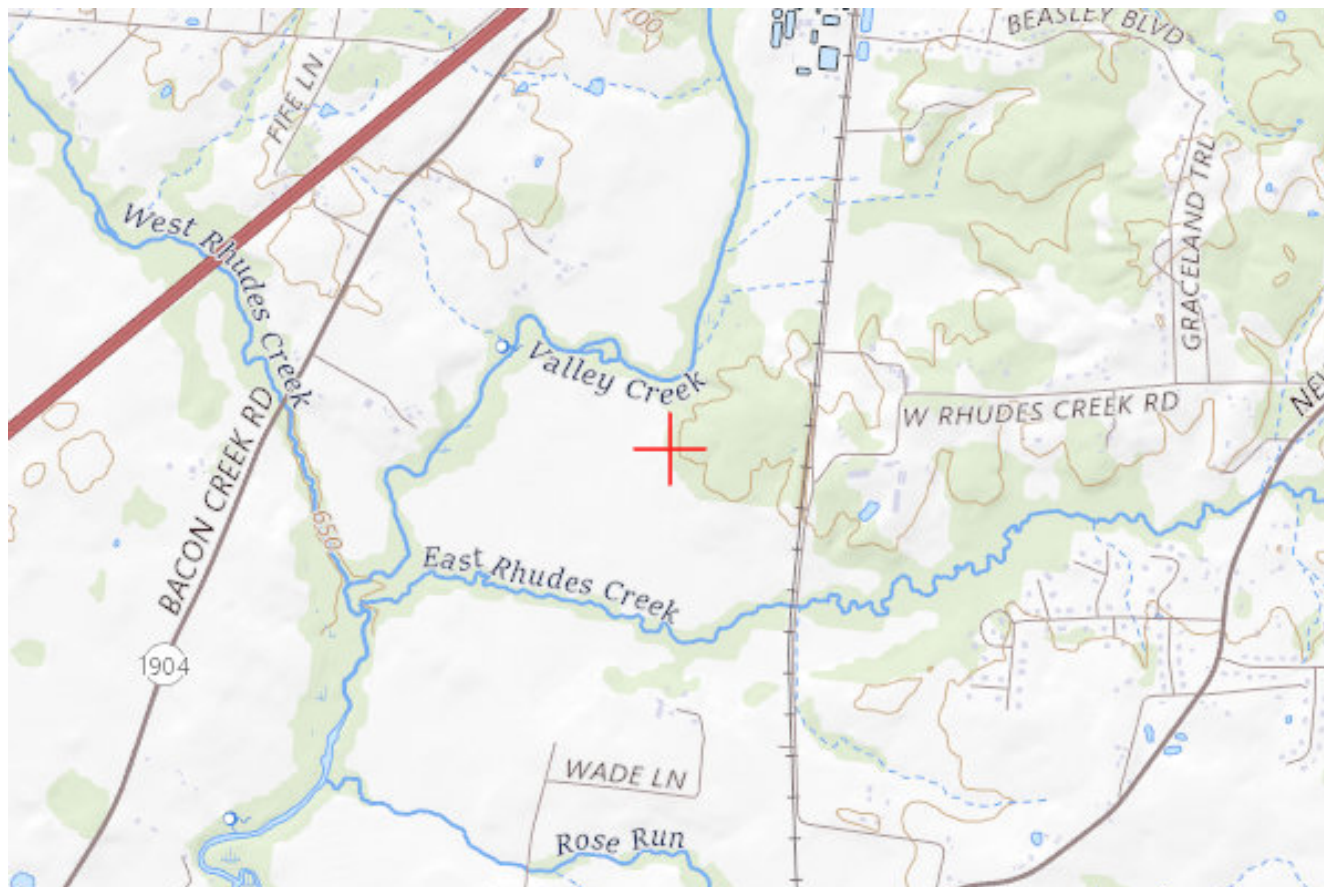
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( DNE )

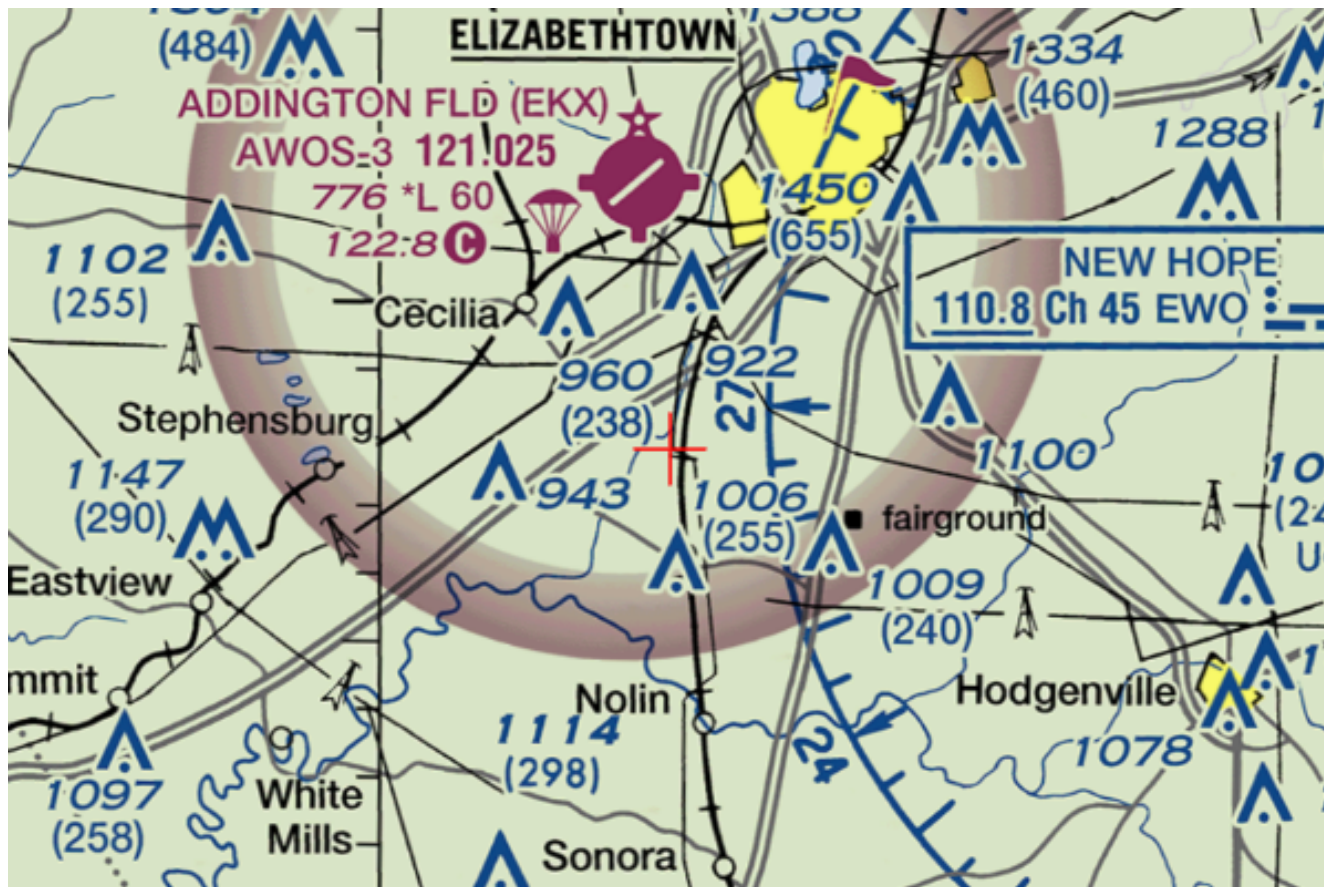
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3978-OE



Sectional Map for ASN 2022-ASO-3978-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3979-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 23
Location:	Elizabethtown, KY
Latitude:	37-37-42.54N NAD 83
Longitude:	85-54-33.29W
Heights:	687 feet site elevation (SE) 91 feet above ground level (AGL) 778 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3979-OE.

**Signature Control No: 510170911-520362612**

( DNE )

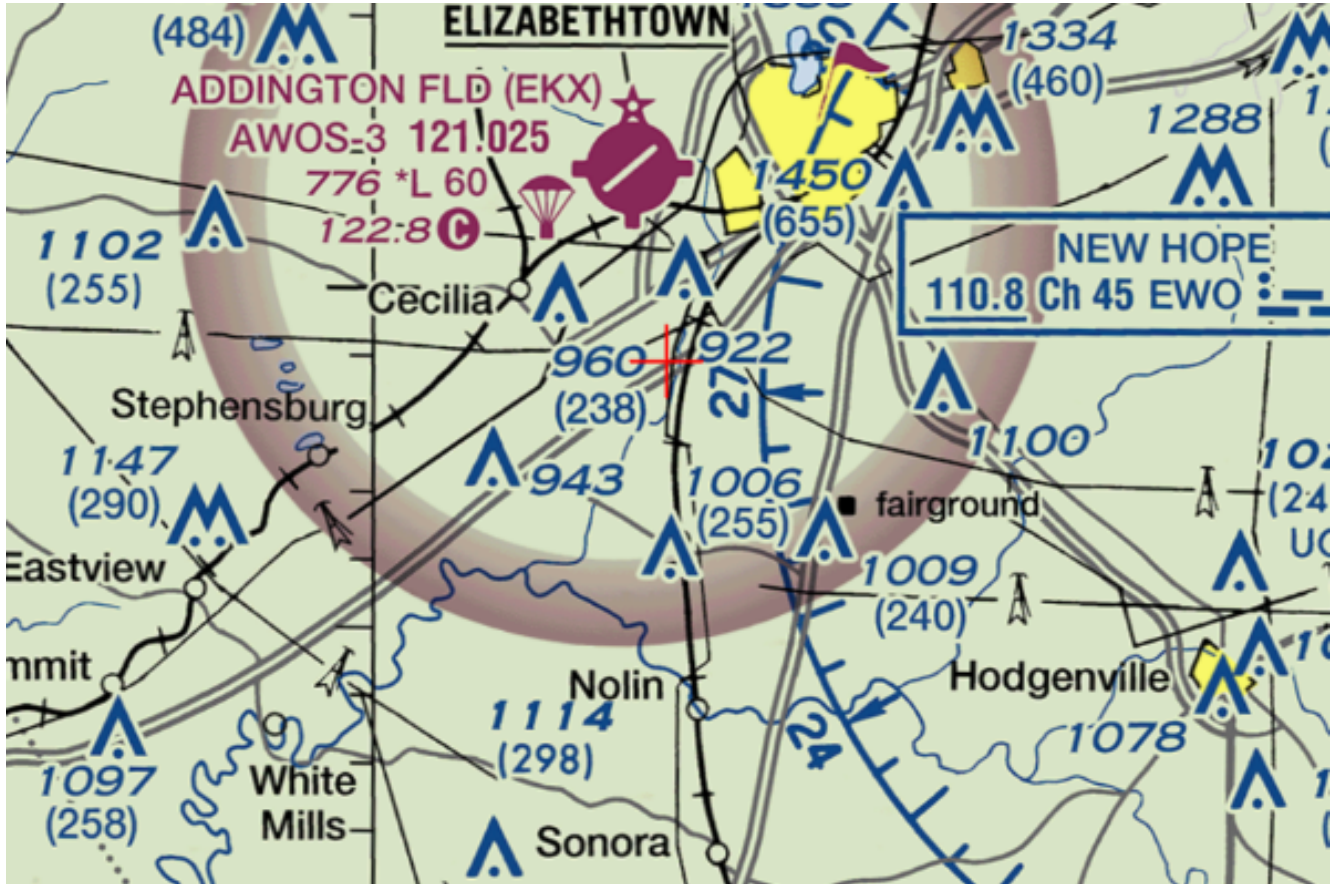
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3979-OE



Sectional Map for ASN 2022-ASO-3979-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3980-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 24
Location:	Elizabethtown, KY
Latitude:	37-37-34.44N NAD 83
Longitude:	85-54-28.31W
Heights:	665 feet site elevation (SE) 91 feet above ground level (AGL) 756 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3980-OE.

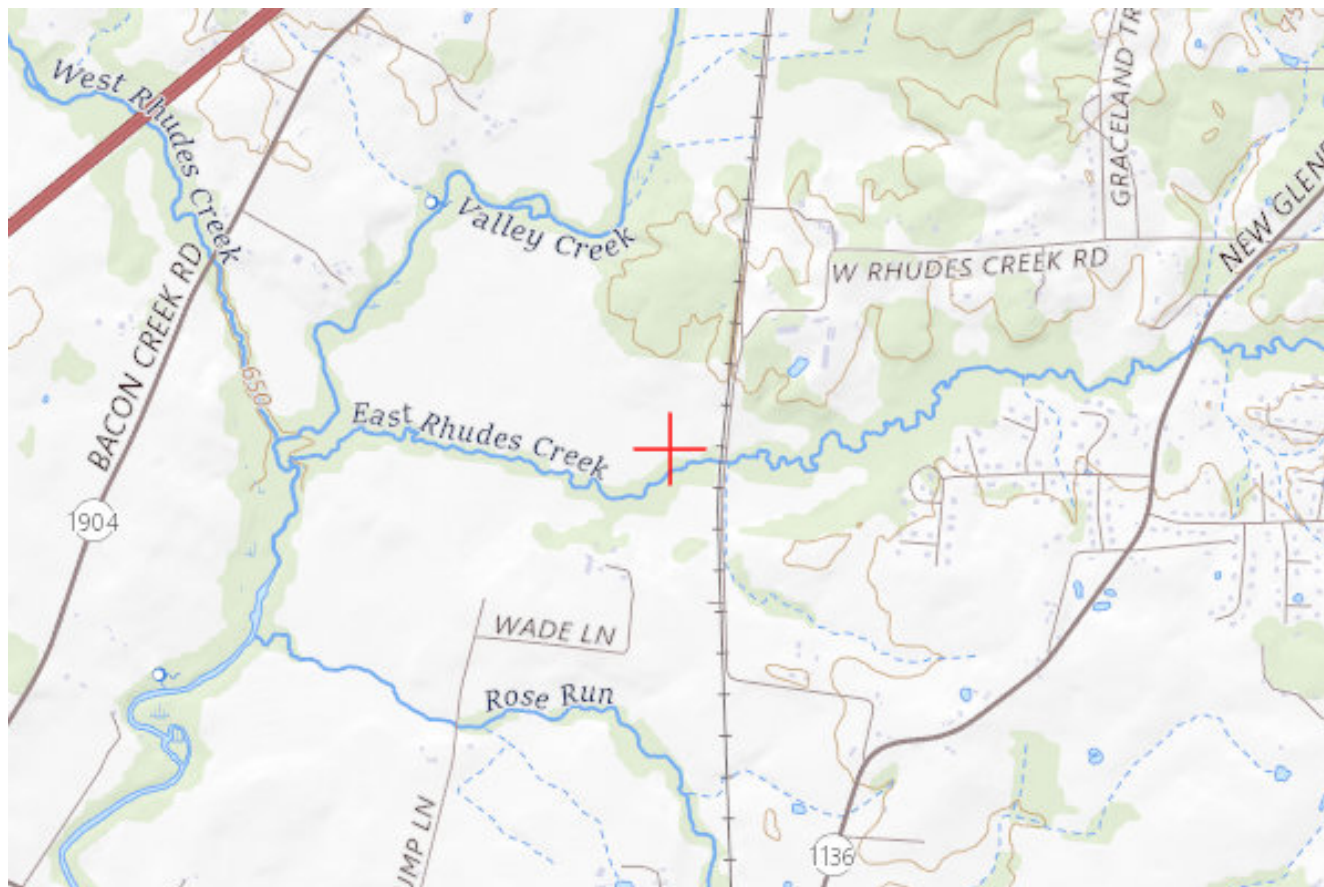
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( DNE )

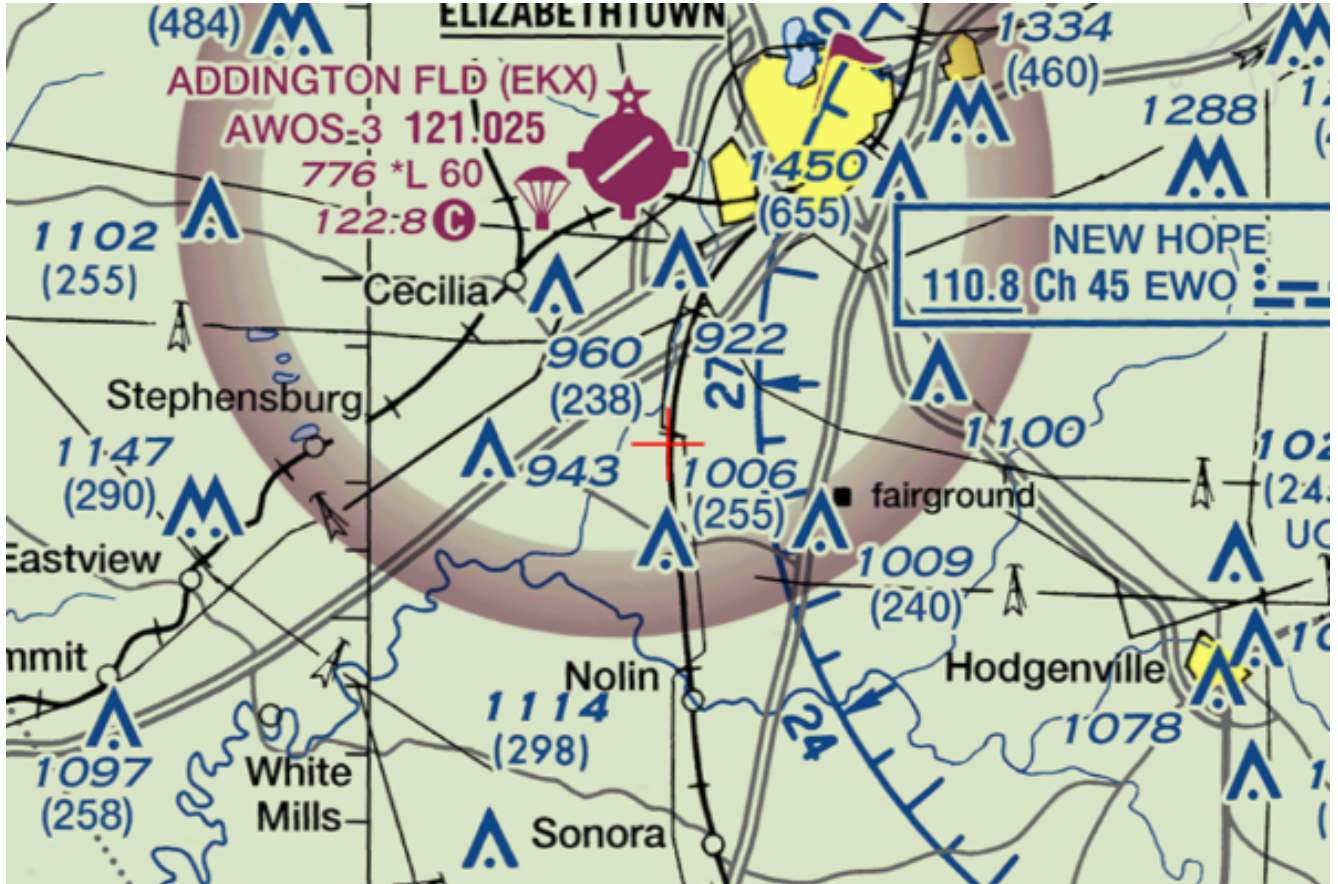
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3980-OE



Sectional Map for ASN 2022-ASO-3980-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3981-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 25
Location:	Elizabethtown, KY
Latitude:	37-37-25.69N NAD 83
Longitude:	85-54-22.94W
Heights:	664 feet site elevation (SE) 115 feet above ground level (AGL) 779 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



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If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3981-OE.

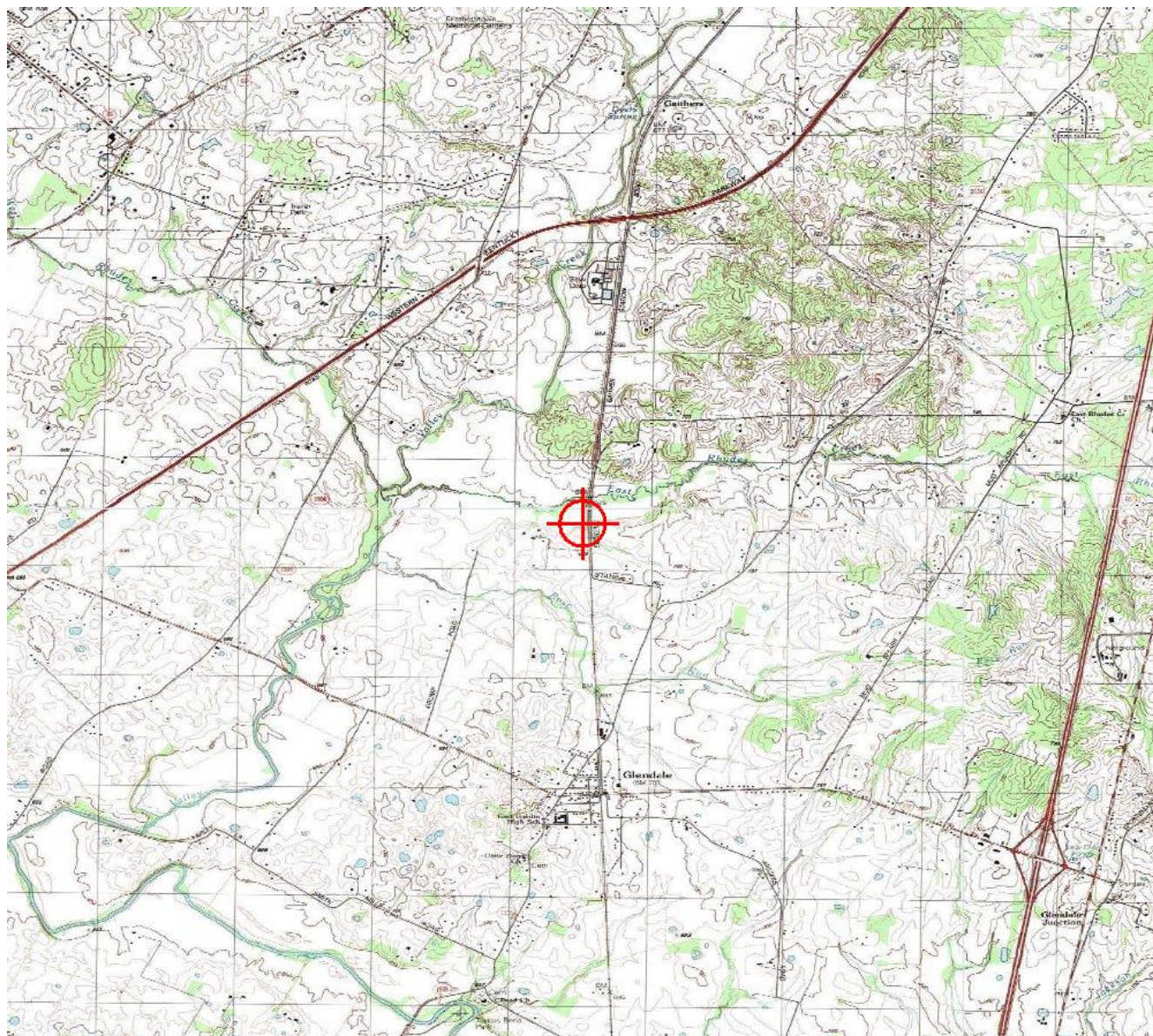
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( DNE )

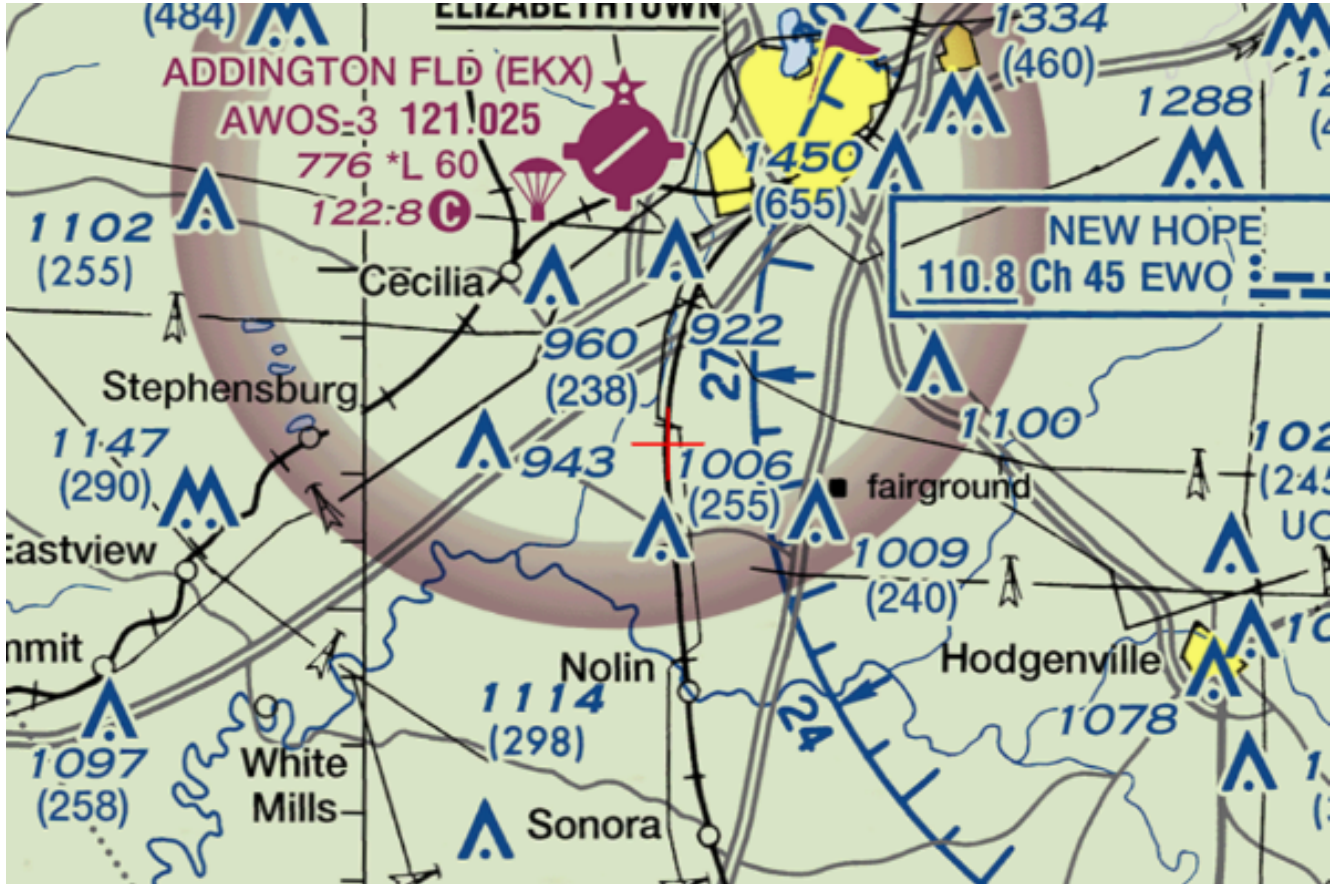
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3981-OE



Sectional Map for ASN 2022-ASO-3981-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3982-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 26
Location:	Elizabethtown, KY
Latitude:	37-37-23.59N NAD 83
Longitude:	85-54-18.91W
Heights:	688 feet site elevation (SE) 105 feet above ground level (AGL) 793 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3982-OE.

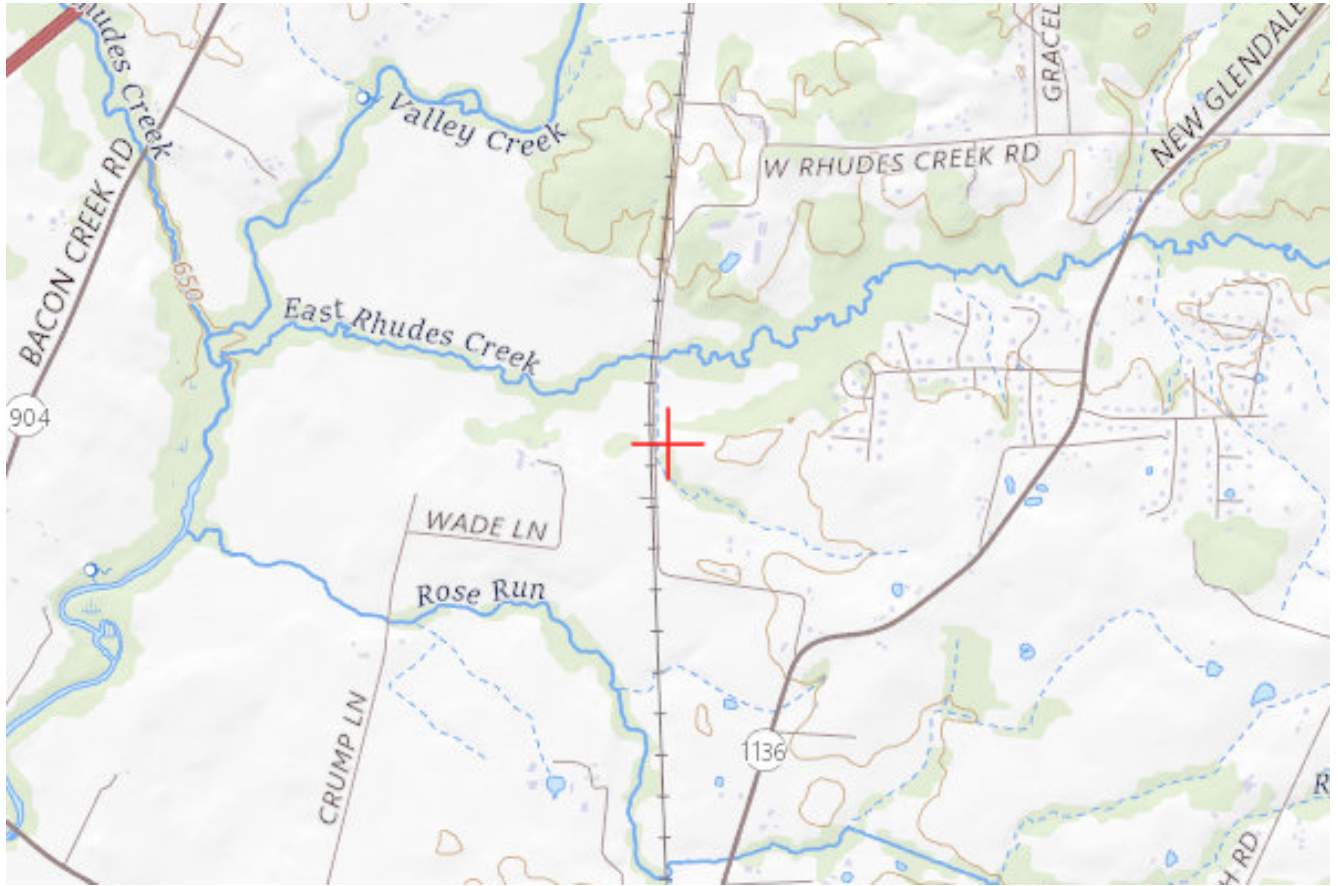
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( DNE )

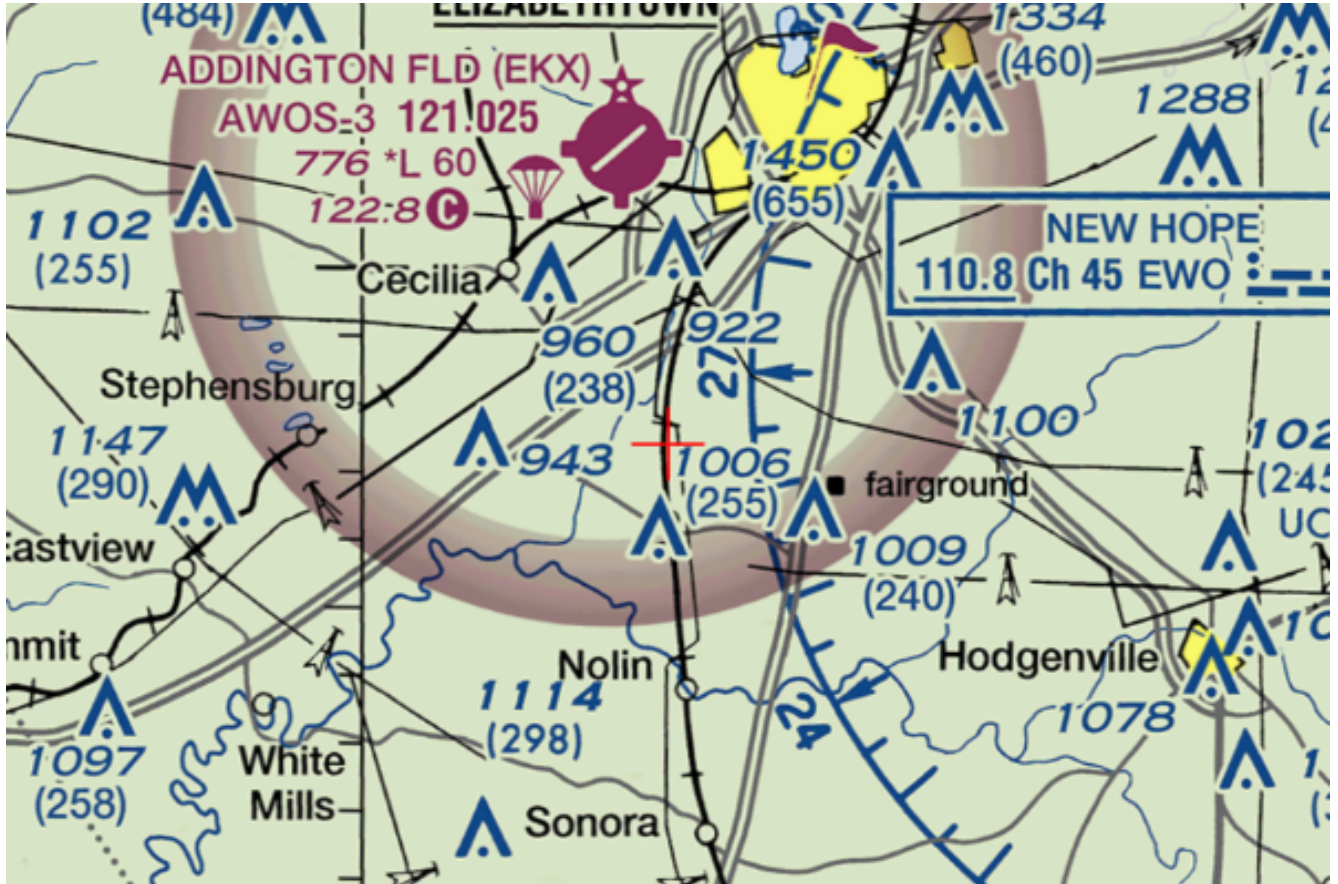
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3982-OE



Sectional Map for ASN 2022-ASO-3982-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3983-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 27
Location:	Elizabethtown, KY
Latitude:	37-37-16.66N NAD 83
Longitude:	85-54-18.60W
Heights:	689 feet site elevation (SE) 86 feet above ground level (AGL) 775 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
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**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3983-OE.

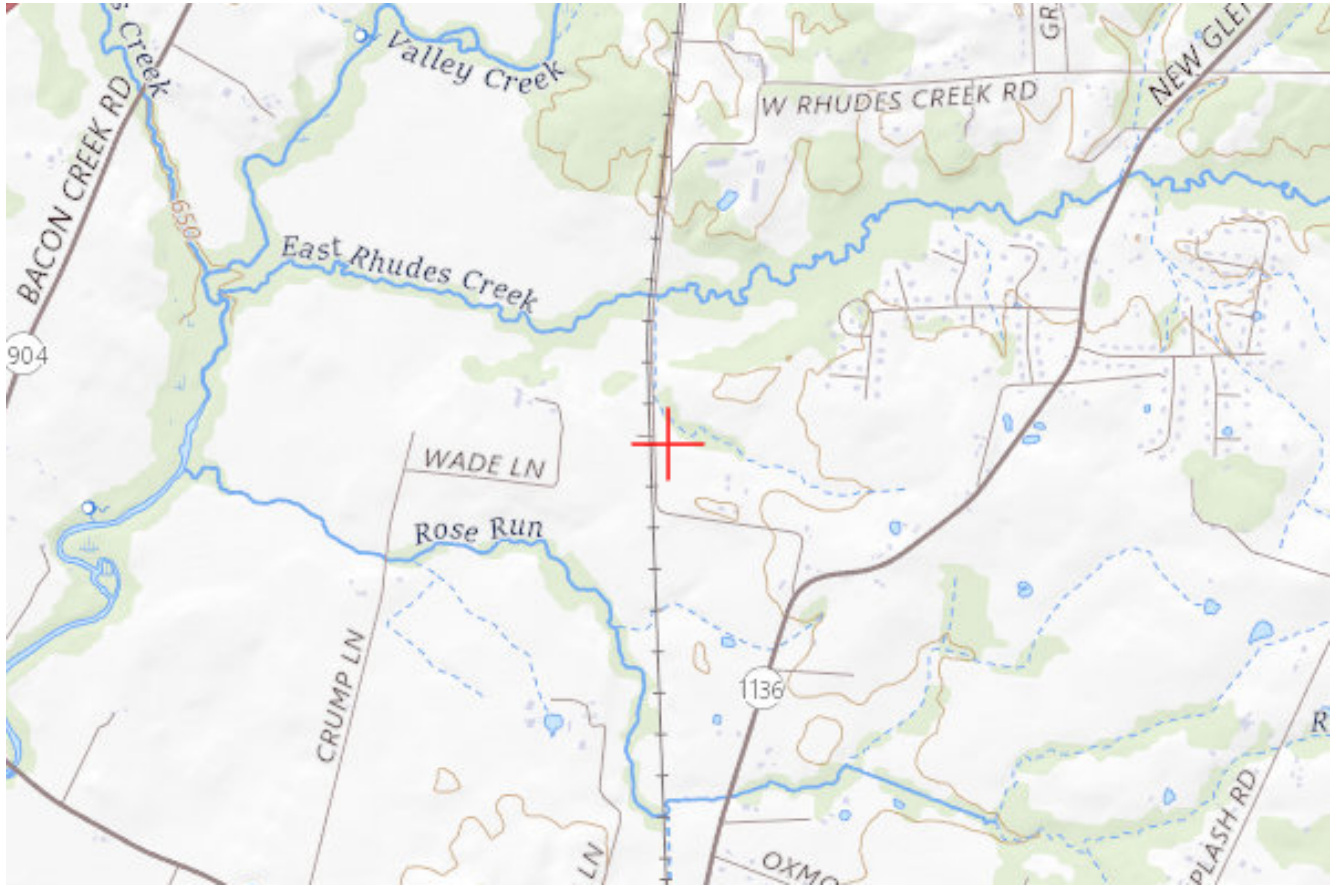
**Signature Control No: 510170916-520362620**

( DNE )

Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3983-OE







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3984-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 28
Location:	Elizabethtown, KY
Latitude:	37-37-09.98N NAD 83
Longitude:	85-54-18.30W
Heights:	691 feet site elevation (SE) 86 feet above ground level (AGL) 777 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3984-OE.

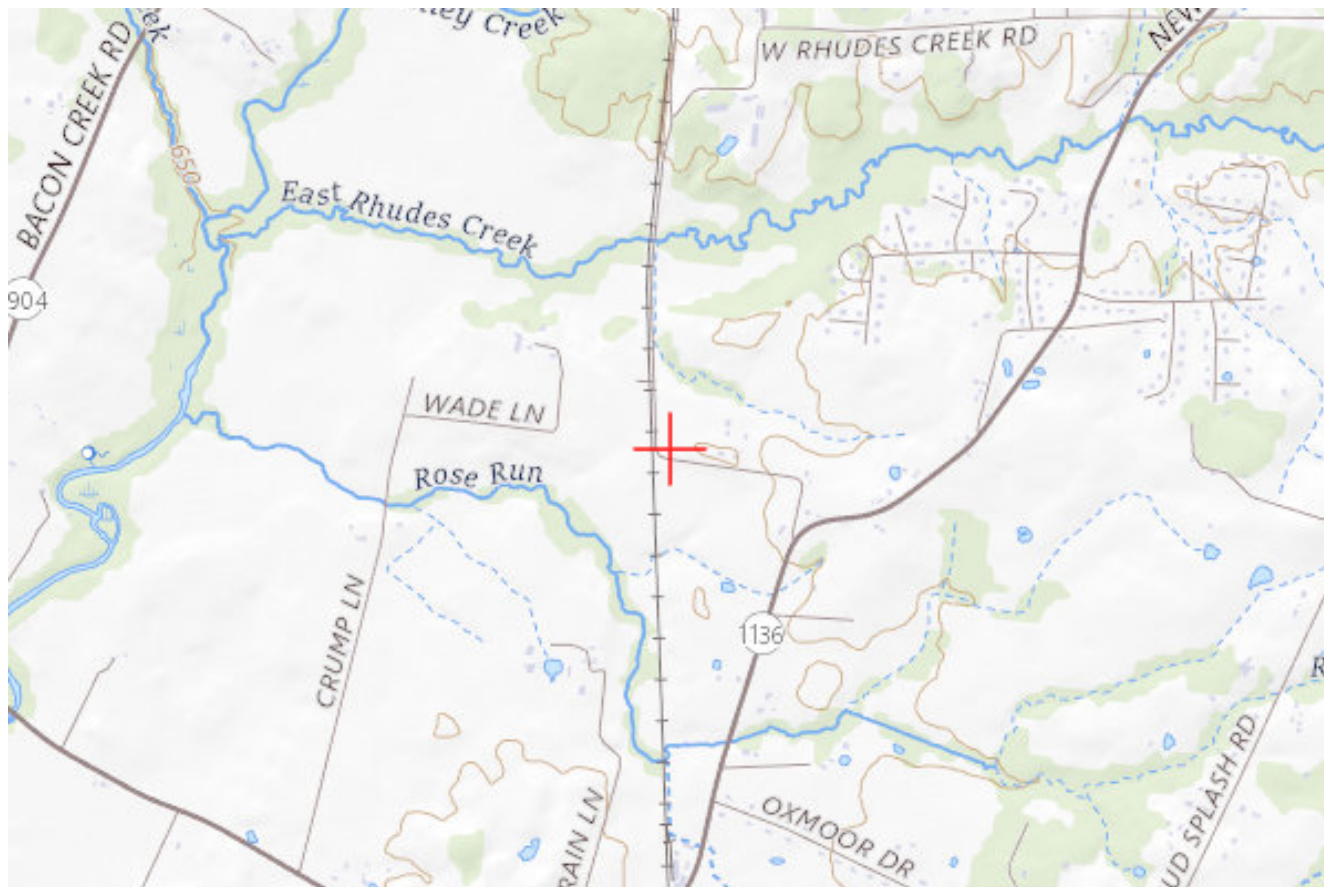
**Signature Control No: 510170917-520362609**

( DNE )

Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3984-OE







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3985-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 29
Location:	Elizabethtown, KY
Latitude:	37-37-02.06N NAD 83
Longitude:	85-54-17.95W
Heights:	688 feet site elevation (SE) 86 feet above ground level (AGL) 774 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3985-OE.

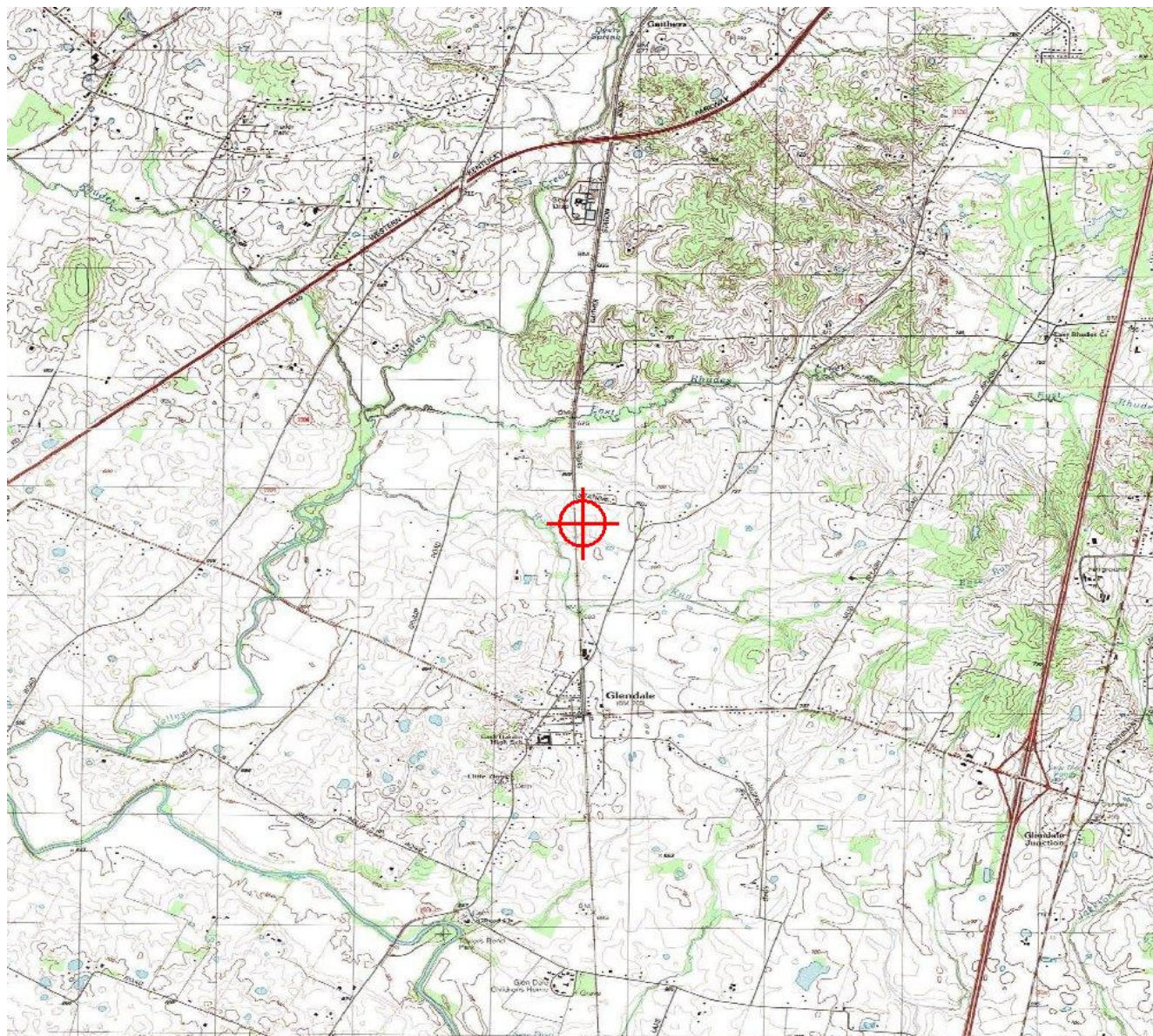
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( DNE )

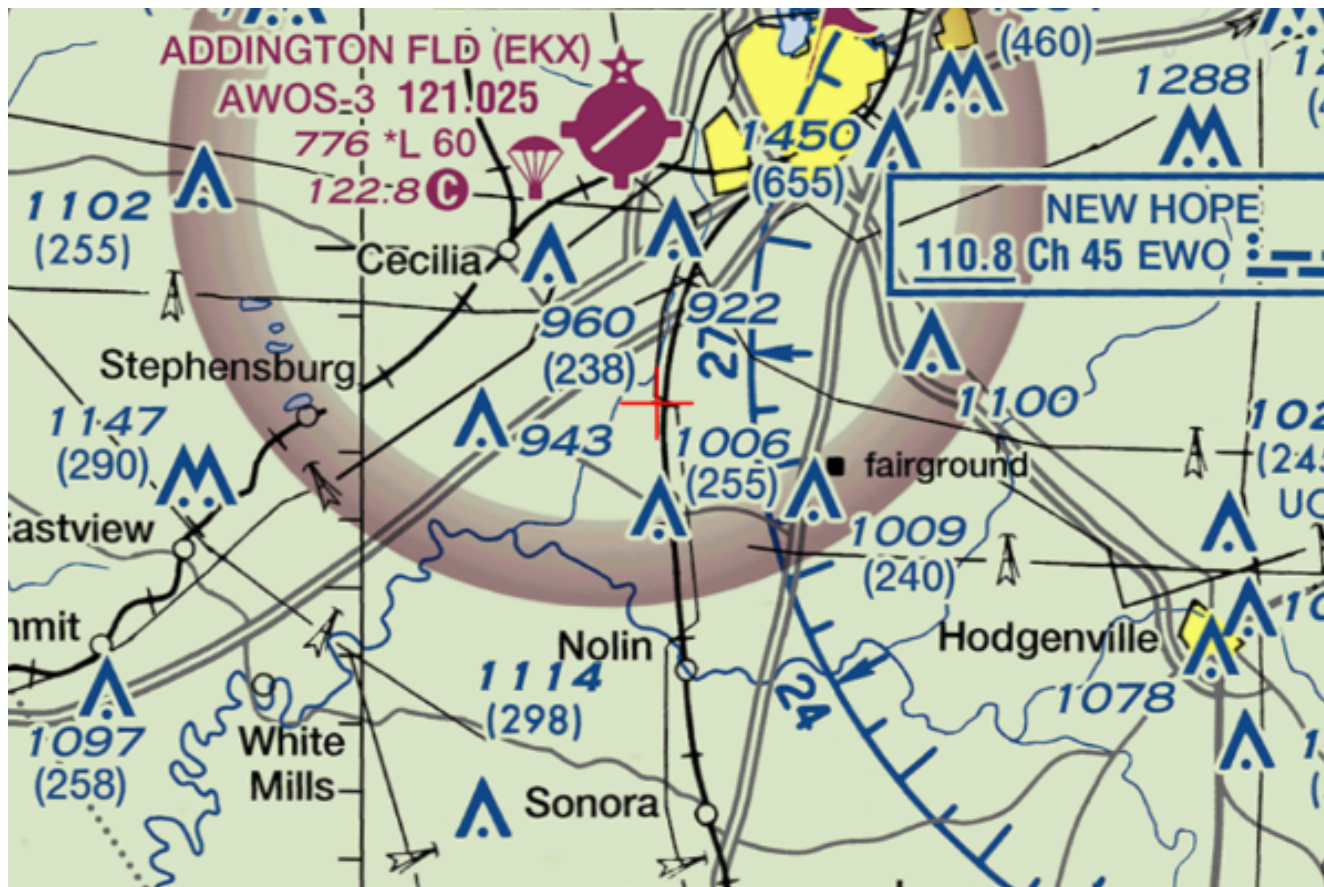
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3985-OE



Sectional Map for ASN 2022-ASO-3985-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3986-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 30
Location:	Elizabethtown, KY
Latitude:	37-36-53.83N NAD 83
Longitude:	85-54-17.58W
Heights:	693 feet site elevation (SE) 86 feet above ground level (AGL) 779 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3986-OE.

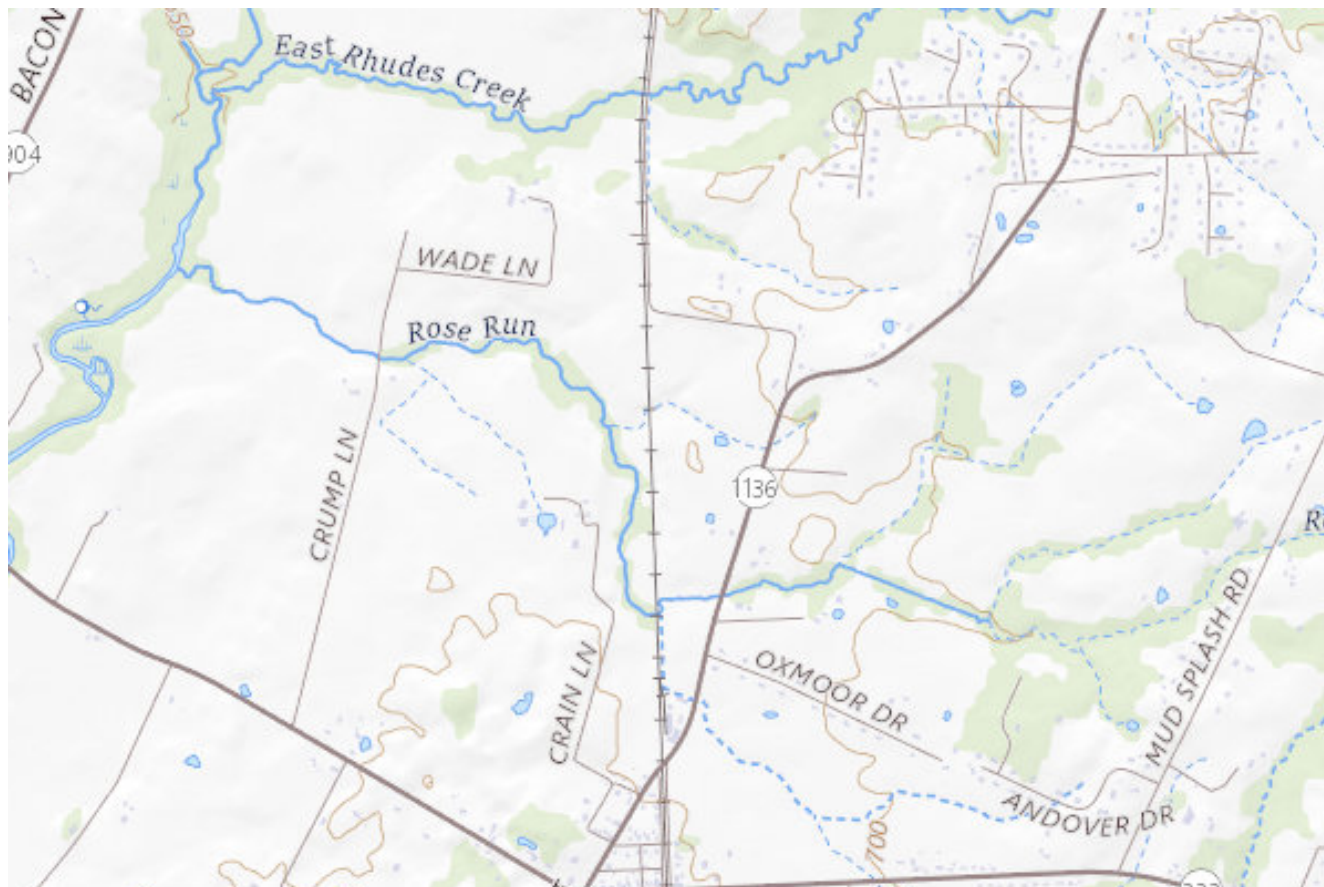
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( DNE )

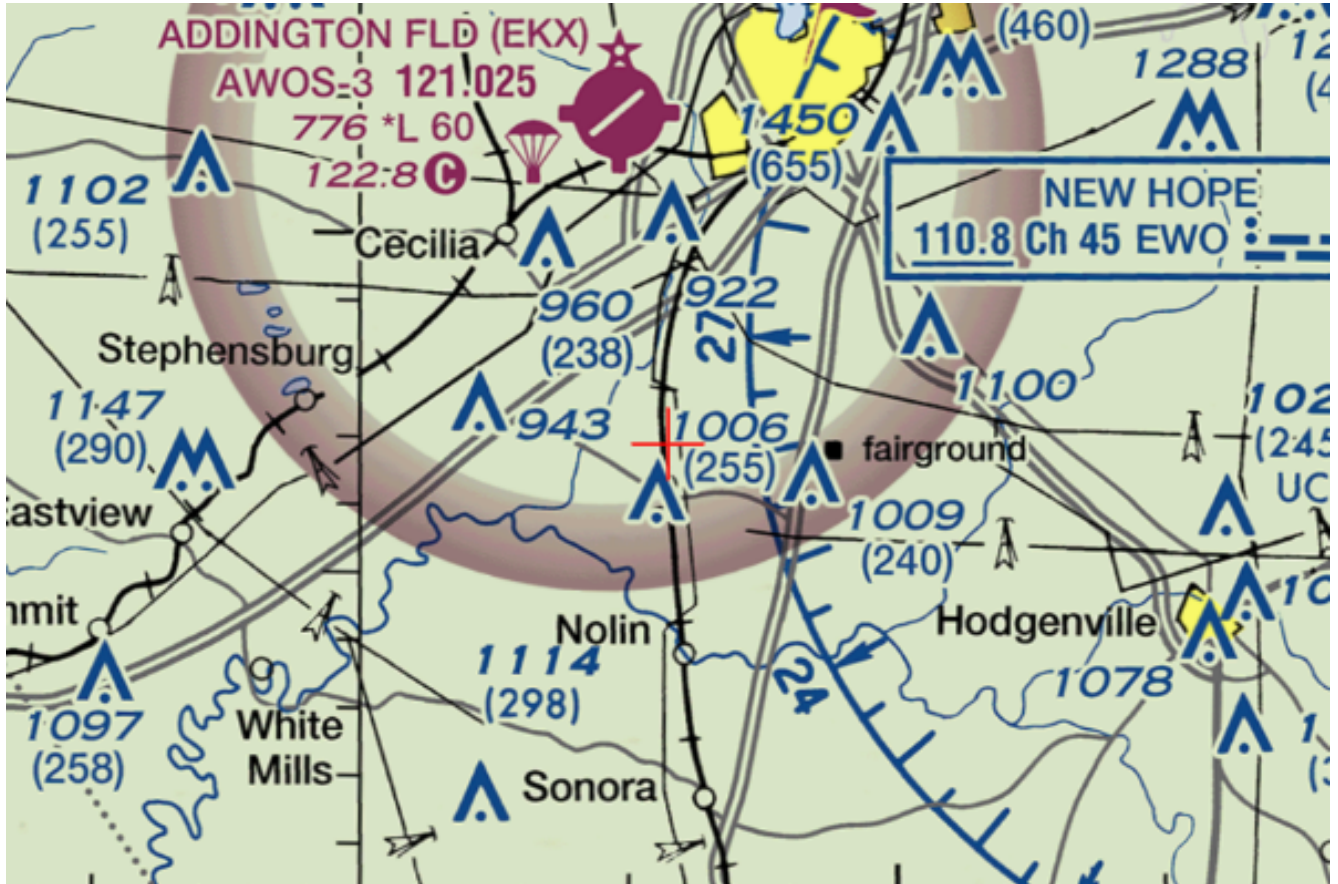
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3986-OE



Sectional Map for ASN 2022-ASO-3986-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3987-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 31
Location:	Elizabethtown, KY
Latitude:	37-36-46.85N NAD 83
Longitude:	85-54-17.27W
Heights:	681 feet site elevation (SE) 91 feet above ground level (AGL) 772 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

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- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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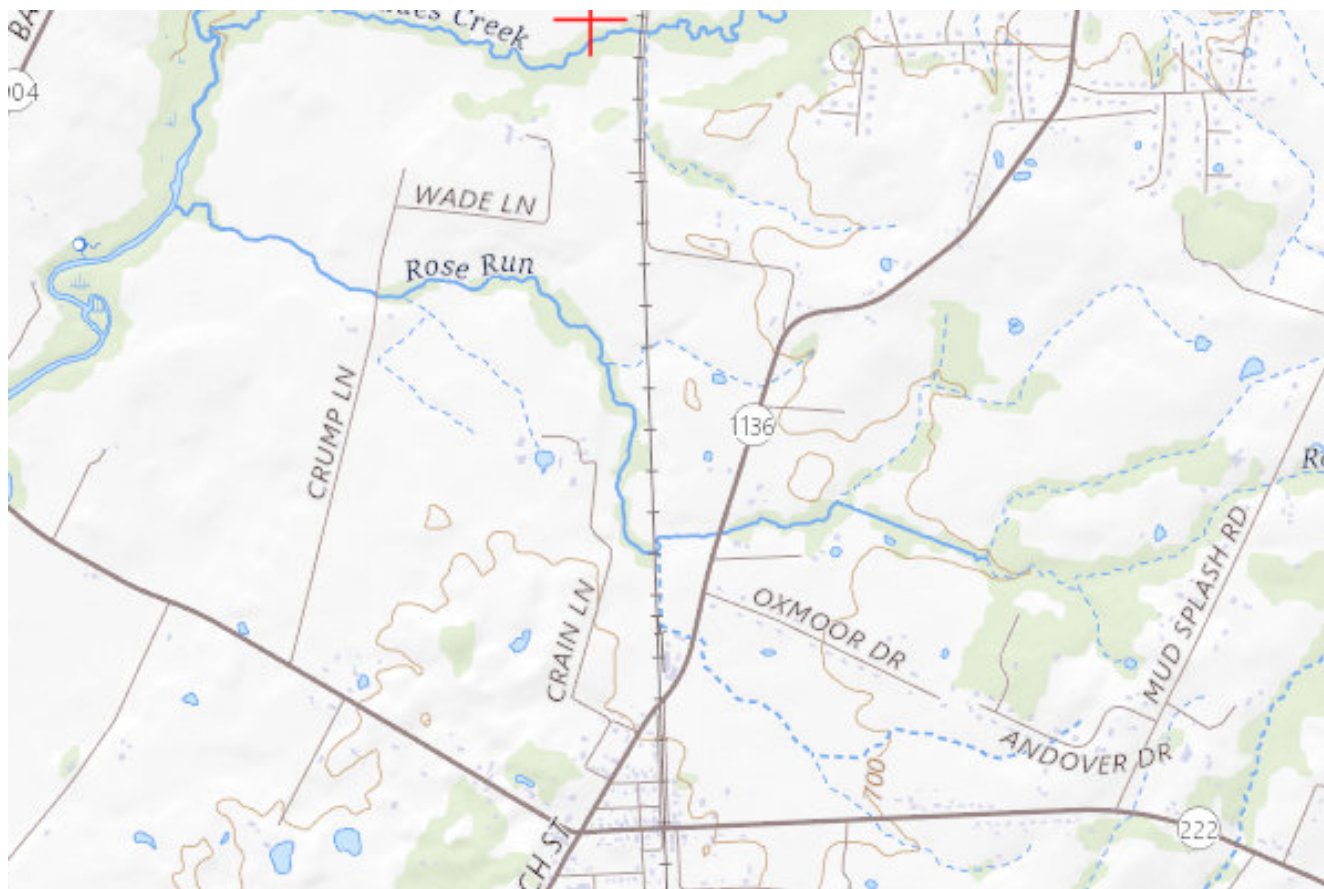
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( DNE )

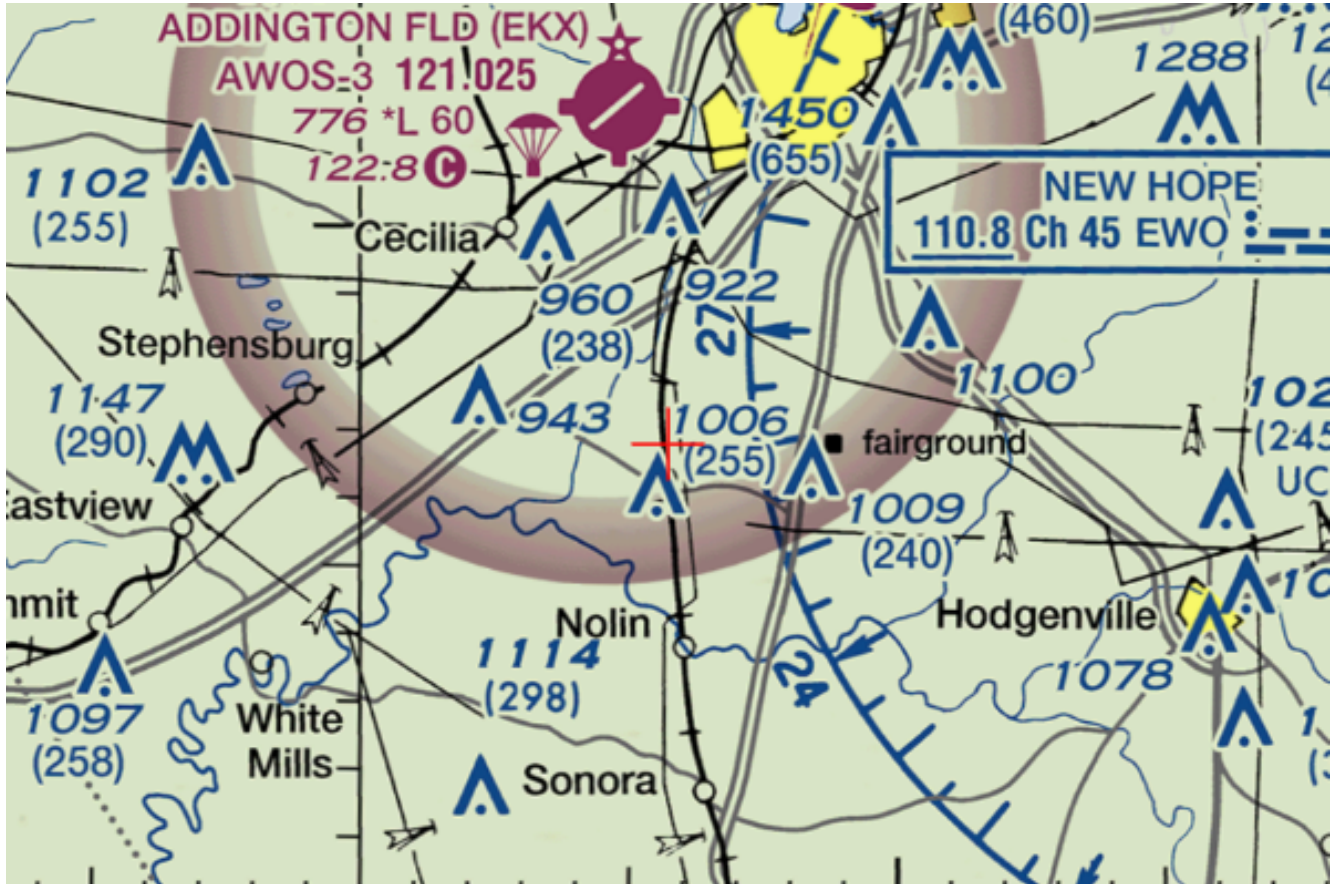
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3987-OE



Sectional Map for ASN 2022-ASO-3987-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3988-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 32
Location:	Elizabethtown, KY
Latitude:	37-36-40.00N NAD 83
Longitude:	85-54-16.97W
Heights:	682 feet site elevation (SE) 86 feet above ground level (AGL) 768 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3988-OE.

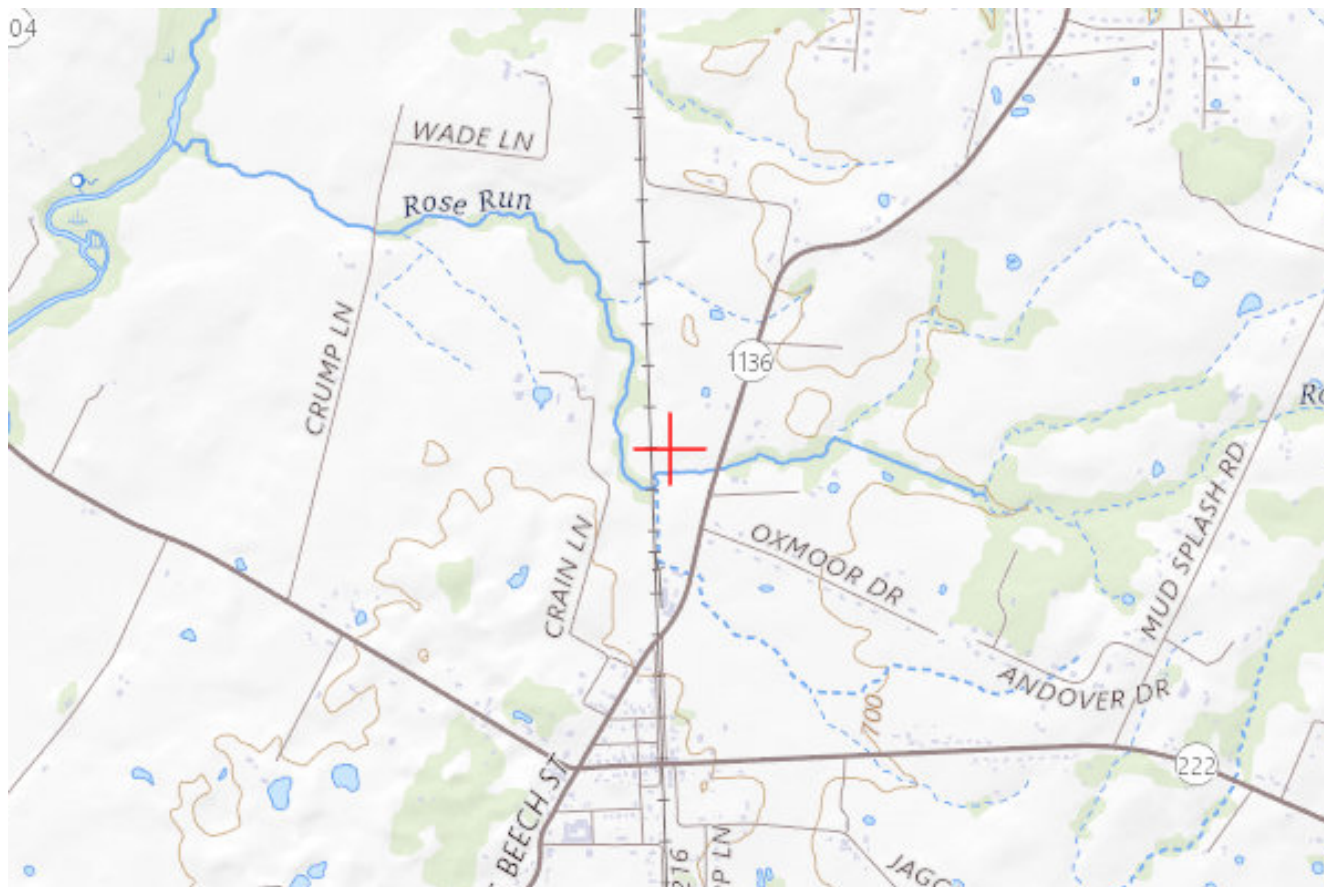
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( DNE )

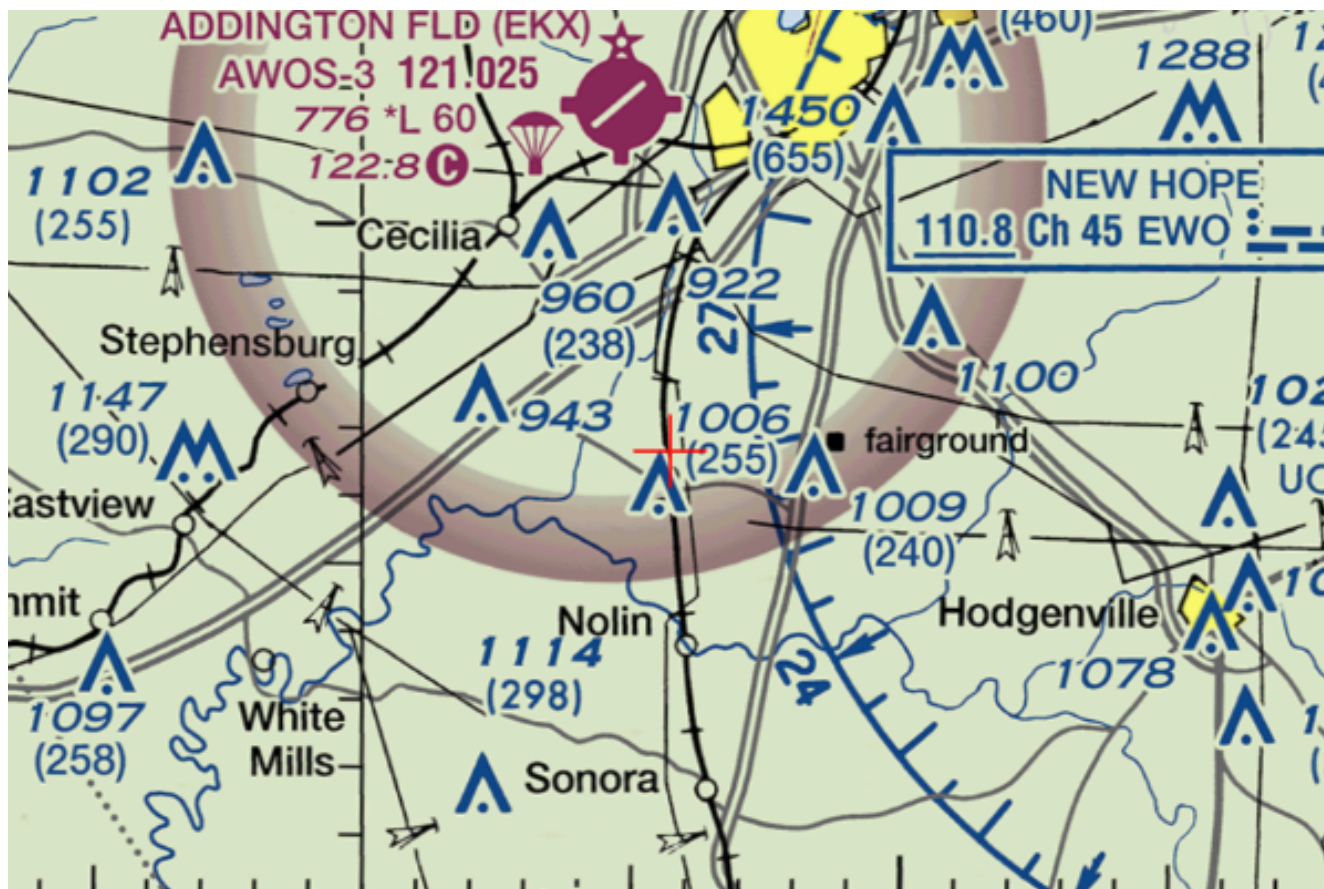
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3988-OE



Sectional Map for ASN 2022-ASO-3988-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3989-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 33
Location:	Elizabethtown, KY
Latitude:	37-36-33.74N NAD 83
Longitude:	85-54-16.69W
Heights:	686 feet site elevation (SE) 94 feet above ground level (AGL) 780 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3989-OE.

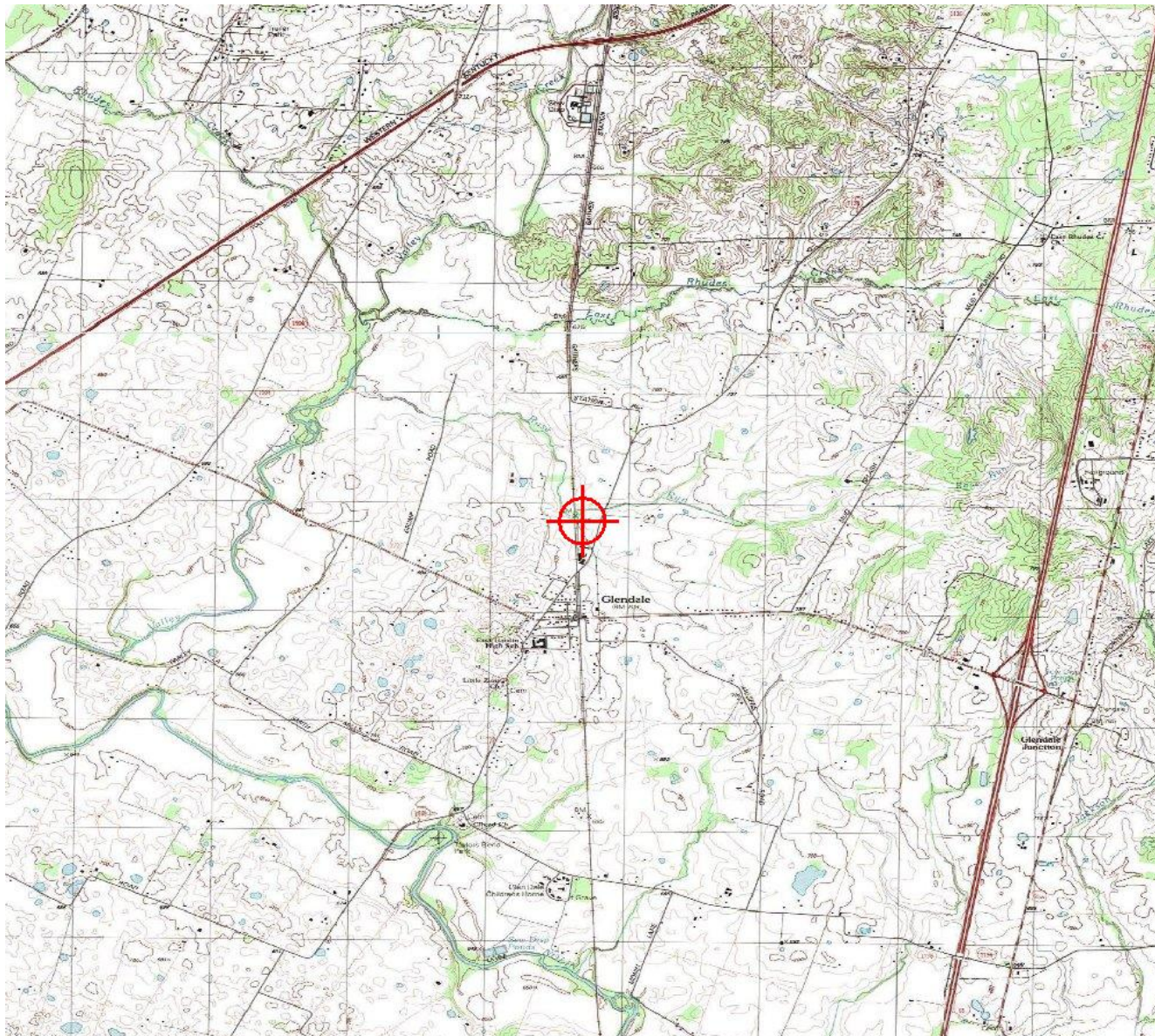
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( DNE )

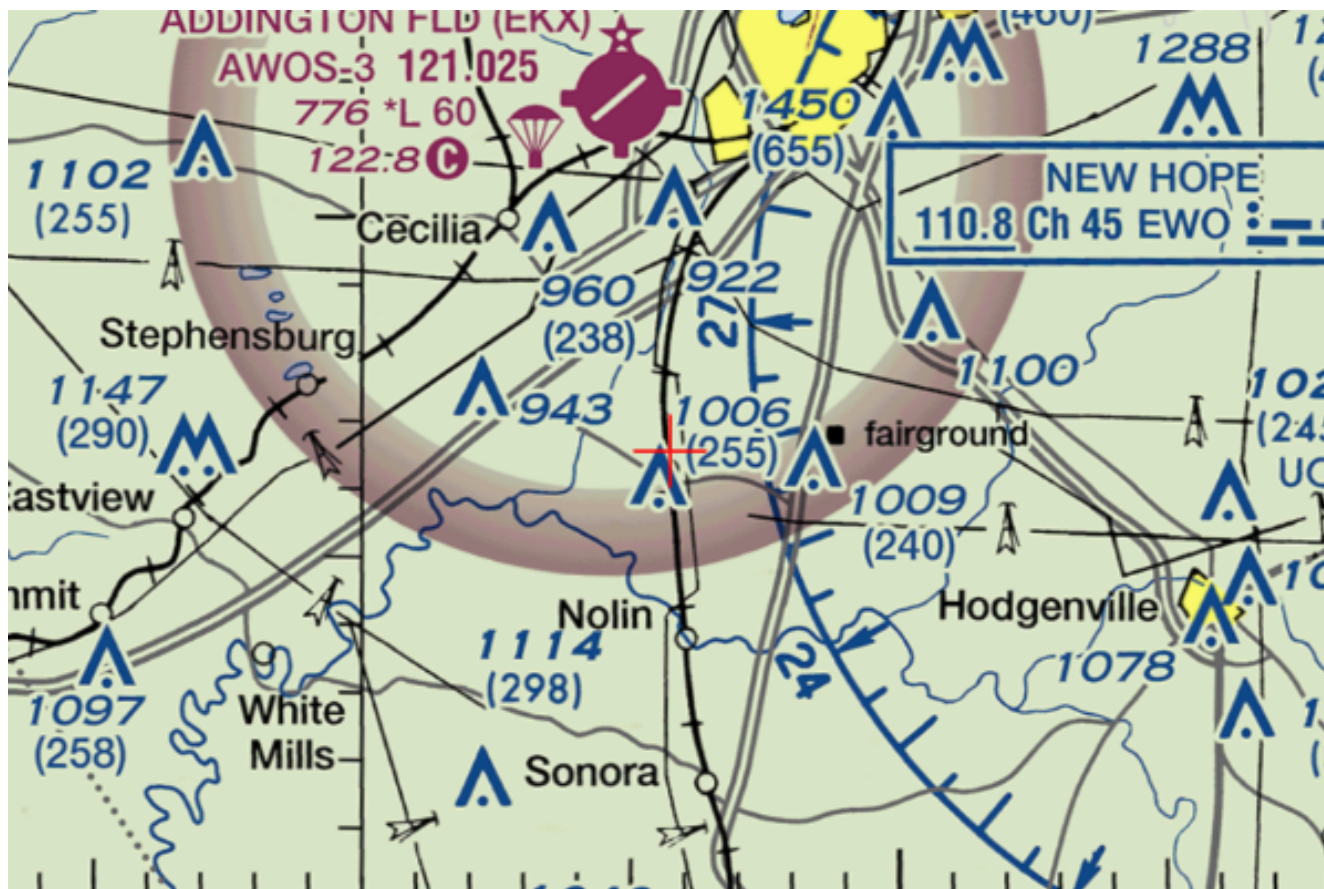
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3989-OE



Sectional Map for ASN 2022-ASO-3989-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3990-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 34
Location:	Elizabethtown, KY
Latitude:	37-36-26.40N NAD 83
Longitude:	85-54-12.71W
Heights:	684 feet site elevation (SE) 91 feet above ground level (AGL) 775 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

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- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3990-OE.

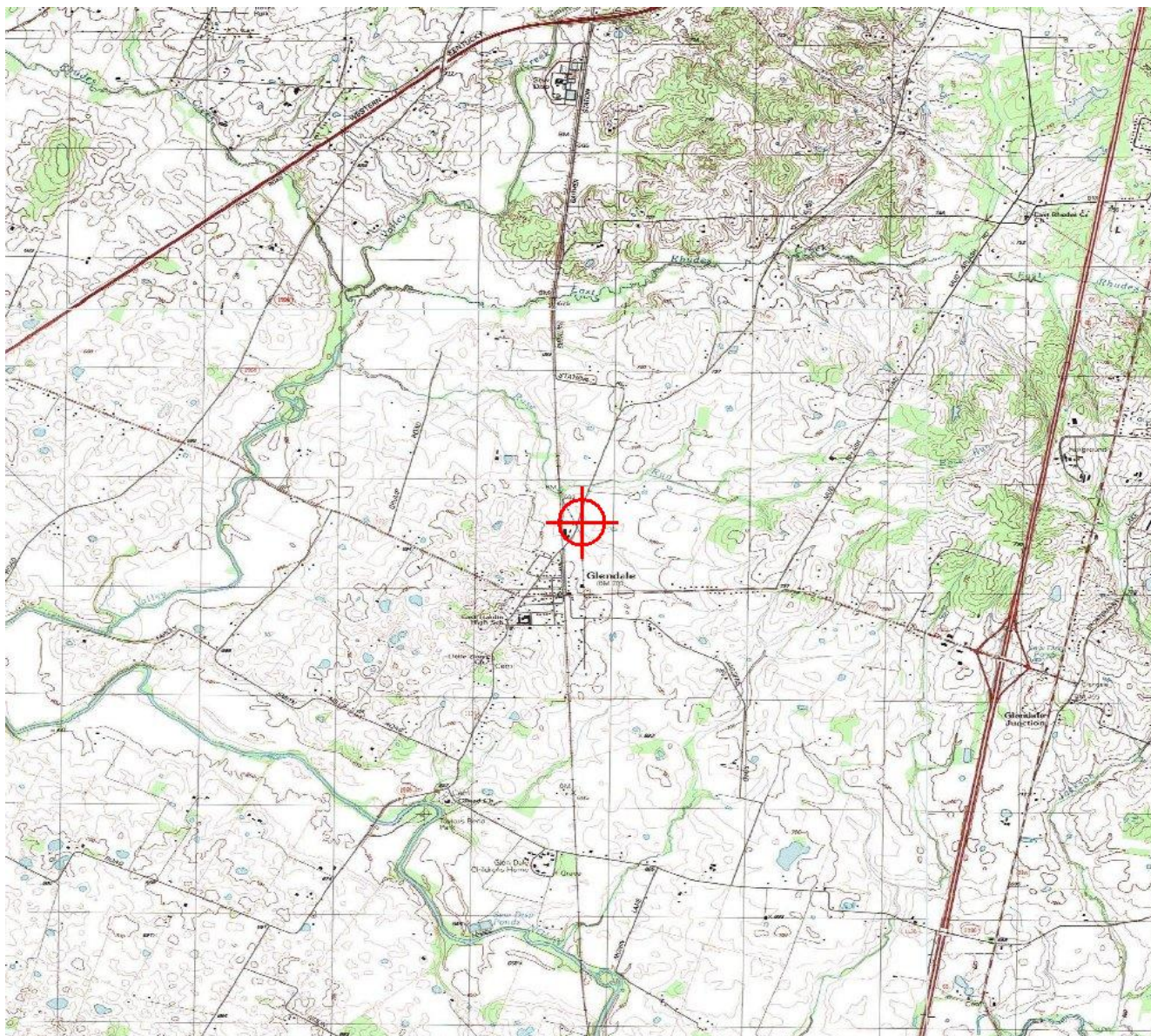
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( DNE )

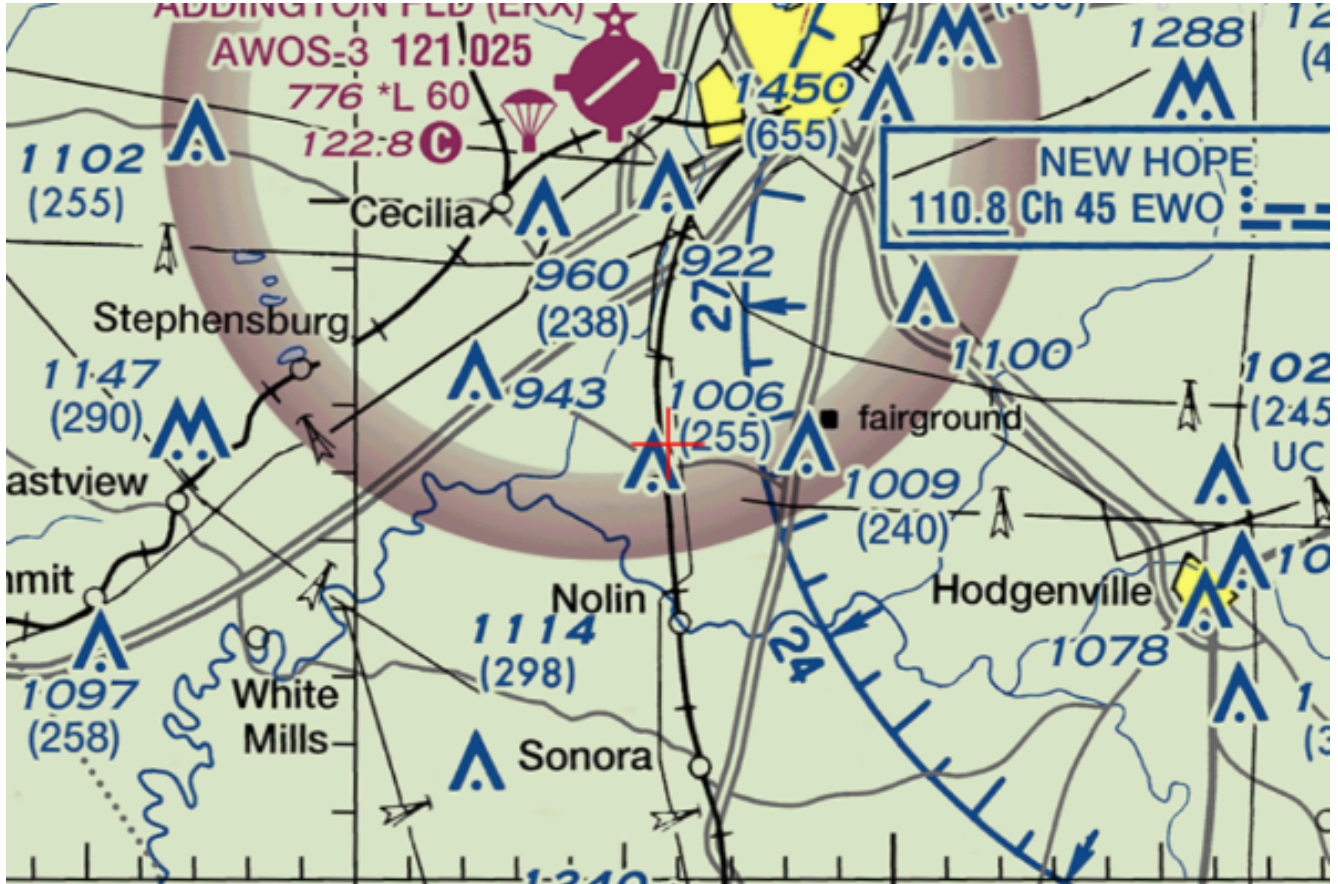
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3990-OE



Sectional Map for ASN 2022-ASO-3990-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3991-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 35
Location:	Elizabethtown, KY
Latitude:	37-36-19.54N NAD 83
Longitude:	85-54-08.99W
Heights:	686 feet site elevation (SE) 86 feet above ground level (AGL) 772 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3991-OE.

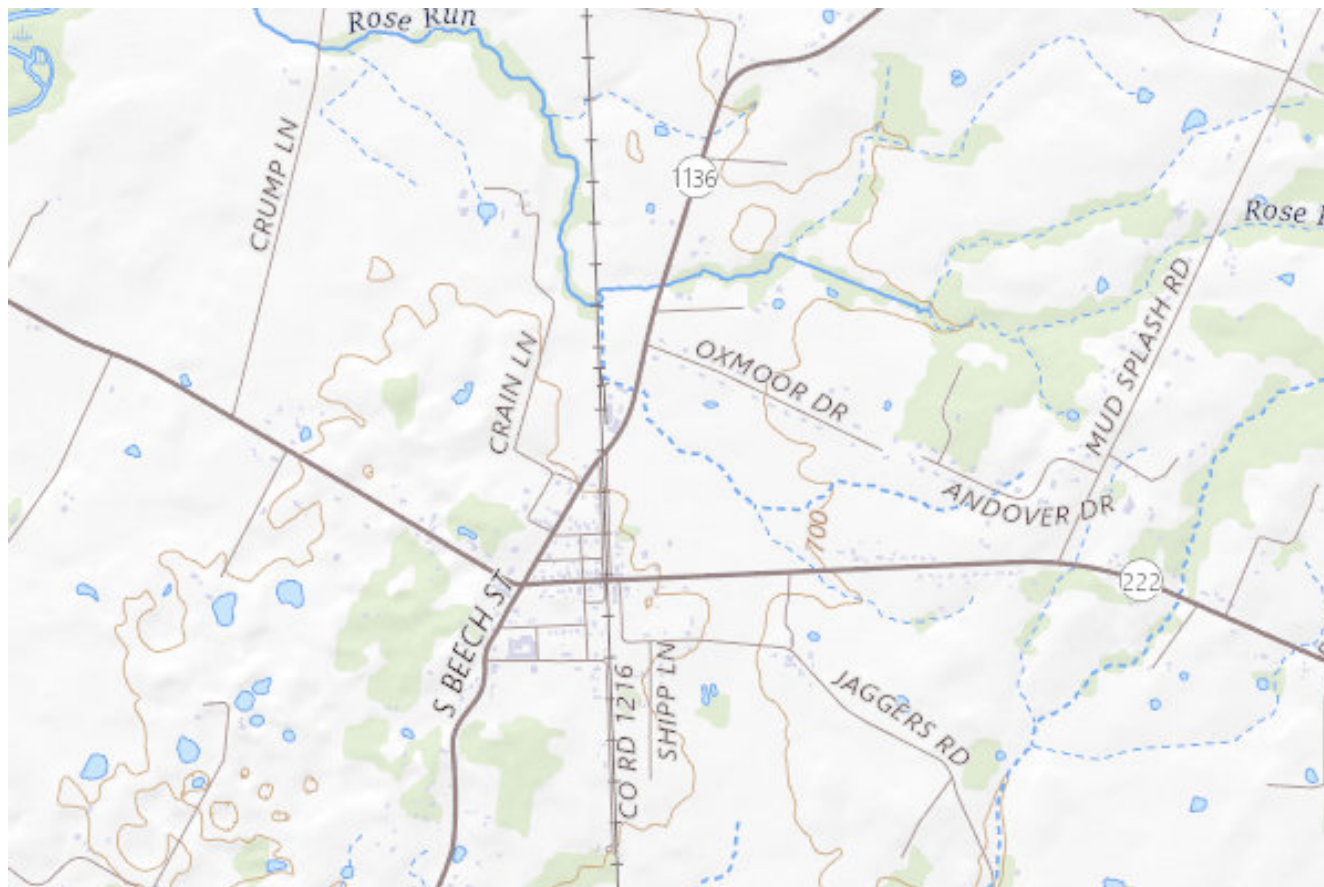
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( DNE )

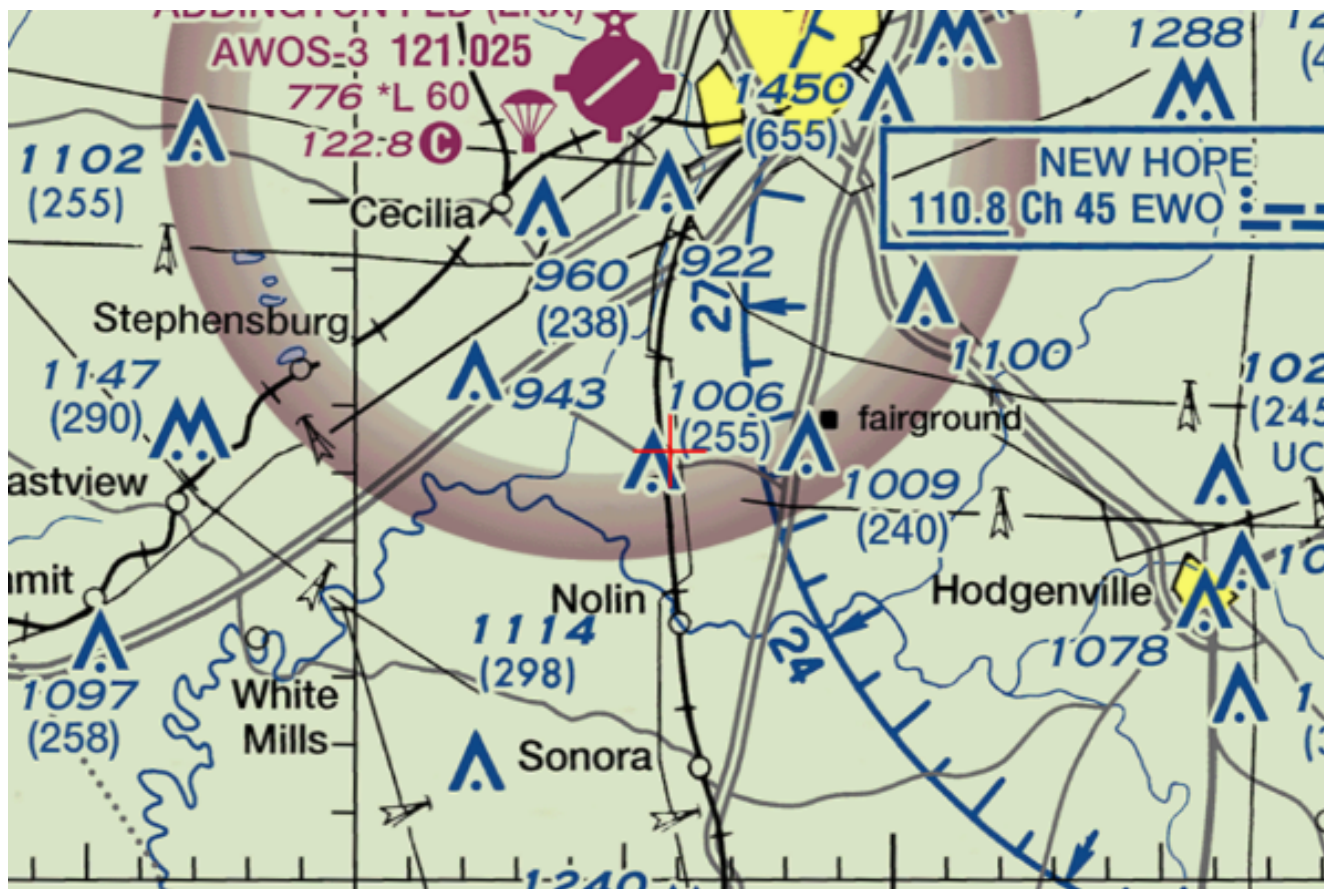
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3991-OE



Sectional Map for ASN 2022-ASO-3991-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3992-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 36
Location:	Elizabethtown, KY
Latitude:	37-36-12.37N NAD 83
Longitude:	85-54-05.10W
Heights:	691 feet site elevation (SE) 91 feet above ground level (AGL) 782 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3992-OE.

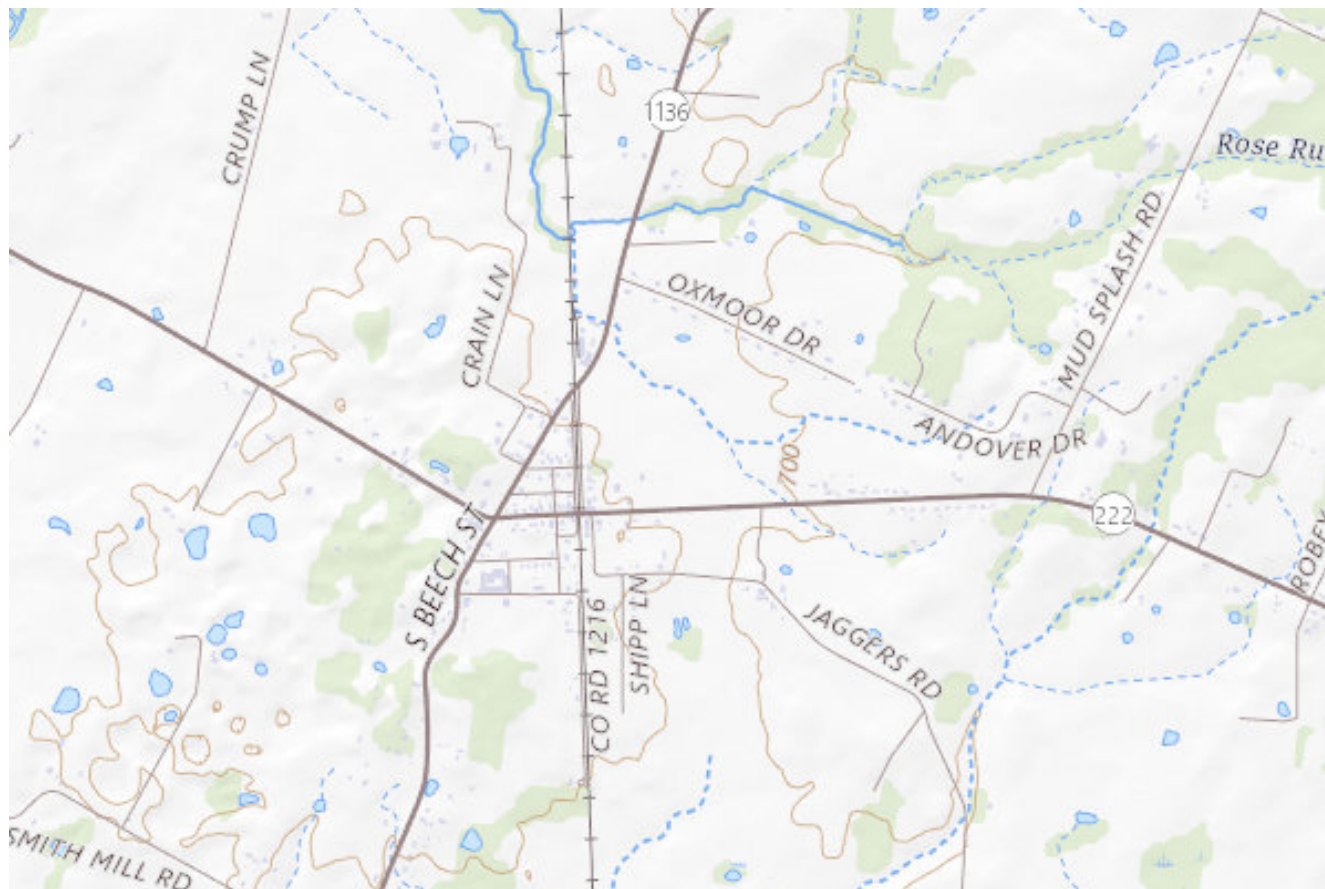
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( DNE )

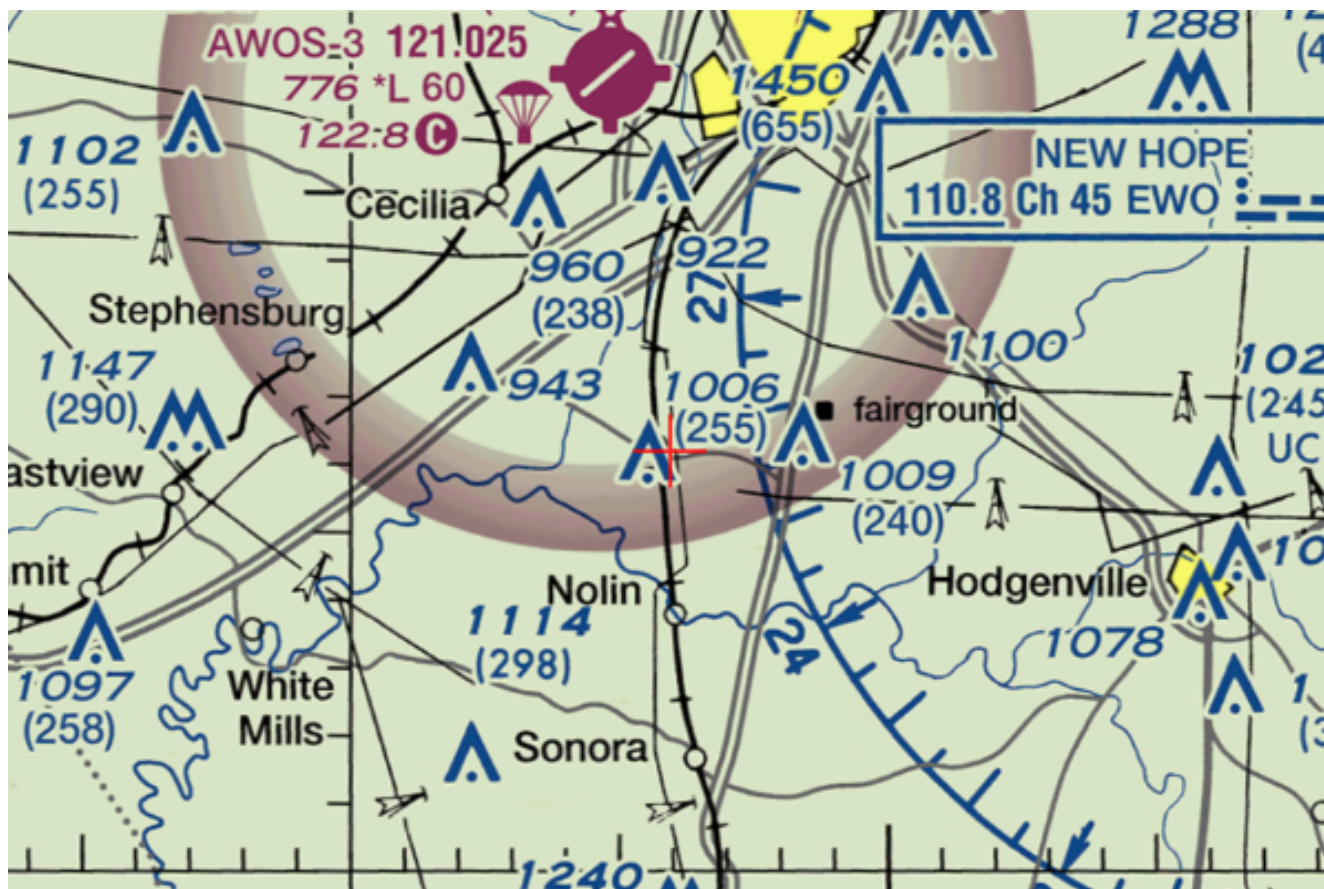
Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3992-OE



Sectional Map for ASN 2022-ASO-3992-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-3993-OE

Issued Date: 03/28/2022

Michael Kern  
LGE-KU\_4 HARDIN Co  
1 Quality Street  
Lexington, KY 40507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Transmission Line Tower 37
Location:	Elizabethtown, KY
Latitude:	37-36-05.06N NAD 83
Longitude:	85-54-01.14W
Heights:	700 feet site elevation (SE) 91 feet above ground level (AGL) 791 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/28/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-3993-OE.

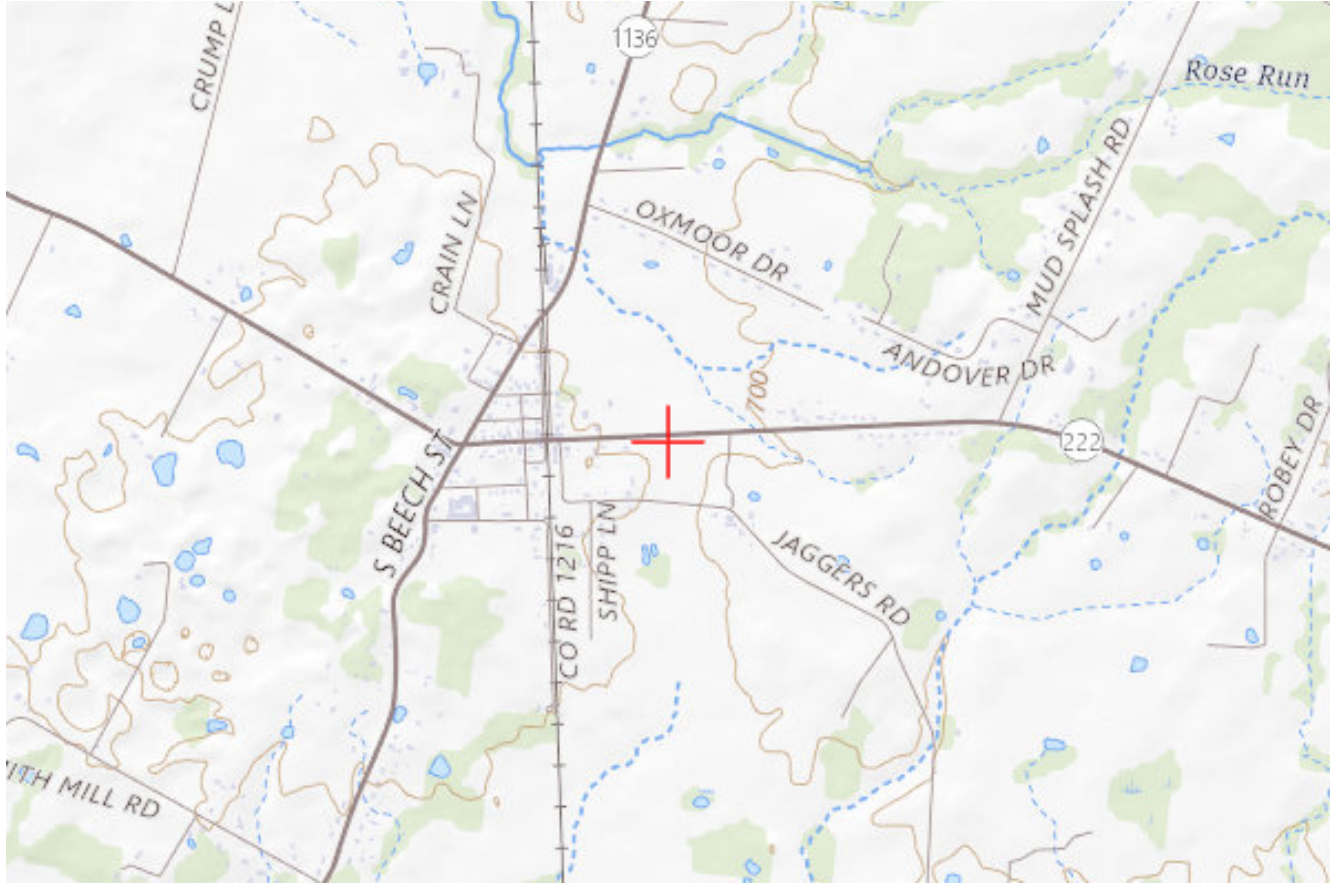
**Signature Control No: 510170926-520362616**

( DNE )

Chris Smith  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2022-ASO-3993-OE



Sectional Map for ASN 2022-ASO-3993-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15396-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-36-05.28N NAD 83
Longitude:	85-54-00.59W
Heights:	699 feet site elevation (SE) 200 feet above ground level (AGL) 899 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15396-OE

**Signature Control No: 525291194-529276332**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15396-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 899 feet above mean sea level.

**Location:** The structure will be located 5.2 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

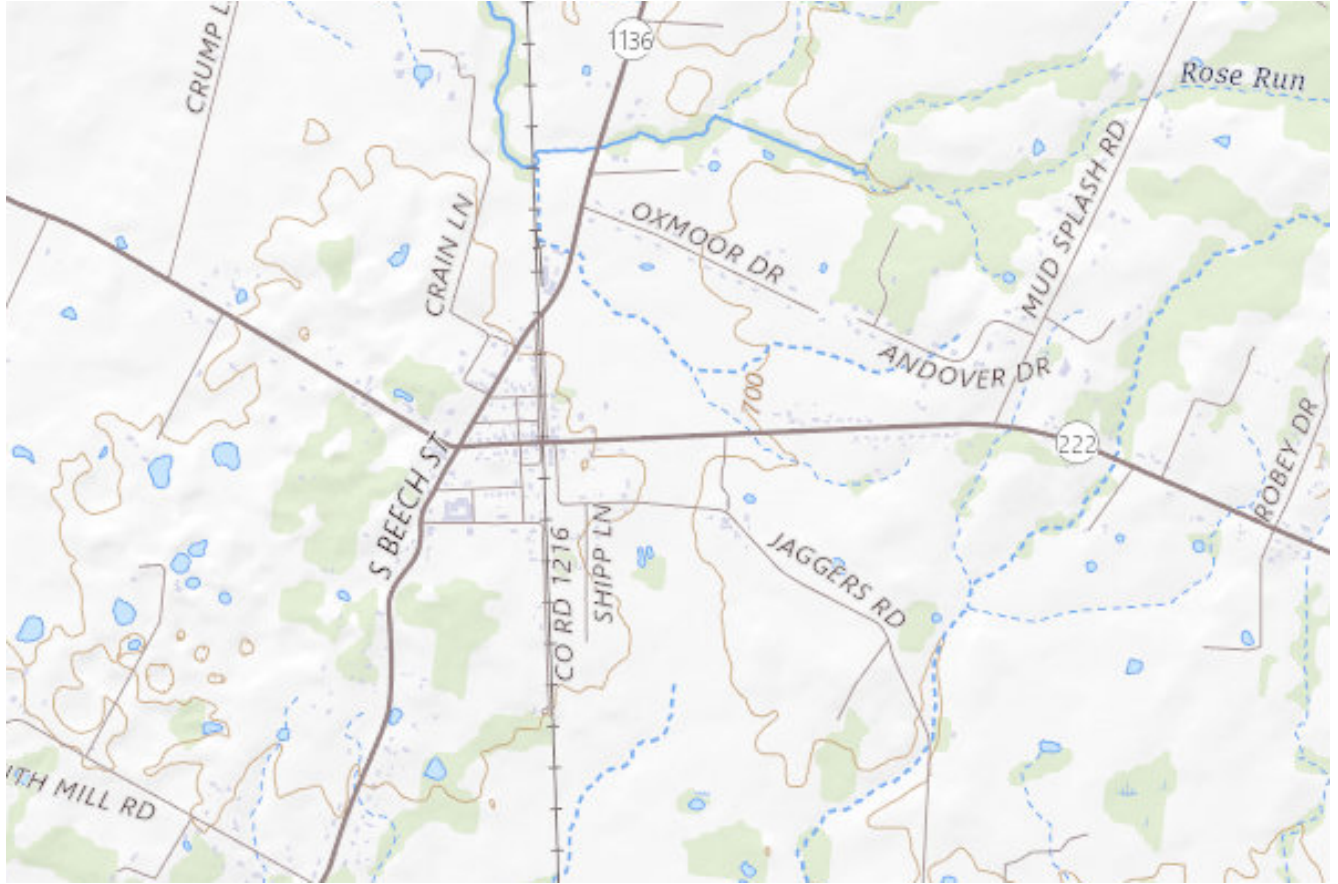
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

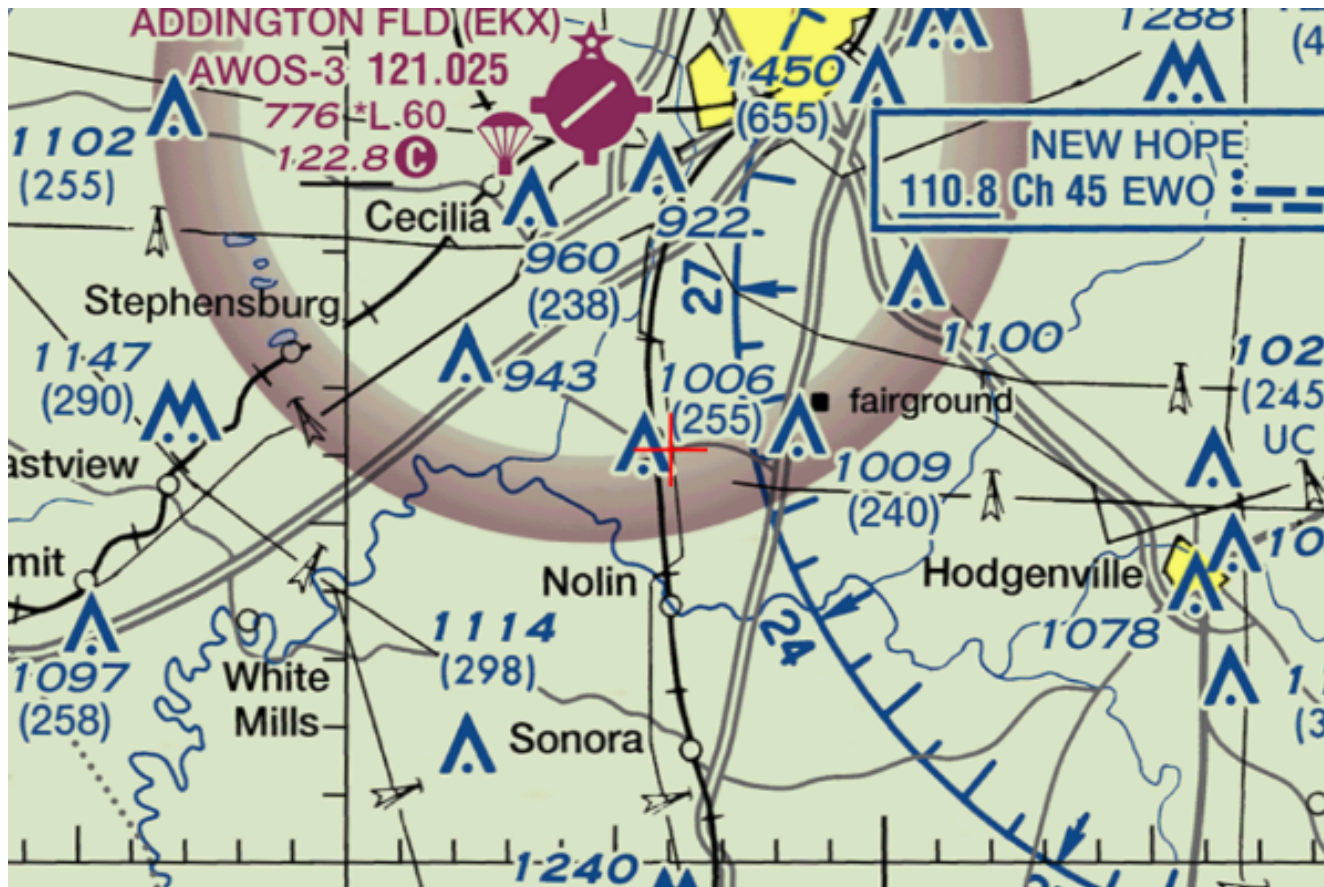
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15396-OE



Sectional Map for ASN 2022-ASO-15396-OE







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15397-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-36-33.85N NAD 83
Longitude:	85-54-16.07W
Heights:	691 feet site elevation (SE) 200 feet above ground level (AGL) 891 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15397-OE

**Signature Control No: 525291196-529276326**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15397-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 891 feet above mean sea level.

**Location:** The structure will be located 4.69 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

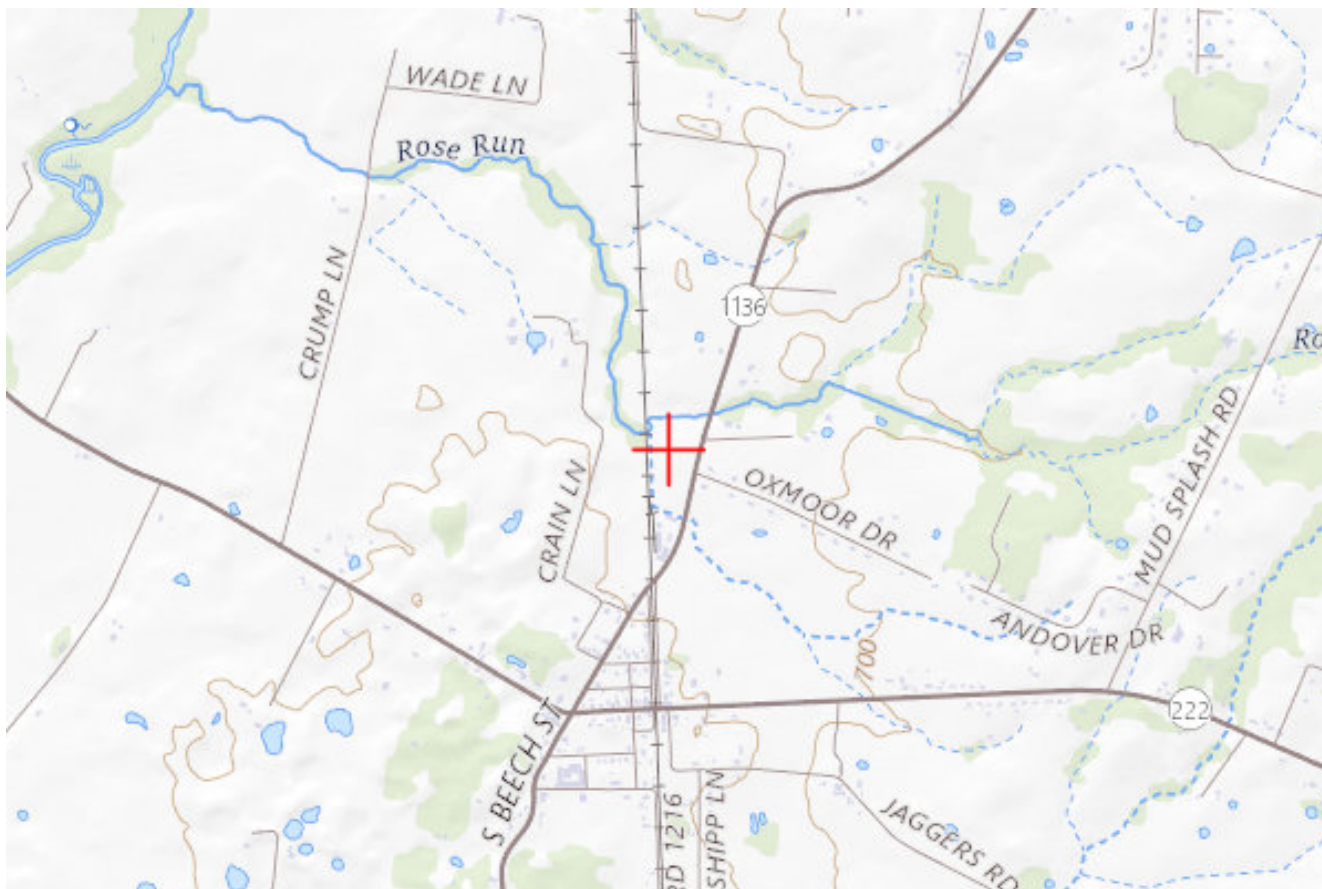
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

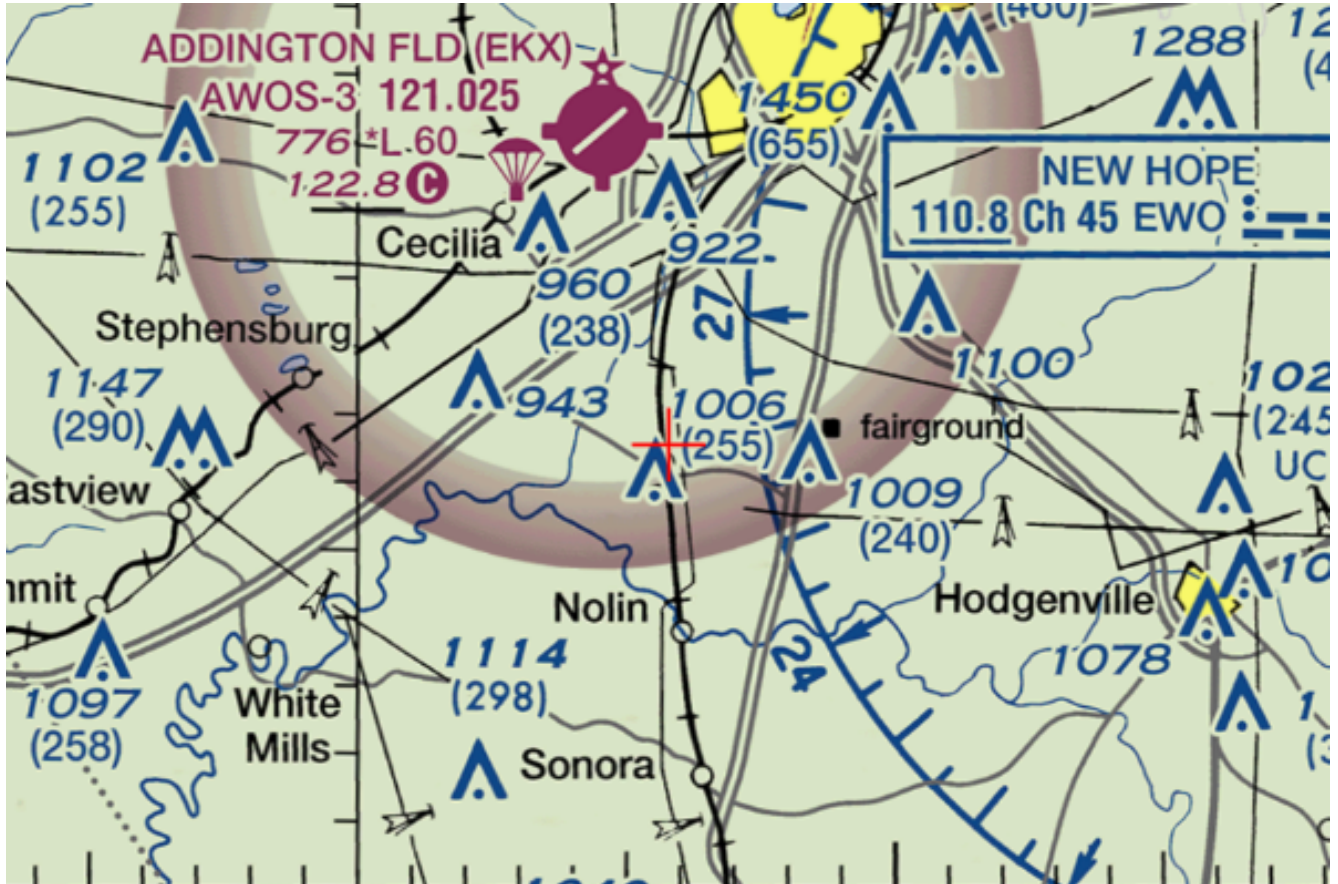
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15397-OE



Sectional Map for ASN 2022-ASO-15397-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15398-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-37-23.84N NAD 83
Longitude:	85-54-18.28W
Heights:	691 feet site elevation (SE) 200 feet above ground level (AGL) 891 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15398-OE

**Signature Control No: 525291198-529276333**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15398-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 891 feet above mean sea level.

**Location:** The structure will be located 3.88 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

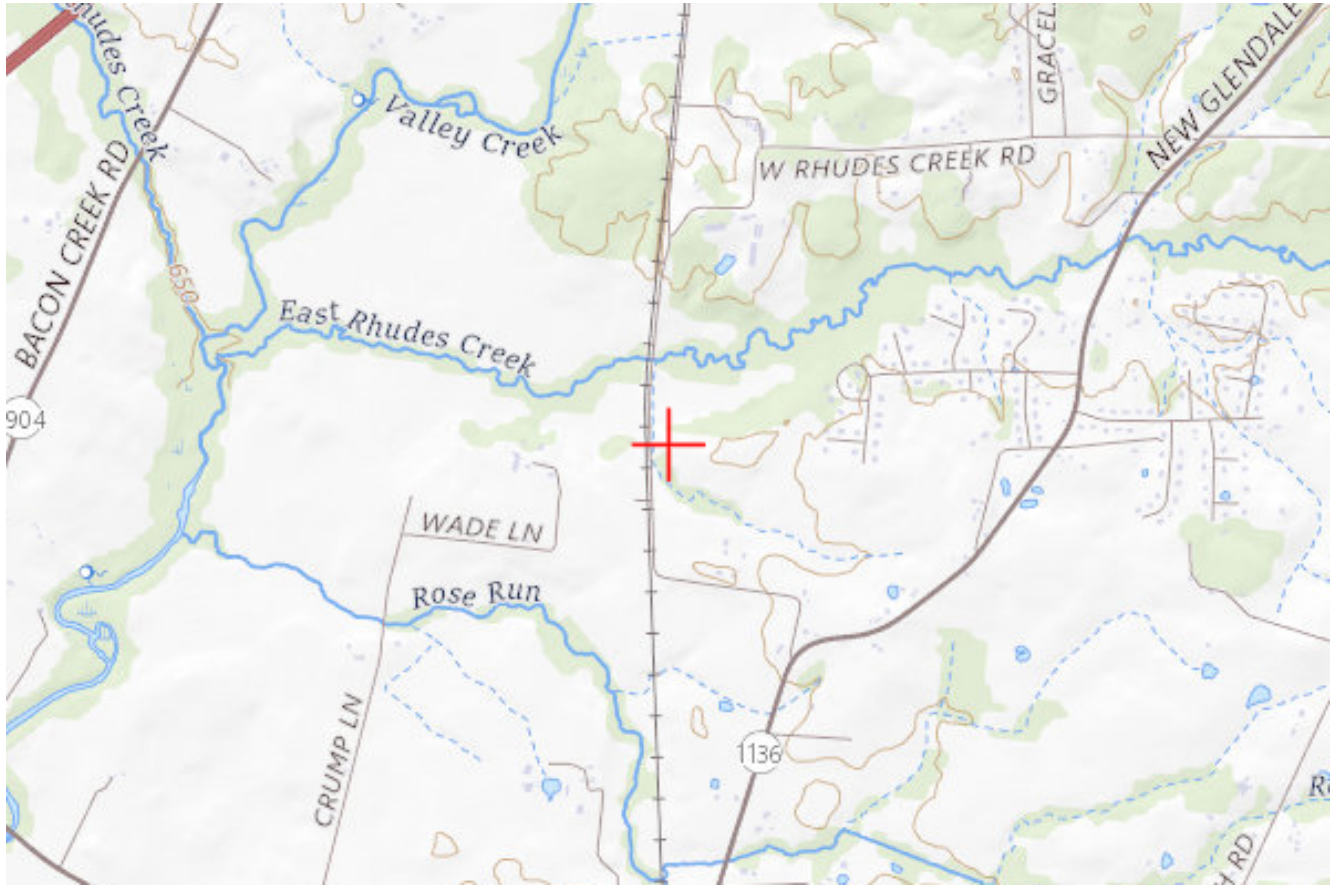
This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

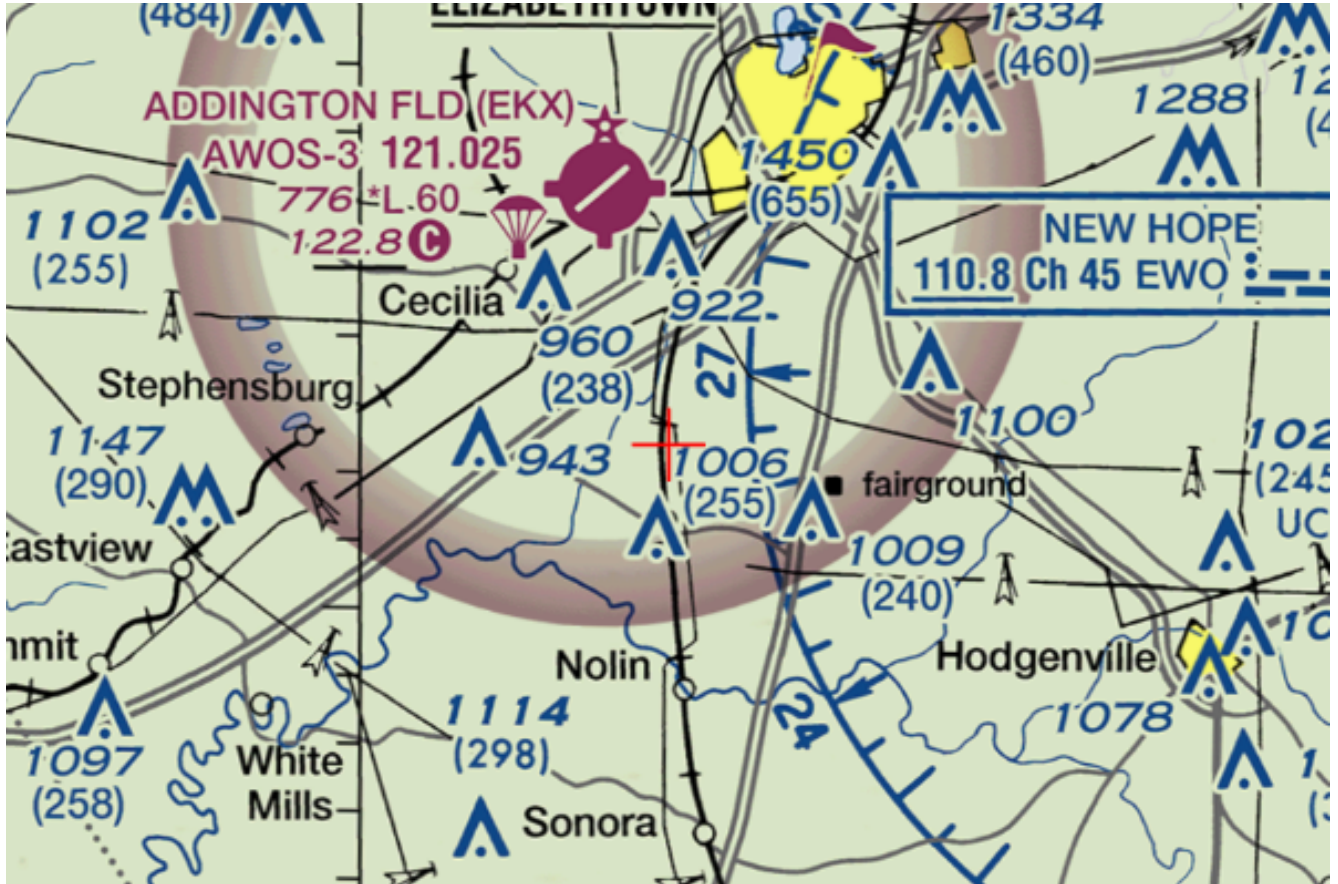
**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



TOPO Map for ASN 2022-ASO-15398-OE



Sectional Map for ASN 2022-ASO-15398-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15399-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-37-26.00N NAD 83
Longitude:	85-54-22.42W
Heights:	664 feet site elevation (SE) 200 feet above ground level (AGL) 864 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15399-OE

**Signature Control No: 525291200-529276327**

( TMP )

Chris Smith  
Specialist

**Additional Condition(s) or Information for ASN 2022-ASO-15399-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 864 feet above mean sea level.

**Location:** The structure will be located 3.83 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

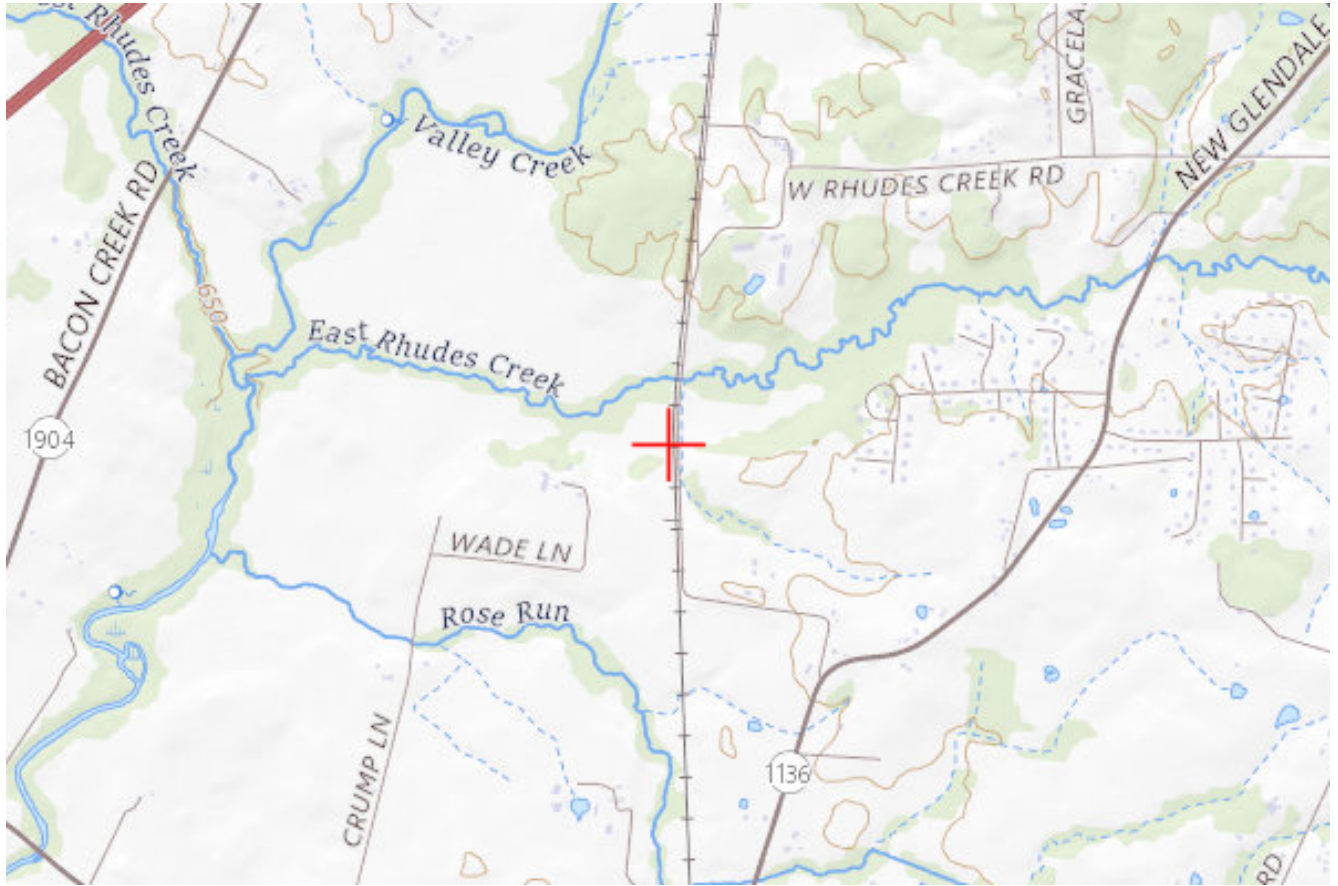
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

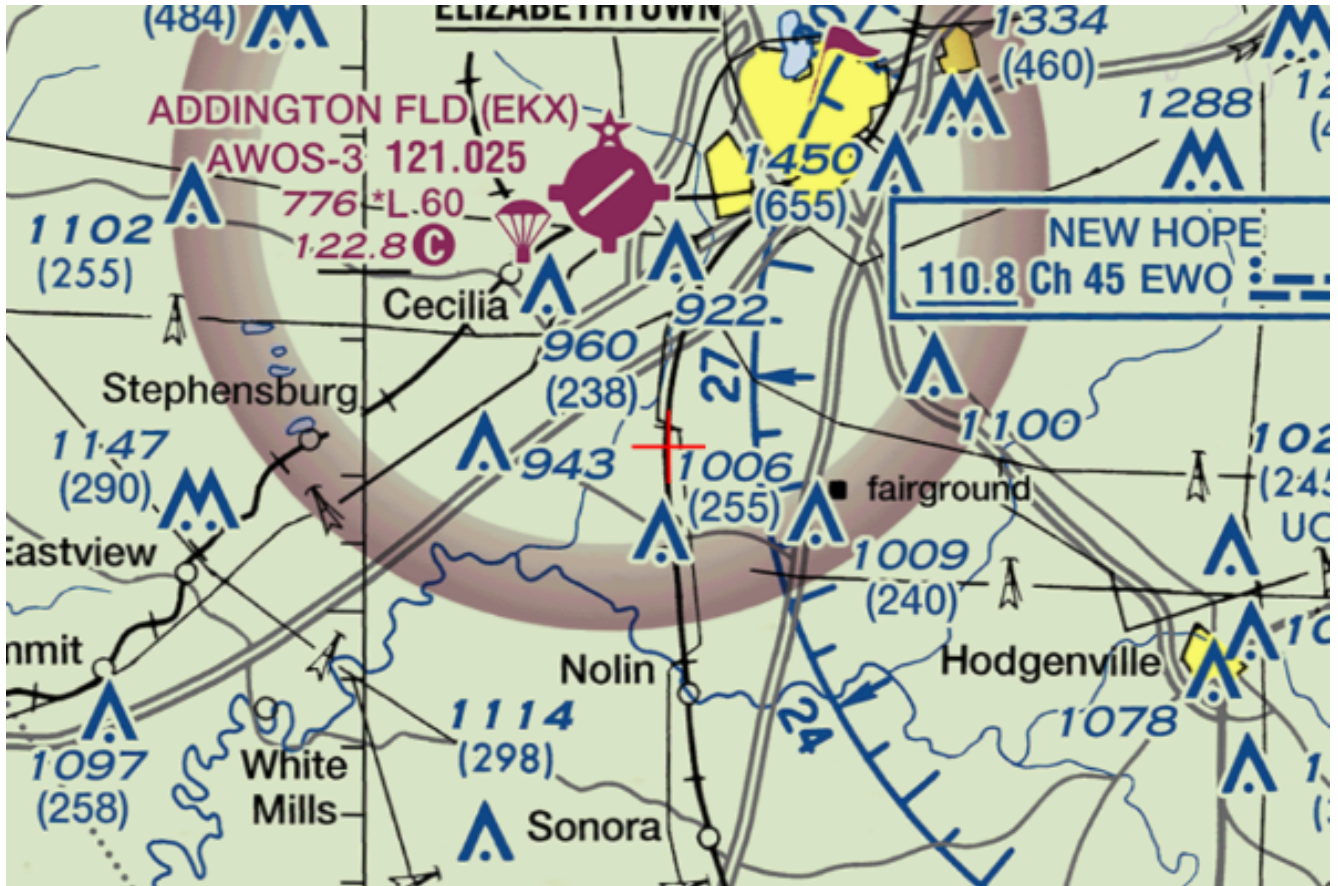
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15399-OE



Sectional Map for ASN 2022-ASO-15399-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15400-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-37-50.63N NAD 83
Longitude:	85-54-37.57W
Heights:	694 feet site elevation (SE) 200 feet above ground level (AGL) 894 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).



If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15400-OE

**Signature Control No: 525291201-529276371**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15400-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 894 feet above mean sea level.

**Location:** The structure will be located 3.38 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

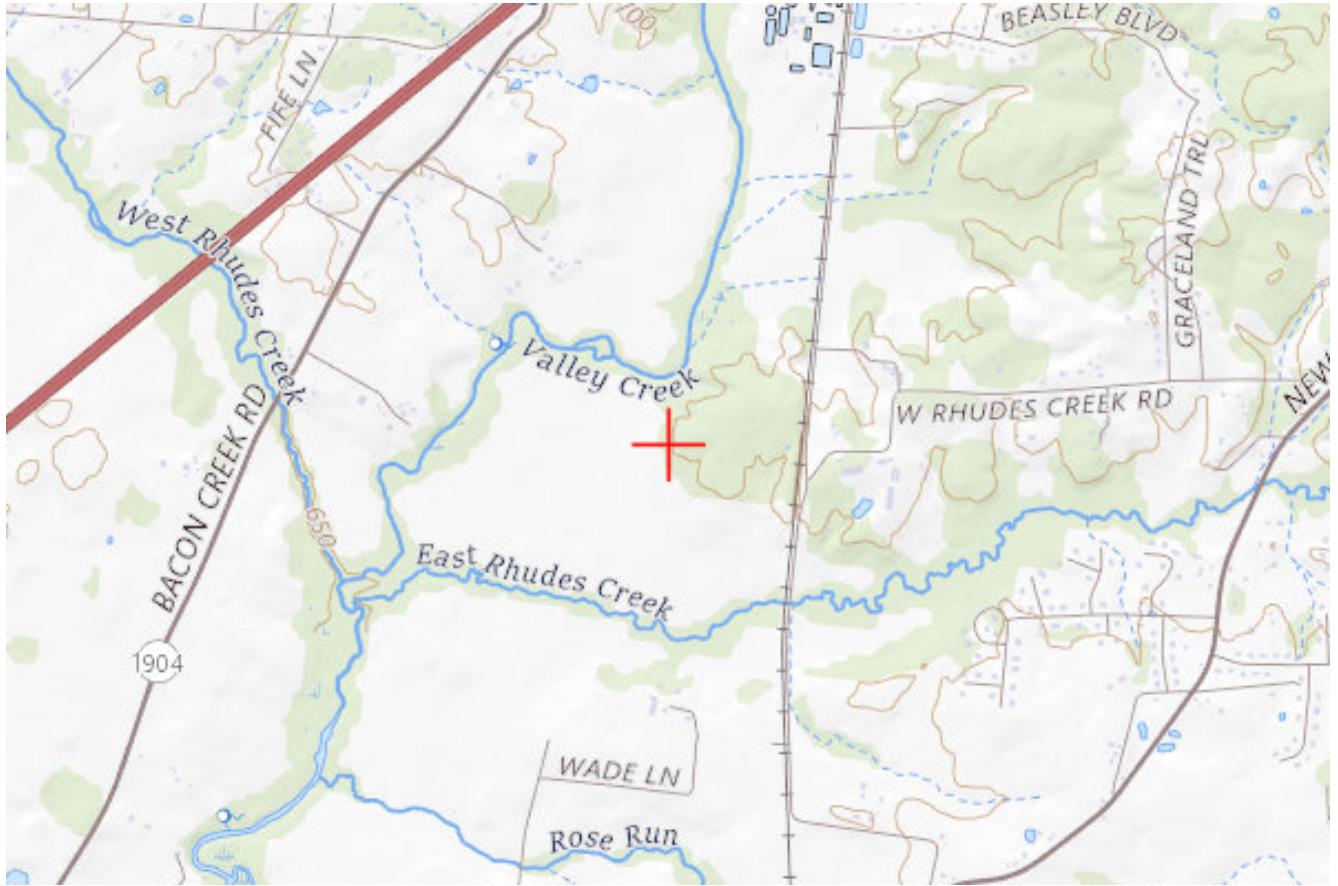
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

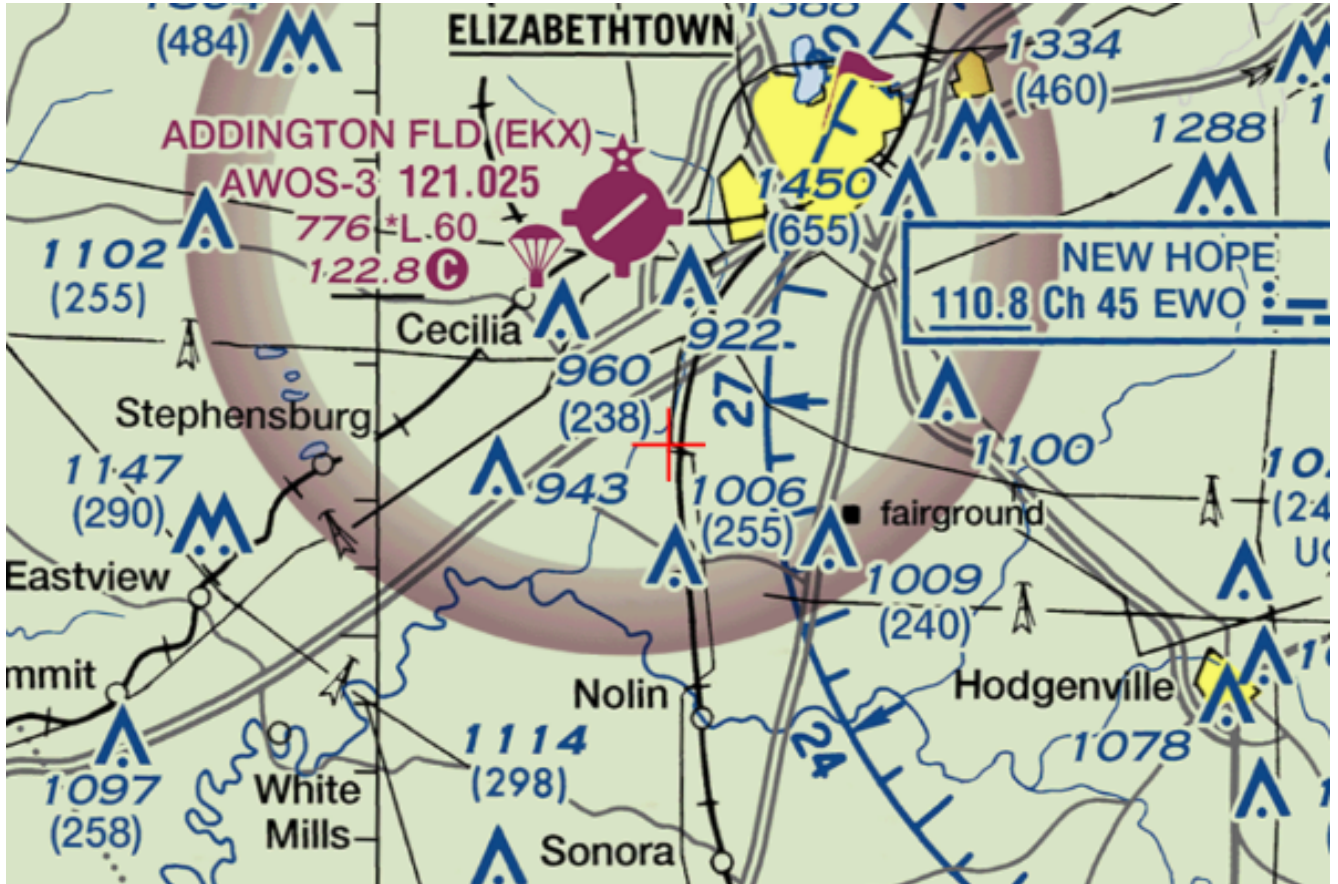
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15400-OE



Sectional Map for ASN 2022-ASO-15400-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15401-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-39-08.20N NAD 83
Longitude:	85-53-33.40W
Heights:	687 feet site elevation (SE) 200 feet above ground level (AGL) 887 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15401-OE

**Signature Control No: 525291203-529276374**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15401-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 887 feet above mean sea level.

**Location:** The structure will be located 2.54 nautical miles southeast of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

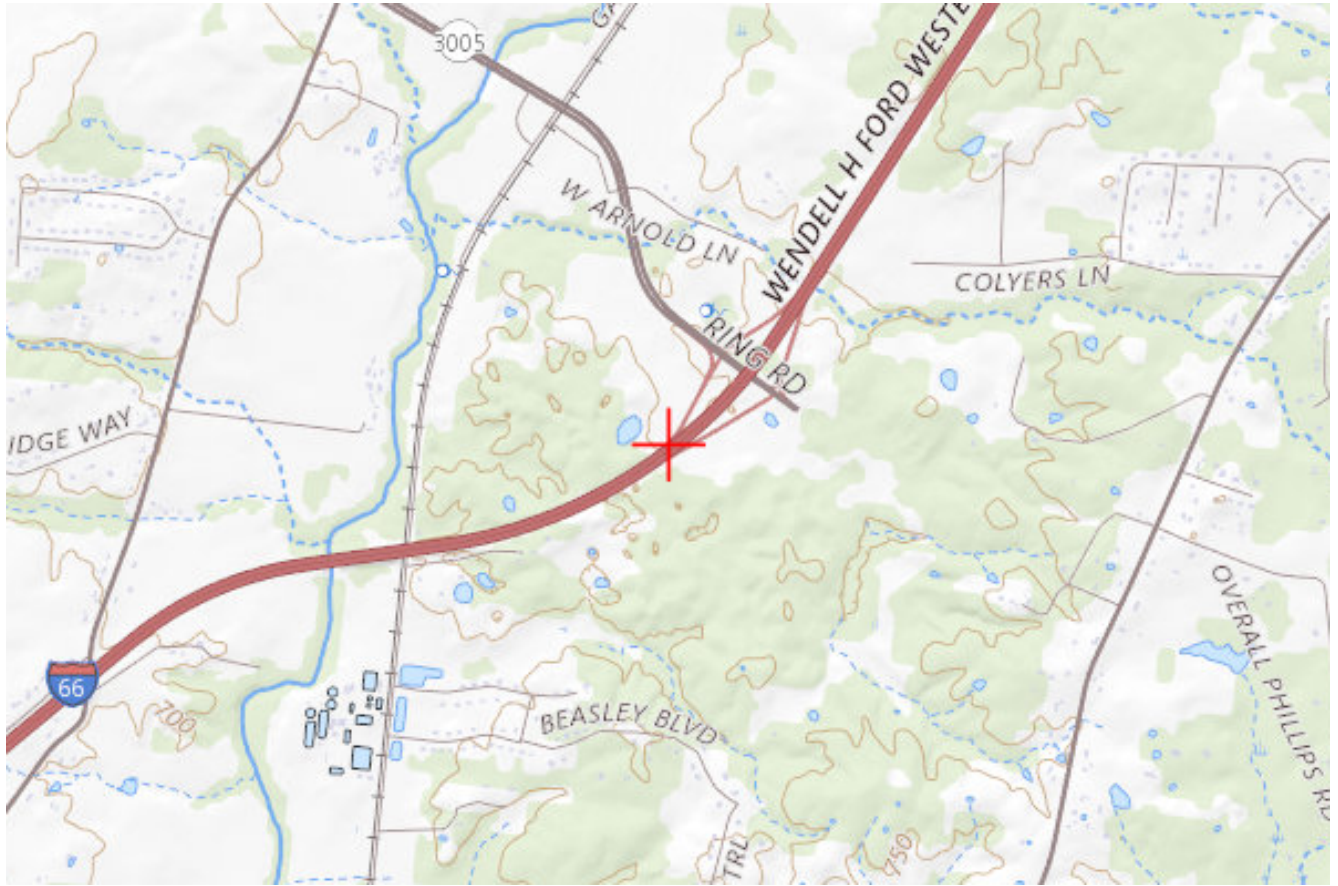
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

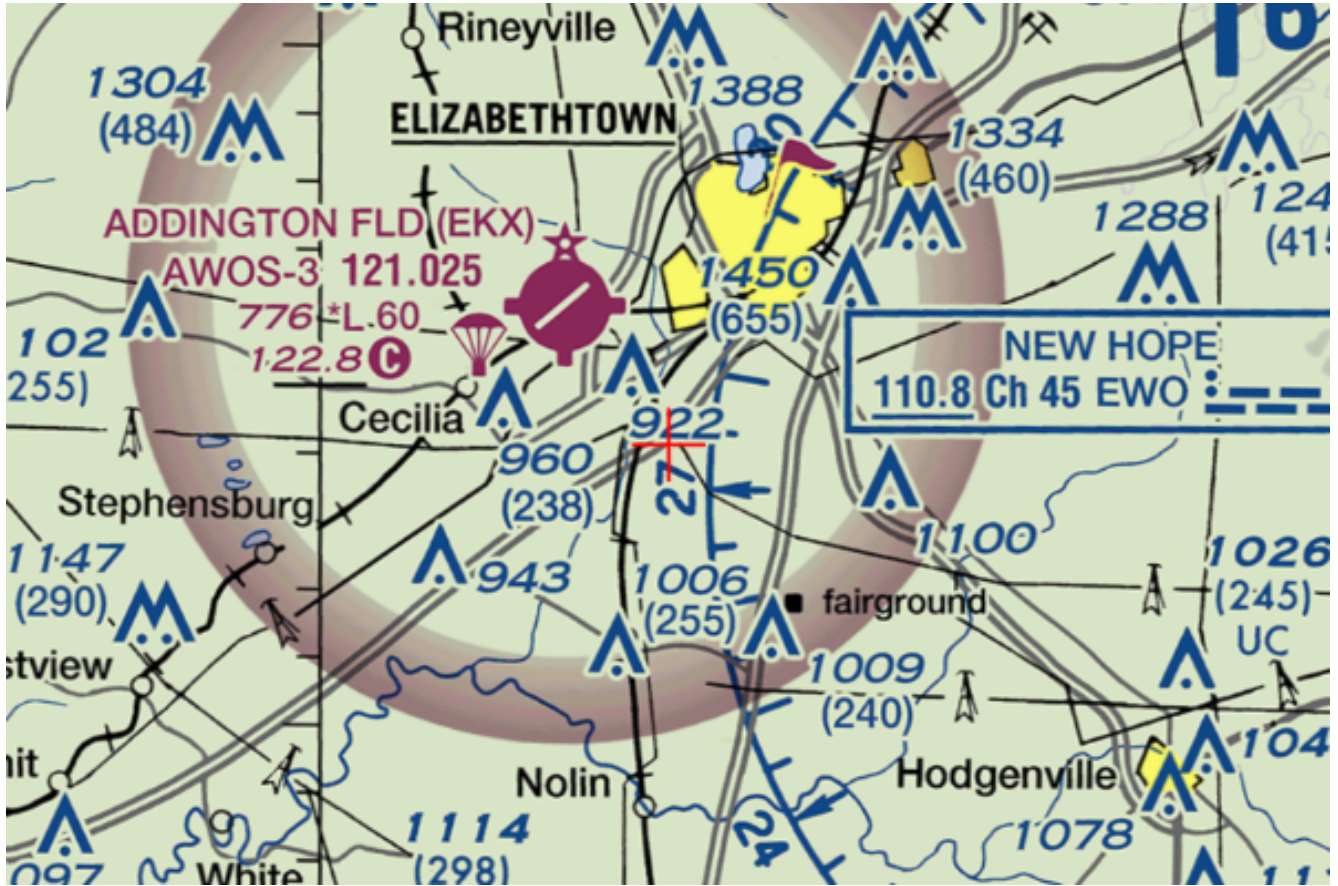
**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15401-OE





Sectional Map for ASN 2022-ASO-15401-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15402-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-39-40.96N NAD 83
Longitude:	85-53-54.97W
Heights:	680 feet site elevation (SE) 200 feet above ground level (AGL) 880 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15402-OE

**Signature Control No: 525291205-529276373**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15402-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 880 feet above mean sea level.

**Location:** The structure will be located 1.94 nautical miles southeast of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

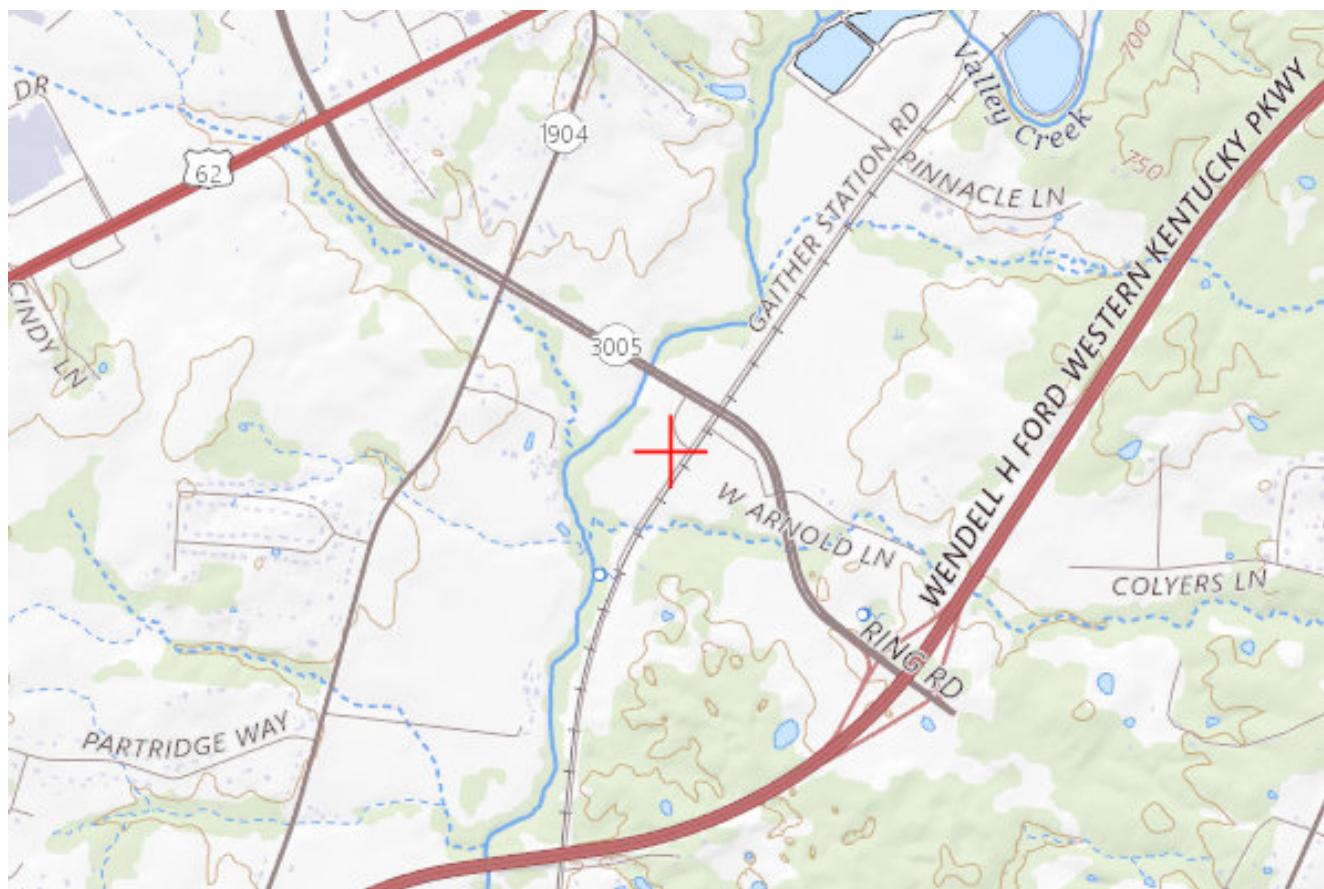
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15402-OE







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15403-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-39-41.64N NAD 83
Longitude:	85-53-55.88W
Heights:	680 feet site elevation (SE) 200 feet above ground level (AGL) 880 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15403-OE

**Signature Control No: 525291207-529276375**  
Chris Smith  
Specialist

( TMP )



**Additional Condition(s) or Information for ASN 2022-ASO-15403-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 880 feet above mean sea level.

**Location:** The structure will be located 1.92 nautical miles southeast of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

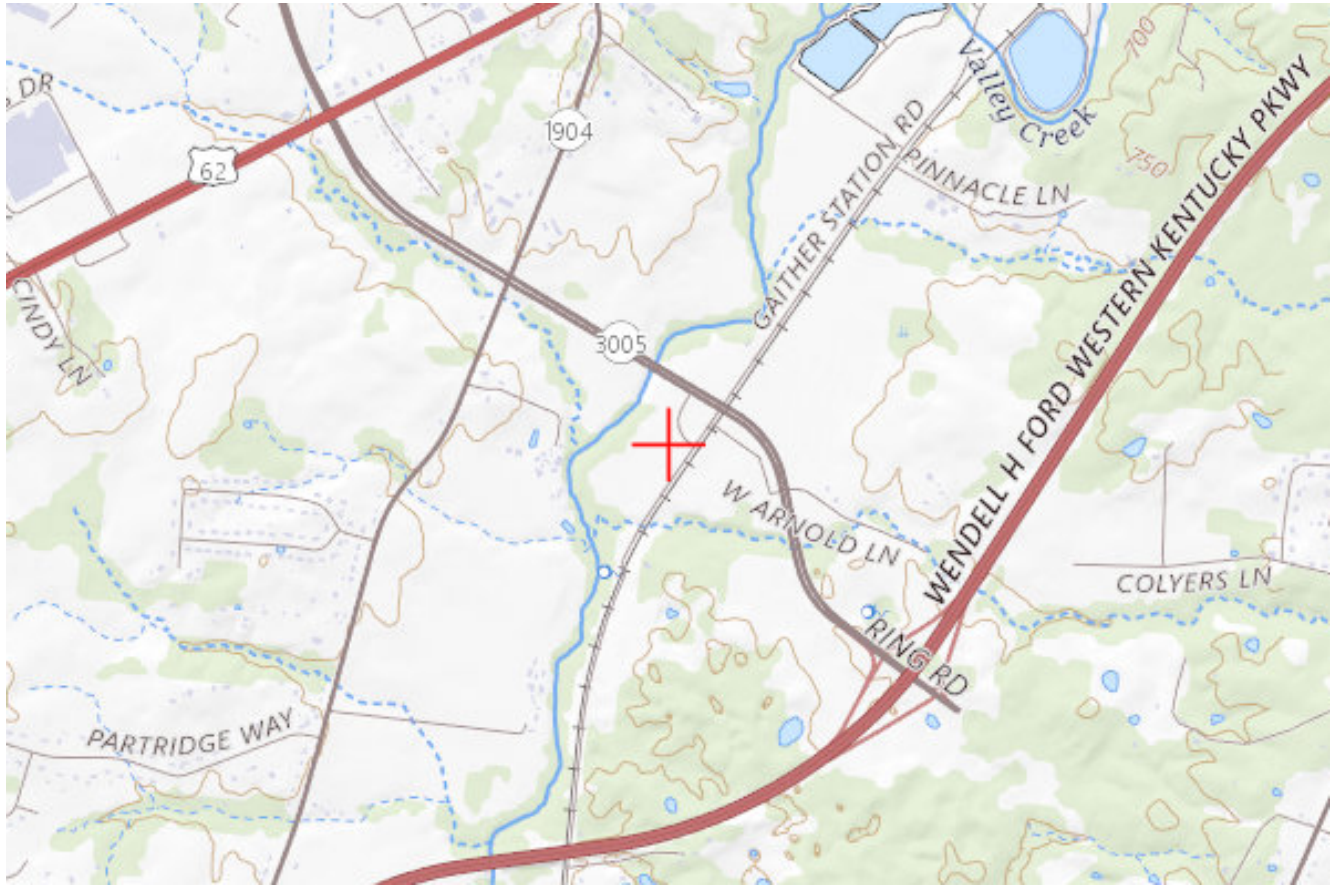
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

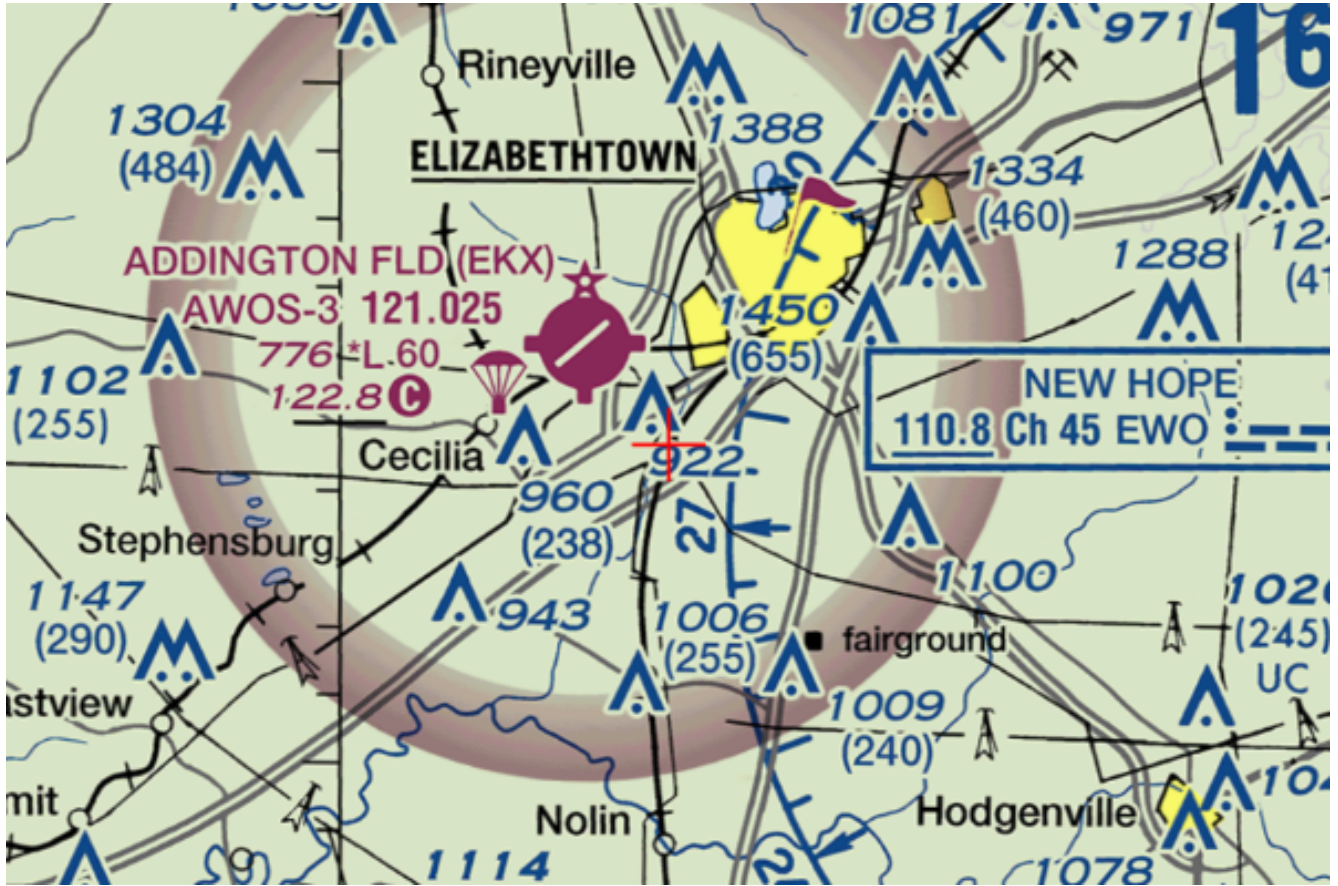
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15403-OE



Sectional Map for ASN 2022-ASO-15403-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15404-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-39-08.60N NAD 83
Longitude:	85-54-34.62W
Heights:	688 feet site elevation (SE) 200 feet above ground level (AGL) 888 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15404-OE

**Signature Control No: 525291208-529276372**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15404-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 888 feet above mean sea level.

**Location:** The structure will be located 2.14 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

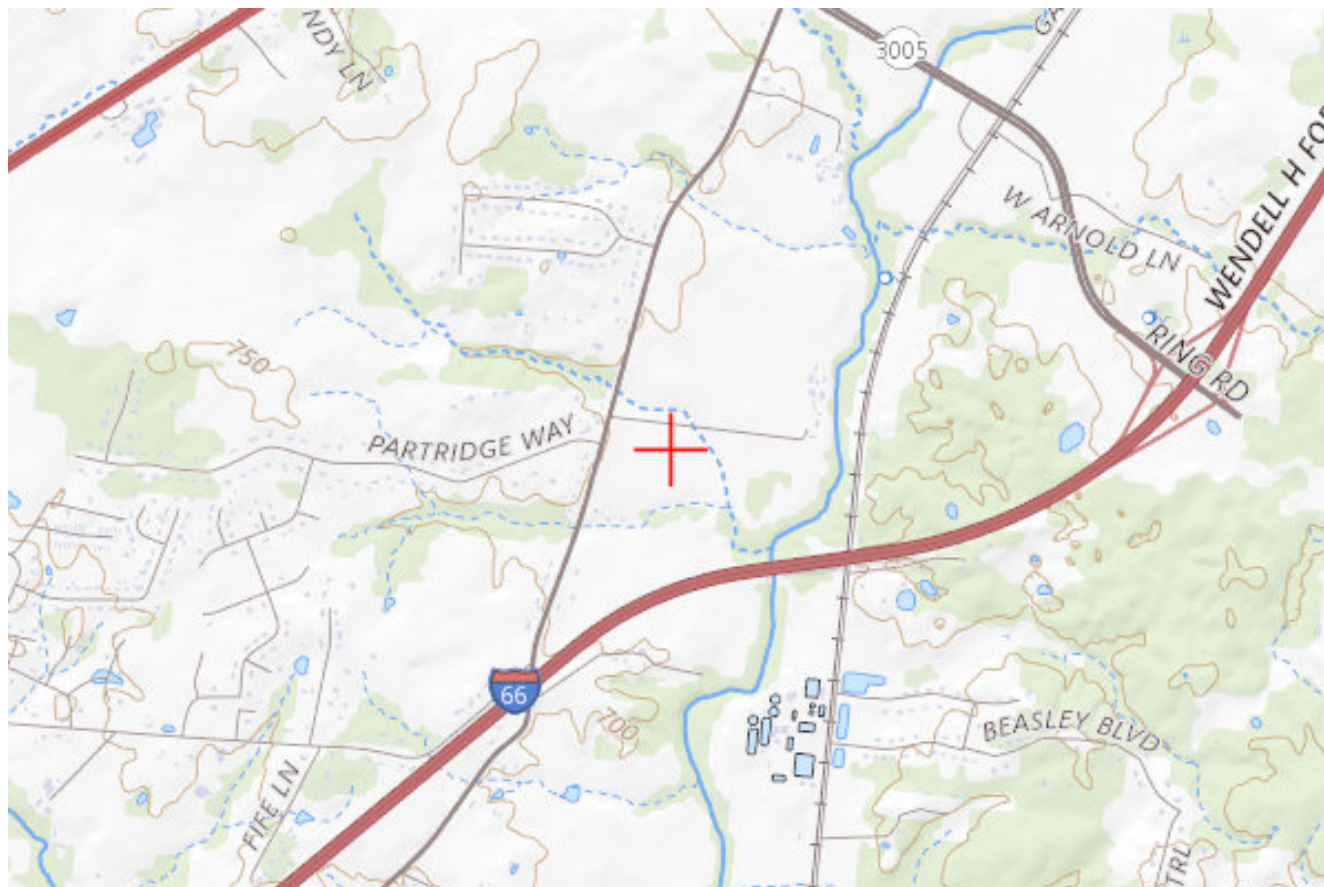
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15404-OE



Sectional Map for ASN 2022-ASO-15404-OE







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15405-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-37-50.43N NAD 83
Longitude:	85-54-38.83W
Heights:	689 feet site elevation (SE) 200 feet above ground level (AGL) 889 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15405-OE

**Signature Control No: 525291210-529276370**

( TMP )

Chris Smith  
Specialist

**Additional Condition(s) or Information for ASN 2022-ASO-15405-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 889 feet above mean sea level.

**Location:** The structure will be located 3.38 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

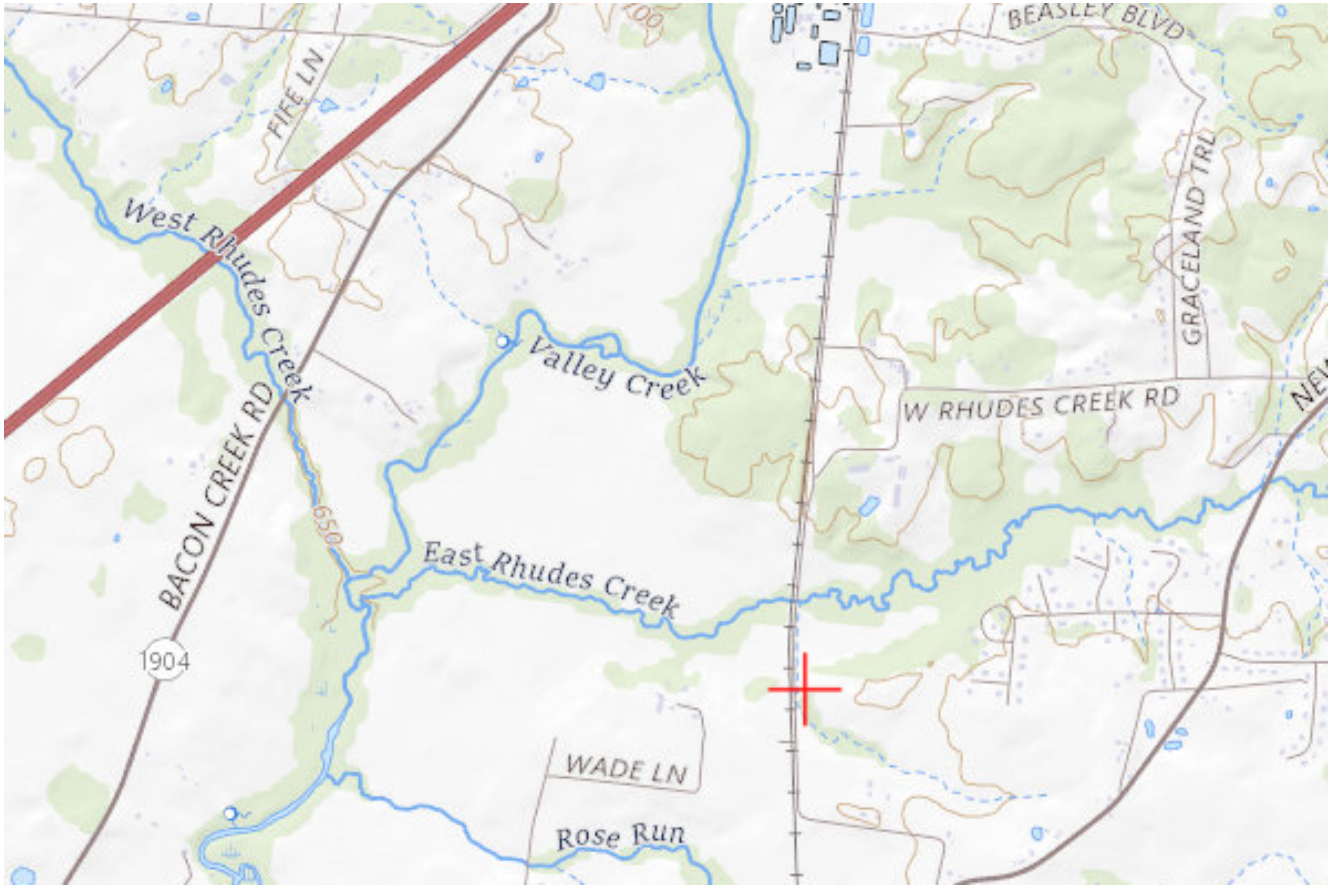
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

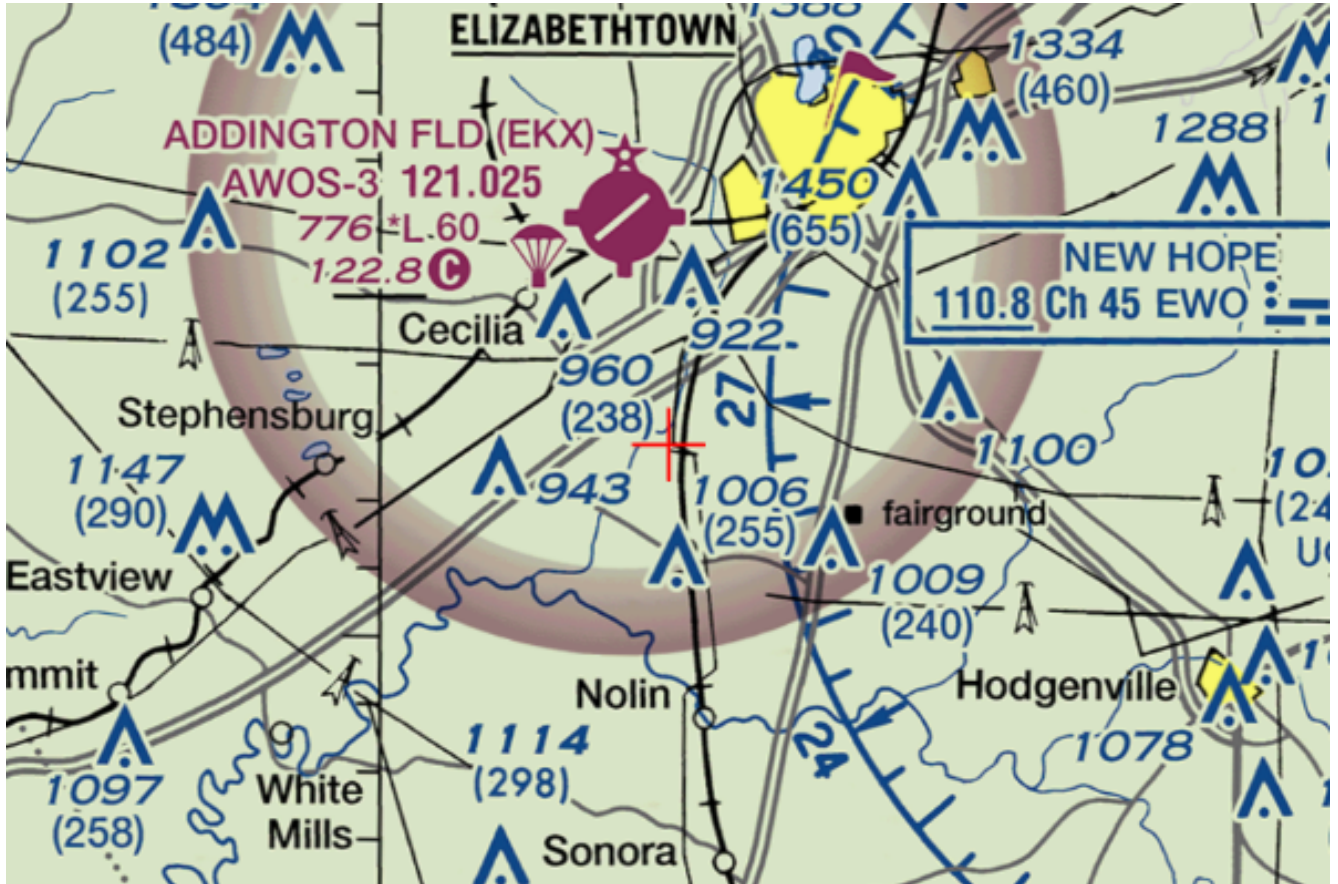
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15405-OE



Sectional Map for ASN 2022-ASO-15405-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15406-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-37-25.32N NAD 83
Longitude:	85-54-23.39W
Heights:	663 feet site elevation (SE) 200 feet above ground level (AGL) 863 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15406-OE

**Signature Control No: 525291212-529276328**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15406-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 863 feet above mean sea level.

**Location:** The structure will be located 3.84 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

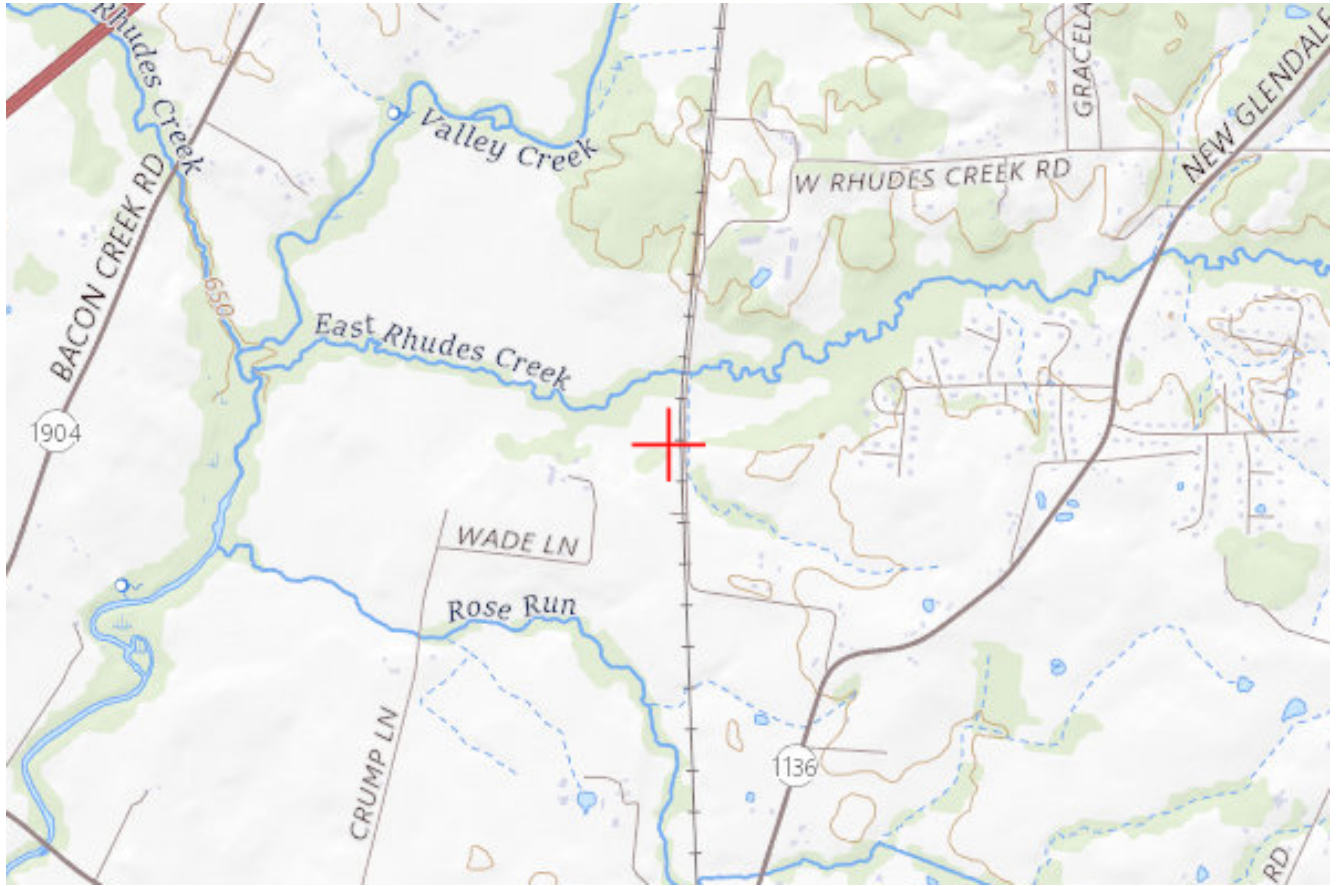
This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**



TOPO Map for ASN 2022-ASO-15406-OE







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15407-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-37-23.29N NAD 83
Longitude:	85-54-19.50W
Heights:	685 feet site elevation (SE) 200 feet above ground level (AGL) 885 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15407-OE

**Signature Control No: 525291214-529276331**  
Chris Smith  
Specialist

( TMP )

**Additional Condition(s) or Information for ASN 2022-ASO-15407-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 885 feet above mean sea level.

**Location:** The structure will be located 3.88 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

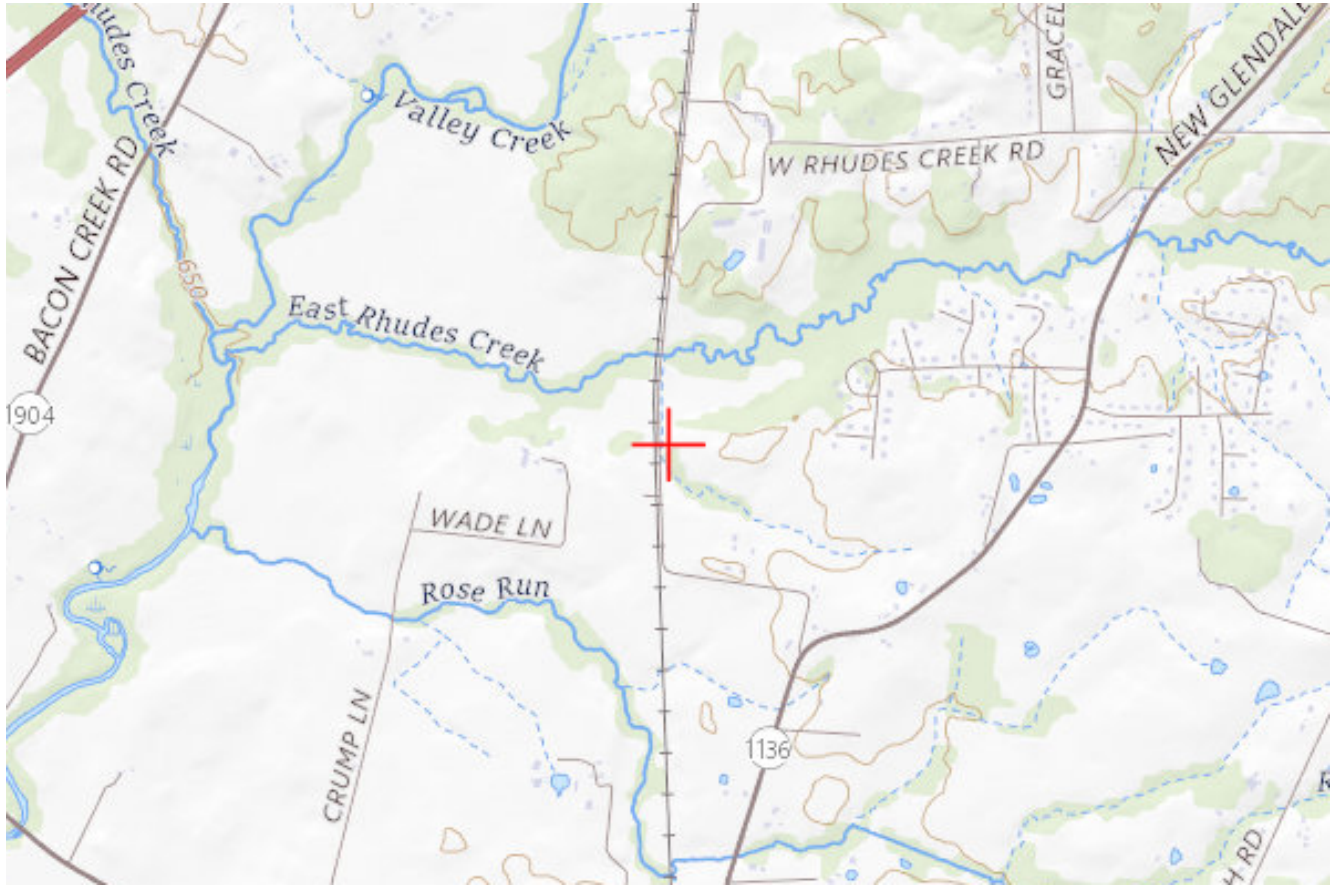
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

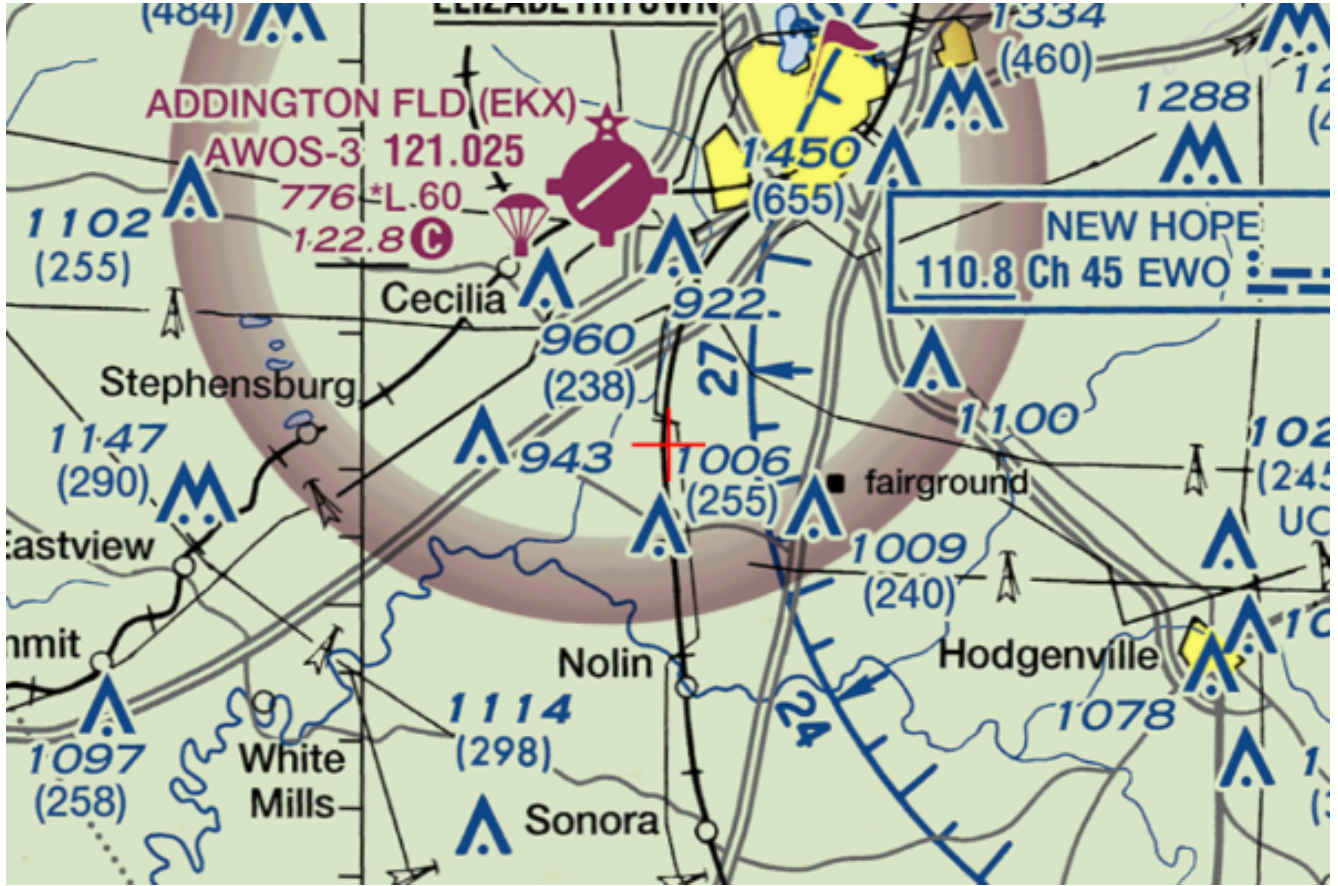
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15407-OE



Sectional Map for ASN 2022-ASO-15407-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15408-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-36-33.63N NAD 83
Longitude:	85-54-17.30W
Heights:	685 feet site elevation (SE) 200 feet above ground level (AGL) 885 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).



If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15408-OE

**Signature Control No: 525291215-529276329**

( TMP )

Chris Smith  
Specialist

**Additional Condition(s) or Information for ASN 2022-ASO-15408-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 885 feet above mean sea level.

**Location:** The structure will be located 4.69 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

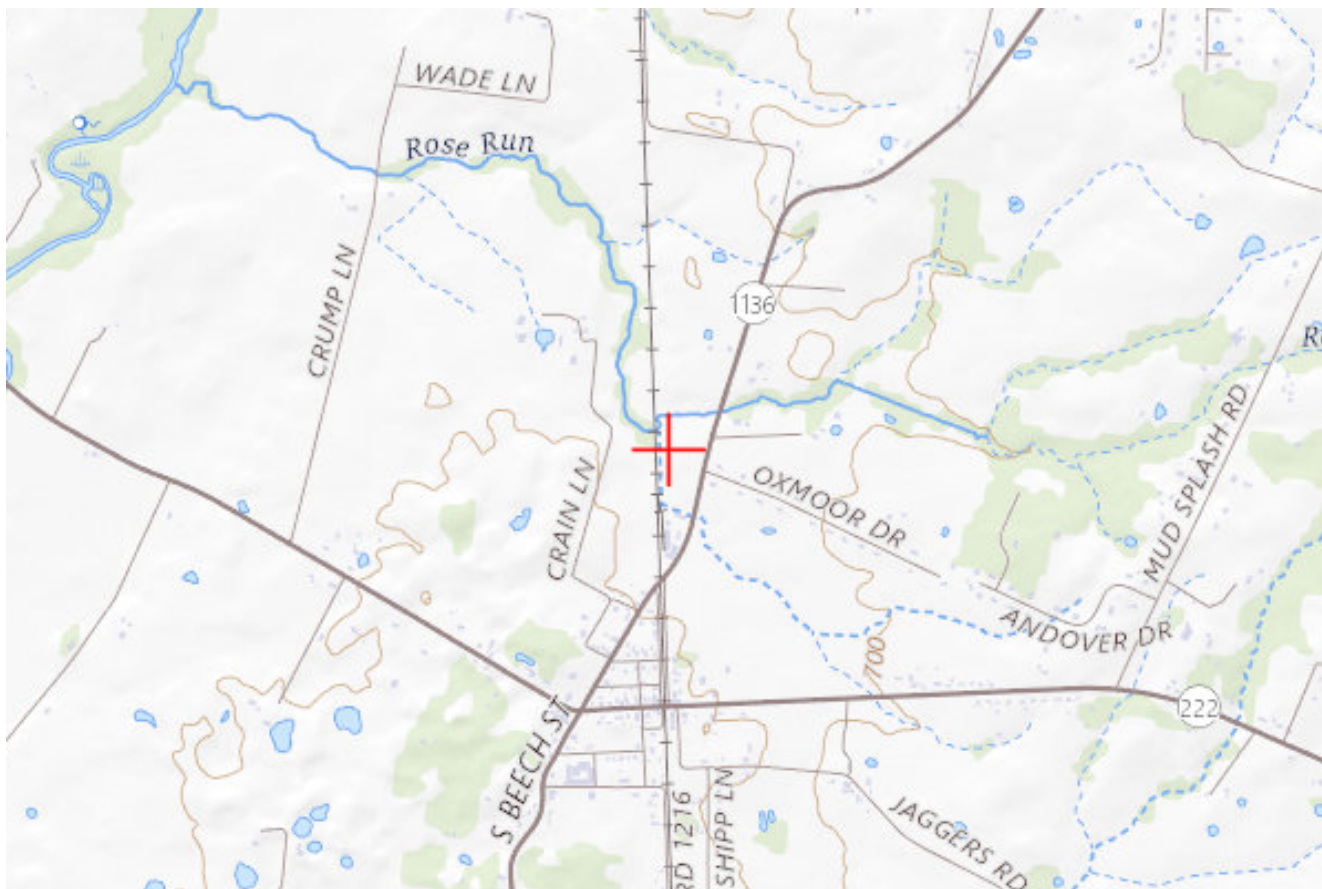
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

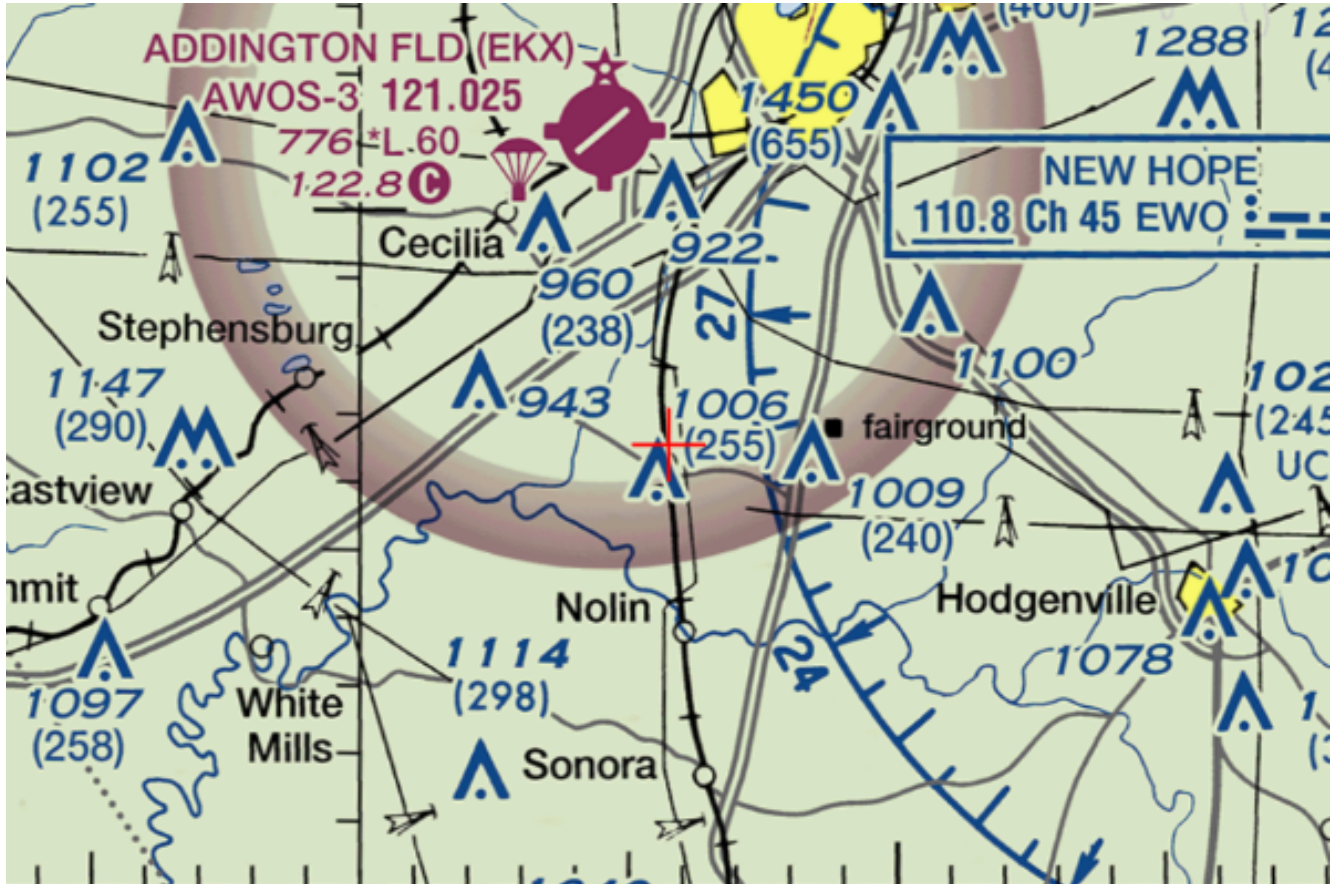
It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15408-OE



Sectional Map for ASN 2022-ASO-15408-OE





Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2022-ASO-15409-OE

Issued Date: 05/09/2022

Michael Kern  
LG&E - Michael Kern  
1 Quality Street  
Lexington, KY 40507

**\*\*DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE\*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Crane Operating Limits
Location:	Glendale, KY
Latitude:	37-36-04.93N NAD 83
Longitude:	85-54-01.76W
Heights:	698 feet site elevation (SE) 200 feet above ground level (AGL) 898 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does not exceed obstruction standards and would not be a hazard to air navigation provided the condition(s), if any, in this letter is (are) met:

**\*\*SEE ATTACHMENT FOR ADDITIONAL CONDITION(S) OR INFORMATION\*\***

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

If you have any questions, please contact our office at (817) 222-5928, or [chris.smith@faa.gov](mailto:chris.smith@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-ASO-15409-OE

**Signature Control No: 525291217-529276330**

( TMP )

Chris Smith  
Specialist

**Additional Condition(s) or Information for ASN 2022-ASO-15409-OE**

**Proposal:** To construct and/or operate a(n) Crane to a height of 200 feet above ground level, 898 feet above mean sea level.

**Location:** The structure will be located 5.2 nautical miles south of EKX Airport reference point.

**Part 77 Obstruction Standard(s) Exceeded and Aeronautical Impacts, if any:**

Aeronautical study revealed that the temporary structure will not exceed any Part 77 obstruction standard. Aeronautical study confirmed that the temporary structure will have no effect on any existing or proposed arrival, departure or en route instrument/visual flight rules (IFR/VFR) operations or procedures. Additionally, aeronautical study confirmed that the temporary structure will have no physical or electromagnetic effect on the operation of air navigation and communications facilities and will not impact any airspace and routes used by the military. Based on this aeronautical study, the FAA finds that the temporary structure will have no adverse effect on air navigation and will not impact any aeronautical operations or procedures.

Based on this aeronautical study, the structure would not constitute a substantial adverse effect on aeronautical operations or procedures because it will be temporary. The temporary structure would not be considered a hazard to air navigation provided all of the conditions specified in this determination are strictly met.

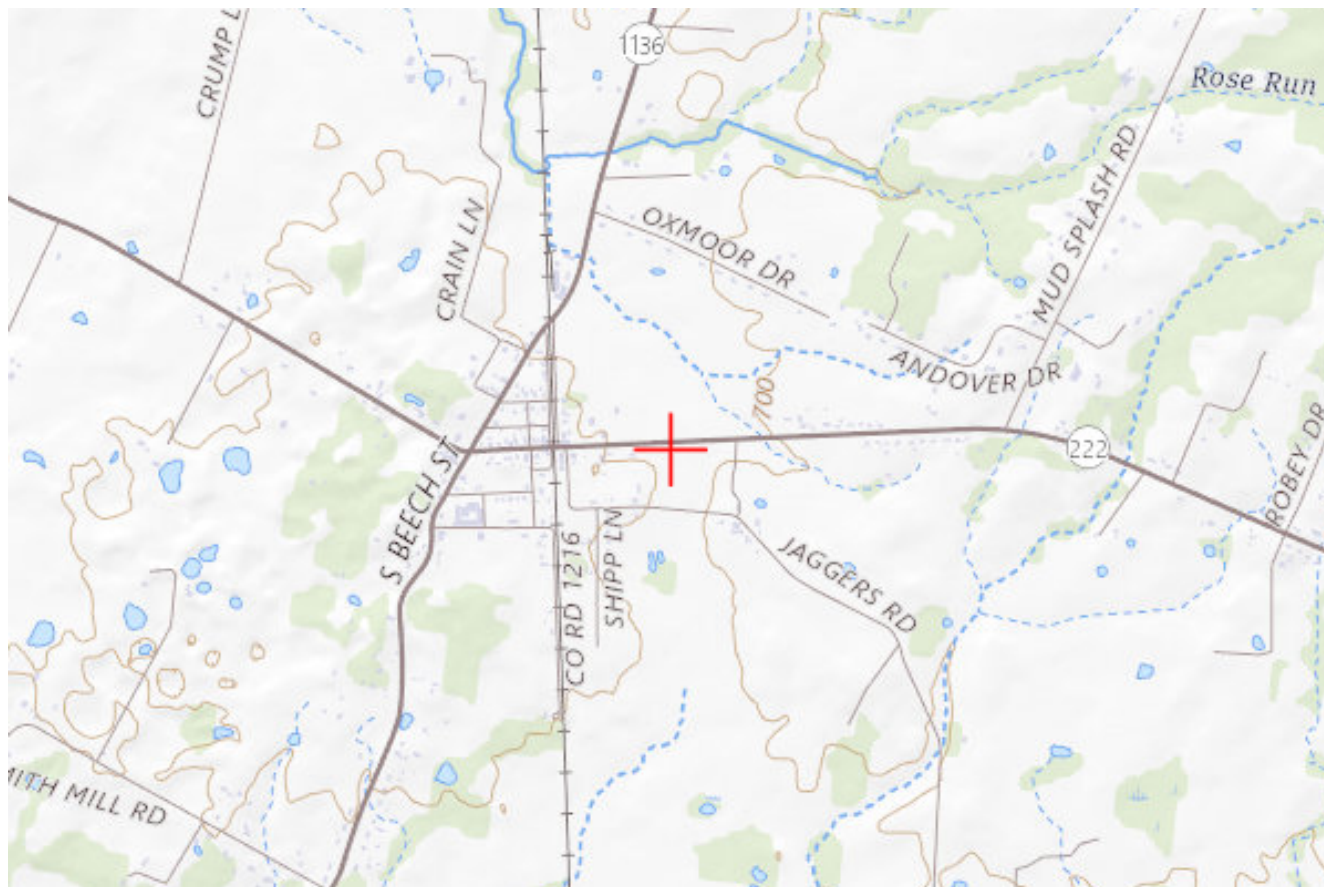
Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/09/2023 unless extended, revised, or terminated by the issuing office.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed within 5 days after the temporary structure is dismantled.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

TOPO Map for ASN 2022-ASO-15409-OE





Sectional Map for ASN 2022-ASO-15409-OE

